



AGENDA

**COMMUNITY REDEVELOPMENT BOARD
FEBRUARY 3, 2026
AT 4:00 PM
COMMISSION CHAMBERS
121 SW FLAGLER AVE.
STUART, FLORIDA 34994**

COMMUNITY REDEVELOPMENT BOARD

**Chair - Frank McChrystal
Vice Chair - Bonnie Moser
Board Member - Philip DeBerard
Board Member - Frederick James
Board Member - Will Laughlin
Board Member - Jeff Manera
Board Member - Scott Whalen**

ADMINISTRATIVE

**CRA Executive Director - Pinal Gandhi-Savdas
Board Secretary - Susej T. Meleqi**

Agenda items are available on our website at <http://www.cityofstuart.us>
Phone: (772) 288-5306. Fax: (772) 288-5305. E-mail: mkindel@ci.stuart.fl.us

In compliance with the Americans with Disabilities Act (ADA), anyone who needs a special accommodation to attend this meeting should contact the City's ADA coordinator at 772-288-5306 at least 48 hours in advance of the meeting, excluding Saturday and Sunday.

If a person decides to appeal any decision made by the Board with respect to any matter considered at this meeting, he will need a record of the proceeding, and that for such purpose he may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

(RC) next to an item denotes there is a City Code requirement for a Roll Call vote.
(QJ) next to an item denotes that it is a quasi-judicial matter or public hearing.

CALL TO ORDER

ROLL CALL

PLEDGE OF ALLEGIANCE

APPROVAL OF AGENDA

APPROVAL OF MINUTES

1. APPROVAL OF 01/06/2026 CRB MINUTES

COMMENTS FROM THE PUBLIC (Non-Agenda Related) (3 Minutes Max.)

COMMENTS BY BOARD MEMBERS (Non-Agenda Items)

ACTION ITEMS

2. 400 S.E. OSCEOLA STREET REZONE TO COMMERCIAL PLANNED UNIT DEVELOPMENT (CPUD) (QUASI-JUDICIAL) (RC):

ORDINANCE No. 2549-2026; AN ORDINANCE OF THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA, REZONING A +/-0.45 ACRE PARCEL LOCATED AT 400 S.E. OSCEOLA STREET WITHIN THE COMMUNITY REDEVELOPMENT AREA SPECIAL DISTRICT, FOR PROPERTY LEGALLY DESCRIBED AS LOTS 23, 24 AND 25, BLOCK 1, STYPMANN'S SUBDIVISION, FROM R-3 – RESIDENTIAL MULTI-FAMILY/OFFICE ZONING DESIGNATION TO COMMERCIAL PLANNED UNIT DEVELOPMENT DESIGNATION AND ADDING BARBERSHOP, BEAUTY SALONS, AND SPECIALTY SALONS AS ALLOWABLE USES; PROVIDING FOR CONFLICT; PROVIDING FOR SEVERABILITY; PROVIDING FOR AN EFFECTIVE DATE; AND FOR OTHER PURPOSES.

3. AVONLEA COMMERCIAL CORNERS EAST MAJOR AMENDMENT TO THE NEW AVONLEA PUD FOR PARCEL D (LOT 11)

ORDINANCE No. 2550-2026; AN ORDINANCE OF THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA, PROVIDING FOR THE THIRD AMENDMENT OF THE "NEW AVONLEA PLANNED UNIT DEVELOPMENT" CONSISTING OF 12.40 ACRES ZONED RESIDENTIAL PLANNED UNIT DEVELOPMENT, 16.24 ACRES ZONED COMMERCIAL PLANNED UNIT DEVELOPMENT, AND 20.4 ACRES OF WETLAND/UPLAND PRESERVE ZONED RESIDENTIAL PLANNED UNIT DEVELOPMENT, BEING GENERALLY LOCATED BETWEEN NE DIXIE HIGHWAY TO THE SOUTH, LAND ON BOTH SIDES OF GREEN RIVER PARKWAY ON THE WEST, LAND ON BOTH SIDES OF CARDINAL AVENUE ON THE EAST AND THE CITY BOUNDARY TO THE NORTH, FOR PROPERTY LEGALLY DESCRIBED BY EXHIBIT "A" ATTACHED; AMENDING THE COMMERCIAL PLANNED UNIT DEVELOPMENT TO PROVIDE FOR THE APPROVAL OF THE MASTER PARCEL SITE PLAN FOR THE AVONLEA COMMERCIAL EAST PARCEL D (LOT 11) TO BE IDENTIFIED AS AVONLEA COMMERCIAL CORNERS EAST; PROVIDING FOR THE APPROVAL OF THE MASTER SITE OFF-SITE INFRASTRUCTURE PLAN FOR THE ENTIRE PLANNED UNIT DEVELOPMENT; PROVIDING FOR REVISED AND NEW DEVELOPMENT CONDITIONS APPROVAL FOR PARCEL D (LOT 11); PROVIDING FOR AMENDED AND NEW TIMETABLES FOR DEVELOPMENT; DECLARING THE

PLAN TO BE CONSISTENT WITH THE CITY'S COMPREHENSIVE PLAN; PROVIDING DIRECTIONS TO THE CITY CLERK; PROVIDING FOR REPEAL OF ALL ORDINANCES IN CONFLICT; PROVIDING FOR SEVERABILITY; PROVIDING FOR AN EFFECTIVE DATE, AND FOR OTHER PURPOSES.

STAFF UPDATE

4. ADVISORY BOARD ORIENTATION REVIEW AND PUBLIC RECORDS PRESENTATION

ADJOURNMENT

WHAT IS CIVILITY? Civility is caring about one's identity, needs and beliefs without degrading someone else's in the process. Civility is more than merely being polite. Civility requires staying "present" even with those persons with whom we have deep-rooted and perhaps strong disagreements. It is about constantly being open to hear, learn, teach and change. It seeks common ground as a beginning point for dialogue. It is patience, grace, and strength of character. Civility is practiced in our City Hall.

PUBLIC COMMENT: If a member of the public wishes to comment upon ANY subject matter, including quasi-judicial matters, please submit a Request to Speak form. These forms are available in the back of the Commission Chambers, and should be given to the City Clerk prior to introduction of the item number you would like to address.

CONSENT CALENDAR: Those matters included under the Consent Calendar are self-explanatory, non-controversial, and are not expected to require review or discussion. All items will be enacted by one motion. If discussion on an item is desired by any City Commissioner that item may be removed by a City Commissioner from the Consent Calendar and considered separately. If an item is quasi-judicial it may be removed by a Commissioner or any member of the public from the Consent Calendar and considered separately.

QUASI-JUDICIAL HEARINGS: Some of the matters on the Agenda may be "quasi-judicial" in nature. City Commissioners will disclose all ex-parte communications, and may be subject to voir dire by any interested party regarding those communications. All witnesses testifying will be "sworn" prior to their testimony. However, the public is permitted to comment without being sworn. Unsworn testimony will be given appropriate weight and credibility by the City Commission.

**CITY OF STUART, FLORIDA
AGENDA ITEM REQUEST
Community Redevelopment Board**

Meeting Date: 2/3/2026

Prepared by: Susej Meleqi

Title of Item:

APPROVAL OF 01/06/2026 CRB MINUTES

Summary Explanation/Background Information on Agenda Request:

APPROVAL OF 01/06/2026 CRB MINUTES

Funding Source:

N/A

Recommended Action:

Approve Minutes.

ATTACHMENTS:

1. 01062026 CRB Minutes

**MINUTES
COMMUNITY REDEVELOPMENT BOARD OF THE CITY OF STUART
JANUARY 6, 2026
AT 4:00 PM
COMMISSION CHAMBERS
121 SW FLAGLER AVE.
STUART, FLORIDA 34994**

COMMUNITY REDEVELOPMENT BOARD

**Chair - Frank McChrystal
Vice Chair - Bonnie Moser
Board Member - Philip DeBerard
Board Member - Frederick James
Board Member - Will Laughlin
Board Member - Jeff Manera
Board Member - Scott Whalen**

ADMINISTRATIVE

**CRA Executive Director - Pinal Gandhi-Savdas
Board Secretary - Susej T. Meleqi**

CALL TO ORDER

4:00 PM

ROLL CALL

PRESENT: Vice Chair Moser, Board Member DeBerard, Board Member James, Board Member Laughlin, Board Member Manera, Board Member McChrystal, Board Member Whalen
ABSENT:

PLEDGE OF ALLEGIANCE

1. ADVISORY BOARD MEMBER OATH

Lee Baggett, City Attorney, provided the Oath to all Board Members

2. ANNUAL BOARD REORGANIZATION - SELECTION OF CHAIR AND VICE CHAIR

Vice Chair Moser asked that each Board Member provide a brief introduction of themselves.

4:06 PM MOTION: Frank McChrystal as Chair.

MOVED BY: Philip DeBerard

SECONDED BY: Jeff Manera

Motion approved unanimously.

**4:08 PM MOTION: Bonnie Moser as Vice Chair.
MOVED BY: Frederick James
SECONDED BY: William Laughlin
Motion approved unanimously.**

APPROVAL OF AGENDA

**4:09 PM MOTION: Approve.
MOVED BY: Bonnie Moser
SECONDED BY: Scott Whalen
Motion approved unanimously.**

APPROVAL OF MINUTES

3. APPROVAL OF 11/04/2025 CRB MINUTES

**4:10 PM MOTION: Approve.
MOVED BY: Bonnie Moser
SECONDED BY: William Laughlin
Motion approved unanimously.**

COMMENTS FROM THE PUBLIC (Non-Agenda Related) (3 Minutes Max.)

None.

COMMENTS BY BOARD MEMBERS (Non-Agenda Items)

Vice Chair Moser

- Noted a recent incident in which a child pedestrian was struck in a crosswalk on MLK Boulevard and recommended reviewing the project plans to ensure maximum safety for children traveling to the middle school and Boys & Girls Club.

Chair McChrystal

- Emphasized the importance of carefully evaluating projects to prioritize those that provide clear, citywide value and distinguishing meaningful public benefit from nonessential elements.

ACTION ITEMS

4. PRESENTATION OF STUART COMMUNITY REDEVELOPMENT AREA (CRA) HISTORY, TIF REVENUE, BUDGET, AND PROJECTS

Pinal Gandhi Savdas, CRA Executive Director, presented on what CRA is, how it was formed, CRA boundaries, CRA plan goals and objectives, Tax Increment Financing (TIF), history of TIF, where the money was invested, infrastructure/capital improvements completed, CRA Programs, master plan and studies/reports, FY 2026 Budget, CRA budget revenues FY 26, CRA appropriations by category, CRA projects, Streetscape improvements, CDBG projects, Drainage and sidewalk improvement projects.

Board Member Laughlin asked several questions in regard to CDBG funds, FY 26 Budgets, admin cost, landscape improvement program, the National Registry of Historic District for East Stuart, tram traffic with ridership, City properties own, definition of slum and blight. Ms. Gandhi-Savdas and Mr. Louis Boglioli, Interim City Manager answered the questions.

Board Member James asked how Project Lift helps the tax base. Ms. Gandhi-Savdas stated it helps the surrounding properties by seeing one property get fixed up.

Chair McChrystal asked about the area square mile area and asked if she had the population in CRA vs just general City, and budget.

Vice Chair Moser stated she is impressed with the way the CRA gets these grants and expressed kudos to the CRA department, asked if there are any plans to expand the CRA. Mr. Boglioli explained.

5. NOMINATE BOARD MEMBER FOR COMMUNITY REDEVELOPMENT AGENCY (RC):

RESOLUTION No. 01-2026 CRB; A RESOLUTION OF THE COMMUNITY REDEVELOPMENT BOARD OF THE CITY OF STUART, FLORIDA TO NOMINATE TWO MEMBERS TO SERVE ON THE COMMUNITY REDEVELOPMENT AGENCY; FOR THE REMAINING CURRENT TERM FROM PRESENT TO DECEMBER 31, 2026; PROVIDING AN EFFECTIVE DATE; AND FOR OTHER PURPOSES.

City Attorney Baggett explained that the CRA consists of the five City Commissioners plus two CRB-appointed members; appointments are made annually and require attendance at monthly CRA meetings.

5:13 PM MOTION: Board Member James and Board Member DeBerard as our representatives.

MOVED BY: William Laughlin

SECONDED BY: Bonnie Moser

Motion approved unanimously.

STAFF UPDATE

None.

ADJOURNMENT

5:14 PM

Susej T. Meleqi, Board Secretary

Frank McChrystal, Chair

Minutes to be approved at the CRB Meeting this 3rd day of February, 2026.

**CITY OF STUART, FLORIDA
AGENDA ITEM REQUEST
Community Redevelopment Board**

Meeting Date: 2/3/2026

Prepared by: Jodi Nentwick

Title of Item:

400 S.E. OSCEOLA STREET REZONE TO COMMERCIAL PLANNED UNIT DEVELOPMENT (CPUD) (QUASI-JUDICIAL) (RC):

ORDINANCE No. 2549-2026; AN ORDINANCE OF THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA, REZONING A +/-0.45 ACRE PARCEL LOCATED AT 400 S.E. OSCEOLA STREET WITHIN THE COMMUNITY REDEVELOPMENT AREA SPECIAL DISTRICT, FOR PROPERTY LEGALLY DESCRIBED AS LOTS 23, 24 AND 25, BLOCK 1, STYPMANN'S SUBDIVISION, FROM R-3 – RESIDENTIAL MULTI-FAMILY/OFFICE ZONING DESIGNATION TO COMMERCIAL PLANNED UNIT DEVELOPMENT DESIGNATION AND ADDING BARBERSHOP, BEAUTY SALONS, AND SPECIALTY SALONS AS ALLOWABLE USES; PROVIDING FOR CONFLICT; PROVIDING FOR SEVERABILITY; PROVIDING FOR AN EFFECTIVE DATE; AND FOR OTHER PURPOSES.

Summary Explanation/Background Information on Agenda Request:

On August 13, 2025, an application was submitted to the City of Stuart by Michael Beneditti, Osceola Street, Suite 3, Stuart, FL 34994 (Applicant), for a Petition requesting approval of a rezoning from the existing R-3-Residential Multi-Family/Office to a Commercial Planned Unit Development (CPUD) within the Community Redevelopment Area (CRA) Special District to add barbershop, beauty salons, and specialty salons as a permitted use property located 400 S.E. Osceola Street.

The City of Stuart Land Development (LDC) defines a Barbershop, Beauty Salons, and Specialty Salons; and

Barbershop means any place of business wherein the practice of barbering is carried on, as defined in F.S. § 473.034, amended from time to time.

Beauty Salon means any establishment where cosmetology services include hair care, nail care, and skin care on a regular basis for compensation.

Specialty Salon means any place of business wherein the practice of one or all of the following specialties are engaged in or carried on:

- (a) Manicuring, or the cutting, polishing, tinting, coloring, cleansing, adding, or extending of the nails, and massaging of the hands. This term includes any procedure or process for the affixing of artificial nails, except those nails which may be applied solely by use of a simple adhesive.
- (b) Pedicuring, or the shaping, polishing, tinting, or cleansing of the nails of the feet, and massaging or beautifying of the feet.
- (c) Facials, or the massaging or treating of the face or scalp with oils, creams, lotions, or other preparations, and skin care services

Funding Source:

N/A

Recommended Action:

Staff is requesting the Board forward a recommendation of approval to the City Commission to rezone to

a Commercial Planned Unit Development (CPUD) zoning designation.

ATTACHMENTS:

1. Ordinance 2549-2026 -Osceola CPUD
2. CRB Staff Report - CPUD
3. CRB Project Narrative
4. CRB Site Plan
5. CRB Landscape Plans
6. CRB Survey
7. CRB Uniform Signage Plan for CPUD Salon Application
8. CRB Drainage Statement
9. CRB Traffic Statement
10. CRB City's Traffic Consultant Approval Letter



**BEFORE THE CITY COMMISSION
CITY OF STUART, FLORIDA**

ORDINANCE NUMBER 2549-2026

* * * * *

AN ORDINANCE OF THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA, REZONING A +/-0.45 ACRE PARCEL LOCATED 400 S.E. OSCEOLA STREET, FOR PROPERTY LEGALLY DESCRIBED AS LOTS 23, 24, AND 25, BLOCK 1, STYPMANN'S SUBDIVISION, WITHIN THE COMMUNITY REDEVELOPMENT AREA SPECIAL DISTRICT FROM R-3 – RESIDENTIAL MULTI-FAMILY/OFFICE ZONING DESIGNATION TO COMMERCIAL PLANNED UNIT DEVELOPMENT DESIGNATION AND ADDING BARBERSHOP, BEAUTY SALONS, AND SPECIALTY SALONS AS ALLOWABLE USES; PROVIDING FOR CONFLICT; PROVIDING FOR SEVERABILITY; PROVIDING FOR AN EFFECTIVE DATE; AND FOR OTHER PURPOSES.

* * * * *

WHEREAS, Florida Statutes § 163.3202 requires the City of Stuart to adopt, amend, and enforce land development regulations that are consistent with and serve to implement the City's Comprehensive Plan; and

WHEREAS, pursuant to the Florida Local Government Development Agreement Act, Florida Statutes § 163.3220 *et. seq.*, the City of Stuart has the authority to hold public hearings in order to enter into, amend or revoke a development agreement; and

WHEREAS, on August 13, 2025, an application was submitted to the City of Stuart by Michael Beneditti, Osceola Street Holdings, LLC, 400 SE Osceola Street, Suite 3, Stuart, FL 34994 (Applicant), for a Petition requesting approval of a rezoning from the existing R-3-

Residential Multi-Family/Office to a Commercial Planned Unit Development (CPUD) within the Community Redevelopment Area (CRA) Special District to add barbershop, beauty salons, and specialty salons as allowable uses on the property located 400 S.E. Osceola Street; and

WHEREAS, pursuant to Section 2.02.02. -Table 2: Land Uses of the City’s Land Development Code (LDC), barbershops, beauty salons & specialty salons are allowable uses in a CPUD; and

WHEREAS, Chapter XII of the City of Stuart’s LDC specifically defines a Barbershop, Beauty Salons, and Specialty Salons; and

WHEREAS, Barbershop means any place of business wherein the practice of barbering is carried on, as defined in F.S. § 473.034, amended from time to time; and

WHEREAS, Beauty Salon means any establishment where cosmetology services including hair care, nail care, and skin care on a regular basis for compensation; and

WHEREAS, Specialty Salon means any place of business wherein the practice of one or more of the following specialties are engaged in or carried on:

- (a) Manicuring, cutting, polishing, tinting, coloring, cleansing, adding, or extending of the nails, and massaging of the hands. This term includes any procedure or process for the affixing of artificial nails, except those nails which may be applied solely by use of a simple adhesive.
- (b) Pedicuring, shaping, polishing, tinting, or cleansing of the nails of the feet, and massaging or beautifying of the feet.
- (c) Facials, or the massaging or treating of the face or scalp with oils, creams, lotions, or other preparations, and skin care services; and

WHEREAS, on February 3, 2026, the Community Redevelopment Board (CRB) conducted a duly noticed public hearing during a regularly scheduled meeting to review the application requesting to rezone to a CPUD and subsequently forwarded a recommendation of *(APPROVE/DENY)* to the City Commission; and

WHEREAS, on XXXX, the City Commission held the first duly noticed public hearing during a regularly scheduled meeting to review the application requesting rezone to a CPUD and

voted to schedule the petition for the second and final public hearing before the City Commission for a final determination; and

WHEREAS, on XXXXX, the City Commission held the second duly noticed public hearing at a regularly scheduled meeting and voted to (*APPROVE / DENY*) the request to rezone to CPUD; and

WHEREAS, at the public hearing, and considering the CRB’s recommendation, all written and oral comments at the public hearings, the analysis by the Development Department, and the evidence presented by the parties at the hearings, the City Commission finds that the Applicant demonstrated through substantial competent evidence that the proposed development approval in the application is consistent with the City of Stuart Comprehensive Plan, Land Development Code, and procedural requirements; and

WHEREAS, the Applicant has committed to the City of Stuart that the development will adhere to all development codes, plans, standards, and conditions approved by the City Commission. Furthermore, the approvals granted shall be binding upon its successors in title regarding any commitments made at the time of approval of the Major CPUD Amendment.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA THAT:

SECTION 1: The foregoing recitals are incorporated as if fully set forth herein.

SECTION 2: The legal description of the +/-0.45-acre subject parcel is attached as “**Exhibit A.**” A map showing the subject parcel is attached as “**Exhibit B**” and incorporated herein by reference.

SECTION 3: The revised conditions of approval for Osceola CPUD are attached hereto as “**Exhibit C**” and made a part hereto by reference, and it shall constitute one of the Development Documents as described herein.

SECTION 4: The following documents on file as public records of the City of Stuart, at the office of the Development Department in City Hall, and attached hereto as **Exhibit "D,"** hereinafter the "Development Documents," shall be deemed a part of the development conditions applicable to the subject CPUD, and shall replace any earlier approvals:

1. **Project Narrative**

2. **Site Plan (*Sheet 1 of 1*)**: dated June 24, 2025 (last revised on September 25, 2025) and prepared by Joseph W. Capra (FL Lic. #376638) with Captec Engineering, Inc.
3. **Boundary Survey (*Sheet 1 of 1*)**: dated May 28, 2025 (field work revised on October 13, 2025), and prepared and electronically signed and sealed by Stephen J. Brown, P.S.M. (Lic. 4049) with Stephen J. Brown, Inc.
4. **Landscape, and Irrigation Plan (*Sheets L1.0, L2.0, L3.0, L4.0 & L5.0*)**: dated July 15, 2025 (Revised dated September 29, 2025), and prepared Plantvet Landscape Design Studio.
5. **Traffic Impact Analysis (*Sheets 1 through 3*)**: dated April 14, 2025, and prepared and electronically signed & sealed by Ayman As-Saidi, P.E. No. 56849, with Traffic & Mobility Consultants, LLC.
6. **Drainage Statement (*Sheet 1 through 1*)**: dated July 14, 2025, and prepared and electronically signed & sealed by Joseph W. Capra, P.E. (Lic. #37638) with Captec Engineering, Inc.
7. **Uniform Signage Plan (*Page 1*)**

SECTION 5: Except as otherwise provided herein, no development permits, site permits, or building permits shall be issued by the City of Stuart except in compliance with the aforesaid Development Documents. Failure by the Applicant, property owner, developer, or successor in interest to complete the development, or to comply with the Development Documents or with any term or condition of development outlined in this Ordinance shall be deemed a zoning violation and no further permits or other development approvals or orders shall be issued by the City until the violation has been resolved, or the matter may become the subject of a code enforcement action brought by the City. This section shall not impair the due process or other legal rights of the Applicant, property owner, developer, or its successor in interest to seek administrative or judicial redress.

SECTION 6: Following the adoption of this ordinance, and in addition to any other action for failure to complete development or otherwise comply with the Development Documents, the City Development Director may obtain a hearing before the City Commission and shall thereupon give at least five (5) days written notice of the time, date and location of the hearing, along with specific notice of the alleged breach. At the hearing before the City Commission, the Applicant, property owner, developer, or successor in interest may appear and may contest the allegation of breach or

explain the reason or reasons for the violation. Upon the finding of a material breach of the Development Documents and therefore, the Ordinance(s) adopting the same, the City Commission may impose or do any or all of the following:

- a. Initiate the process to amend or repeal this or any other Ordinance pertaining to the development.
- b. Direct the City Development Director to initiate the process to rezone the PUD property or any portion of the PUD property.
- c. Impose an administrative penalty of up to \$1,000.00 for each violation, and up to \$5,000.00 for each repeat violation that occurs, along with all reasonable costs, including attorney's fees incurred by the City.

Any breach of any provision or condition of this PUD ordinance by the Applicant or successor in interest shall be considered a zoning violation subject to any remedies provided herein, or as otherwise provided by law. In the event a violation continues from day to day, each day the violation is found to continue shall be deemed a separate violation.

SECTION 6: All Ordinances or parts of Ordinances in conflict with this Ordinance or any part thereof are hereby repealed to the extent of such conflict. If any provision of this Ordinance conflicts with any contractual provision between the City of Stuart and the Applicant, property owner, developer, or successor in interest of the site, this Ordinance shall prevail.

SECTION 7: If any provision of this ordinance or the application thereof to any person or circumstance is held invalid, the invalidity shall not affect other provisions or applications that can be given effect without the invalid provision or application, and to this end, the provisions of this Ordinance are declared severable.

SECTION 8: This enactment of this Ordinance shall be deemed to constitute a quasi-judicial action of the City and shall become effective upon the adoption by the City Commission and execution by the mayor.

Commissioner _____ offered the foregoing Ordinance and moved its adoption. The motion was seconded by Commissioner _____ and upon being put to a roll call vote, the vote was as follows:

CHRISTOPHER COLLINS, MAYOR
SEAN REED, VICE MAYOR

YES	NO	ABSENT	ABSTAIN

Ordinance 2549-2026 – Rezone to Osceola CPUD
400 S.E. Osceola Street

EULA R. CLARKE, COMMISSIONER
LAURA GIOBBI, COMMISSIONER
CAMBELL RICH, COMMISSIONER

ADOPTED this _____ day of _____, 2025.

ATTEST:

MARY R. KINDEL
CITY CLERK

CHRISTOPHER COLLINS
MAYOR

APPROVED AS TO FORM
AND CORRECTNESS:

LEE J. BAGGETT, ESQ.
CITY ATTORNEY

“EXHIBIT A” – LEGAL DESCRIPTION

LOTS 23, 24, AND 25, BLOCK 1, STYPMANN’S SUBDIVISION, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 5, PAGE 82, OF THE PUBLIC RECORDS OF PALM BEACH (NOW MARTIN) COUNTY, FLORIDA.

(ALSO RECORDED IN ORB 3024, PAGE 1889)

Parcel Area: 0.448-Acres

Parcel I.D. No.: 04-38-41-020-001-00240-0

Parcel Address: 400 S.E. Osceola Street, Stuart, FL 34997

“EXHIBIT B” – LOCATION MAP



“EXHIBIT C” – CONDITIONS OF APPROVAL

All conditions adopted under Ordinance No. 2549-2026 shall remain in effect unless revised or superseded by the conditions outlined below:

1. Any modifications to the Master CPUD Site Plan or the Final Site Plan for Stuart Landings II that exceeds 10% of the approved building footprint, building setbacks, gross square footage, building location, parking size, location and number of parking, drainage areas, and location of landscaping may be approved by the City’s Development Director, provided however, the Development Director may refer the matter to the City Commission for approval via a public hearing.
2. CPUD standards regarding circulation, parking, utilities, drainage, open space, and all other applicable standards shall apply as described in the LDR except as modified in this and any future ordinance.

Landscaping

3. All landscape areas shall be provided with an irrigation system of sufficient capacity to maintain the landscaping in a healthy growing condition.

Required Open Space and Native Preserve Area

4. Not less than 25% of the CPUD shall be open space as defined in the City Land Development Regulations. Required open space may include native vegetation areas and landscape buffers between the CPUD and adjacent property.
5. All development activities shall comply with the City’s Concurrency Management System before the issuance of building permits.
6. In the event of any conflict in the provisions of Exhibit ‘C’ Development Conditions and attached exhibits, Exhibit ‘C’ shall prevail.
7. Subject to the provisions of the LDR, the Developer shall comply with all requirements of the City Comprehensive Plan and of the Land Development Code, including but not limited to, Planned Unit Development regulations applicable to the property at time of permitting.

“EXHIBIT D” – DEVELOPMENT DOCUMENTS

The following "Development Documents" are on file as public records of the City of Stuart, at the office of the Development Department in City Hall, and shall be deemed a part of the conditions applicable to the Osceola CPUD, and shall replace any earlier approvals:

1. **Project Narrative**
2. **Site Plan (*Sheet 1 of 1*):** dated June 24, 2025 (last revised on September 25, 2025) and prepared by Joseph W. Capra (FL Lic. #376638) with Captec Engineering, Inc.
3. **Boundary Survey (*Sheet 1 of 1*):** dated May 28, 2025 (field work revised on October 13, 2025), and prepared and electronically signed and sealed by Stephen J. Brown, P.S.M. (Lic. 4049) with Stephen J. Brown, Inc.
4. **Landscape, and Irrigation Plan (*Sheets L1.0, L2.0, L3.0, L4.0 & L5.0*):** dated July 15, 2025 (Revised dated September 29, 2025), and prepared Plantvet Landscape Design Studio.
5. **Traffic Impact Analysis (*Sheets 1 through 3*):** dated April 14, 2025, and prepared and electronically signed & sealed by Ayman As-Saidi, P.E. No. 56849, with Traffic & Mobility Consultants, LLC.
6. **Drainage Statement (*Sheet 1 through 1*):** dated July 14, 2025, and prepared and electronically signed & sealed by Joseph W. Capra, P.E. (Lic. #37638) with Captec Engineering, Inc.
7. **Uniform Signage Plan (*Page 1*)**



TO: Community Redevelopment Board
FROM: Jodi Kugler, Development Director
MEETING DATE: Tuesday, February 3, 2026
SUBJECT: Osceola CPUD
Rezone to Commercial Planned Unit Development (*Project #: 54*)

AGENDA REQUEST:

McCarty & Associates Land Planning & Design, LLC is requesting on behalf of Commerce Avenue, LLC, the City Commission to grant a Rezone petition for a +/-1.45-acre parcel from M-2: Industrial (*County*) to IPUD: Industrial Planned Unit Development (*City*).

GENERAL INFORMATION:

Property Owner/Applicant: Michael Benedetti
Osceola Street Holdings LLC
400 E Osceola Street, Suite 3
Stuart, FL 34994

Location: 400 SE Osceola Street, Stuart, FL 34994

Parcel ID#: 04-38-41-020-001-00240-0

Parcel Size: +/-0.448-acres

Existing Future Land Use: Office/Residential

Proposed Future Land Use: Office/Residential

Existing Zoning: R-3 – Residential Multi-Family/Office

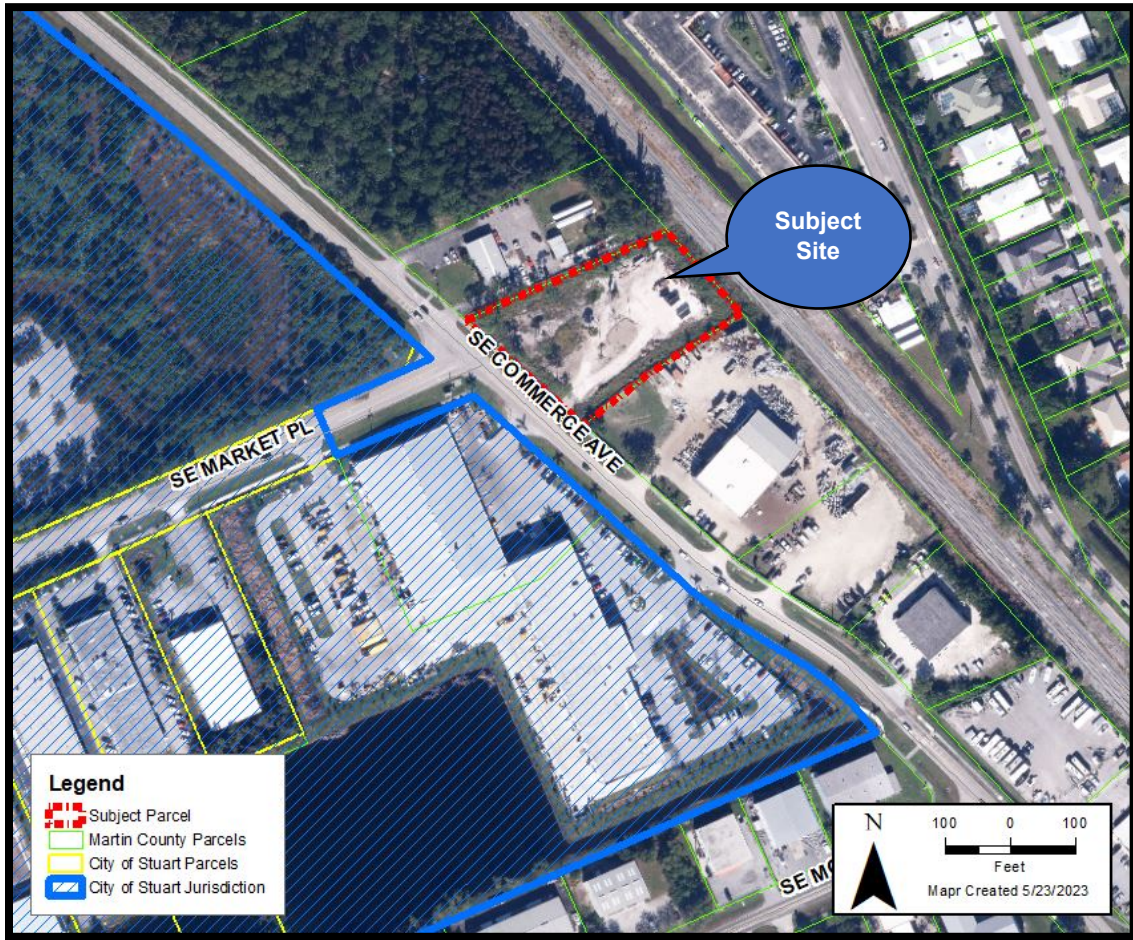
Proposed Zoning: CPUD – Commercial Planned Unit Development

Existing Use: Professional Services/Medical Offices

Proposed Use: Professional/Medical/Dental Offices & Specialty Salon Services

BACKGROUND

McCarty & Associates Land Planning & Design, LLC has concurrently submitted three (3) petitions, on behalf of Commerce Ave, LLC, (1) for a Voluntary Annexation to incorporate the +/-1.45-acres parcel from the jurisdiction of unincorporated Martin County to the City of Stuart, (2) a Small-Scale Comprehensive Plan Amendment to assign the Future Land Use category from Industrial (County) to Industrial (City), and subsequent Rezone petition from the M-2: Industrial (County) to IPUD: Industrial Planned Unit Development (City) zoning designation, for a 1.45-acre parcel of land situated on the east side of SE Commerce Avenue.



LAND USE AND ZONING

The subject site currently has a City of Stuart Future Land Use Designation of Office/Residential, and a City of Stuart Zoning Designation of Residential Multi-Family/Office. The subject property is occupied by professional and medical offices and located on the east side of SE Commerce Avenue. The site is located at the southeast corner of Osceola Street and Georgia Avenue. The existing uses, zoning districts, and future land use designations of the adjacent properties are summarized in Table 1 below.

Table 1. Existing Use, Zoning and Land-Use Designations

Location		Zoning District	Future Land Use	Existing Land Use
Subject Property		R-3 – Residential Multi-Family/Office	Office/Residential	Professional/Medical Offices
Adjacent Parcels	North	R-3 – Residential Multi-Family/Office & UW- Urban Waterfront	Office/Residential & Downtown Redevelopment	Professional/Medical Offices & Salon Services
	South	R-3 – Residential Multi-Family/Office	Office/Residential	Professional Office
	East	R-3 – Residential Multi-Family/Office	Office/Residential	Professional/Medical Offices
	West	CPUD – Commercial Planned Unit Development	Downtown Redevelopment	Professional Offices

REZONE TO THE OFFICIAL ZONING ATLAS
Standards of Review as Outlined in
Land Development Code Section 11.01.09

In accordance with LDC 11.01.09(G), the Community Redevelopment Board (CRB) shall hear from all interested parties regarding whether the application complies with the requirements of this Code and the Comprehensive Plan. The CRB shall consider the application, written comments of each responding department, consultant, and agency, as well as the compliance recommendation of the Development Department.

Specifically, the CRB and City Commission shall consider the following criteria:

a. The existing land use pattern

Staff Findings: The existing land use pattern surrounding subject site is consistent with the proposed uses within the surrounding properties.

b. The possible creation of an isolated land use classification unrelated to adjacent and nearby classifications.

Staff Findings: The land use designations within the surrounding area are predominantly designated by the Office/Residential and Downtown Redevelopment.

c. The population density pattern of the area and possible increase or overtaxing of the load on public facilities such as schools, utilities, and streets.

Staff Findings: The proposed Commercial Planned Unit Development zoning designation is consistent with adjacent professional offices, salon services and medical zoning and will not provide for an increase in residential density.

- d. The possible overloading of the city's sewage collection, treatment, and disposal facilities.**

Staff Findings: The site is currently served by the City's sewage collection, treatment and disposal facilities.

- e. The possible overloading of the city's drainage system.**

Staff Findings: The existing site is built out. Any future building additions, or major renovations to the site would be required to comply with the City's and other agency requirements.

- f. The existing classification boundaries in relation to existing conditions on the subject property.**

Staff Findings: The existing site does not result in any conflict boundaries or existing conditions on the subject property.

- g. The existence of changed or changing conditions which make the passage of the proposed amendment necessary or appropriate.**

Staff Findings: The proposed rezone from R-3 – Residential Multi-Family/to CPUD – Commercial Planned Unit Development is consistent with the current zoning, as well as the patterns of current uses and development within the surrounding area.

- h. The impact of the proposed amendment upon living conditions in the adjacent neighborhood.**

Staff Findings: The proposed type and intensity of development on the subject property is anticipated to be compatible with the surrounding area and will not result in any adverse impacts to the adjacent neighborhood.

- i. The impact of the amendment upon the flow of light and air to adjacent areas.**

Staff Findings: The type and intensity of development proposed for the subject property would not result in any adverse impact on the flow of light and air to adjacent properties.

- j. The impact of the proposed amendment upon property values in the adjacent area.**

Staff Findings: The proposed change in land use designation from commercial to residential would likely result in increased property values in surrounding area by establishing local customer base to support nearby commercial properties.

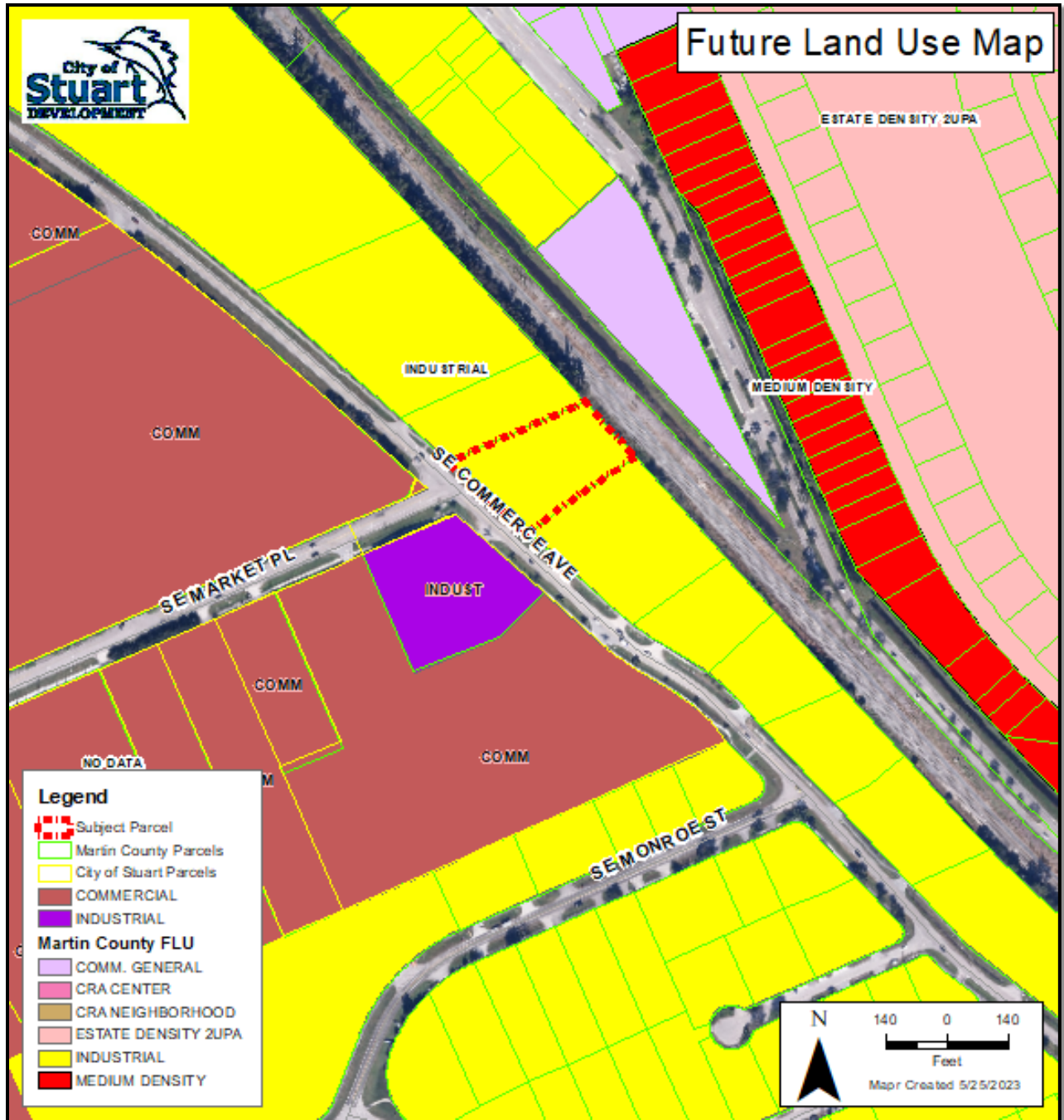
- k. The impact of the proposed amendment upon improvement or development of adjacent property in accordance with existing regulations; and**

Staff Findings: The proposed rezone petition is consistent with the surrounding commercial properties to the north, south, and west. Additionally, the proposed rezone is consistent with the Goals, Objectives, and Policies of the Comprehensive Plan.

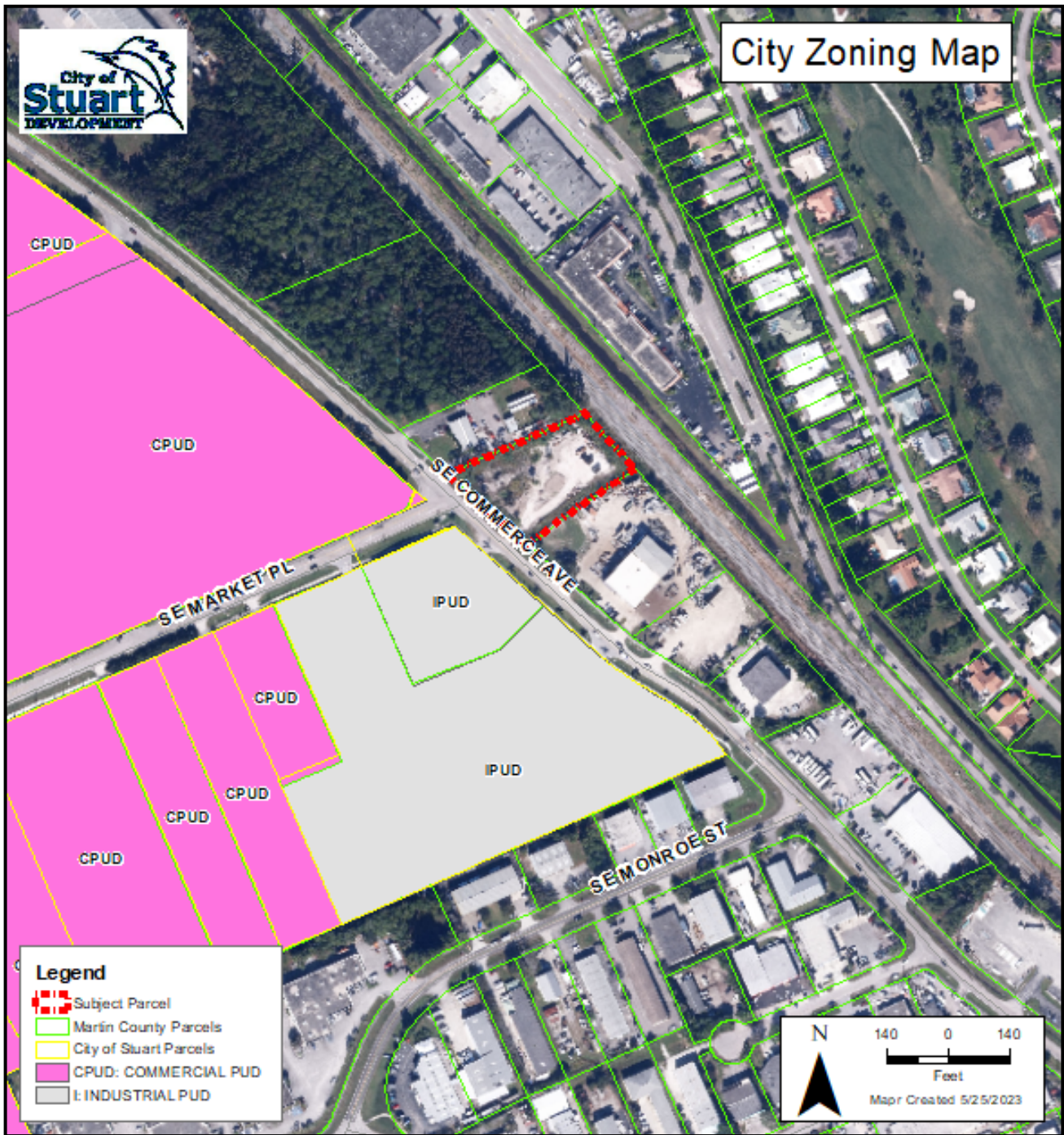
- i. The existence of other adequate sites in the city for the proposed land use classifications already permitting such use.**

Staff Findings: The purpose amendment is consistent with the adjacent site and the proposed land use classifications already permitting such use.

City of Stuart & Martin County Existing Future Land Use Map



City of Stuart Existing Zoning Map



Proposed CPUD Zoning Designation:

According to Land Development Code (LDC) Section 2.07.00(G), the proposed IPUD (*Industrial Planned Unit Development*) Zoning District is consistent with the proposed Industrial Future Land Use designation, as per LDC Section 2.01.03. Table 1. – *Zoning District/Future Land Use Category Compatibility Chart*.

Table 1 - Zoning District/Future Land Use Category Compatibility Chart

Future Land Use Categories	Implementing Zoning Categories		
	B-4 (Limited Business/Manufacturing)	I (Industrial)	CPUD, IPUD, MXPUD, UPUD
Industrial	X	X	X

Site Layout:

According to LDC Section 2.04.01 – Table 7, the Industrial Zoning Designation does not identify a minimum required front, rear, or side yard setback. (See Table 7 Below)

TABLE 7 - Public Service, Industrial and Hospital Area, Impervious Surface Coverage, Setback and Height Requirements:

Standard	Public Service	Industrial	Hospital
Minimum Lot Size			
Minimum Lot Area (ft²)	None	15,000 ft ²	None
Minimum Lot Width	Not Provided	100 ft.	Not Provided
Maximum Impervious Surface Coverage	65%	65%	Not Provided
Minimum Building Setback			
Minimum Front Yard	25 ft.	Not Provided	25 ft.
Minimum Side Yard	15 ft.	Not Provided	15 ft.
Minimum Rear Yard	25 ft.	Not Provided	25 ft. ¹
Maximum Building Height	45 ft.	45 ft.	45 ft. ¹
Maximum Stories	4	4	4

Footnotes:

- This provision is subject to amendment at such time as a master facilities plan is adopted for the hospital district for hospital use and development, thereupon all portions of the land development regulations in conflict therewith shall be repealed and superseded by the said master facilities plan.*

Osceola CPUD
Project # 54 – Rezone to Commercial Planned Unit Development

The proposed development plan identifies two (2) industrial buildings, with a total of eight (8) flex units and an overall building area of +/-21,445 ft² (Bldg. #1: 7,480 ft² & Bldg. #2: 13,965 ft²). According to the applicant, the units will be made available for lease to small local businesses.

The submitted site development plan identifies a minimum twenty (20) foot wide front yard setback, twenty-one (21) foot wide side yard setbacks, and thirty (30) foot wide rear yard setback for the proposed structures. As required with the proposed development, the applicant will be required to install landscape buffers within these front, side, and rear setbacks.

The two (2) proposed one-story industrial flex buildings will be constructed at twenty-four (24) feet for Building #1 and twenty-two (22) feet for Building #2, which is less than the maximum allowed building height of forty-five (45) feet within the Industrial Zoning District.

Additionally, the proposed industrial planned development identifies a maximum impervious surface coverage of 64.75%, which is consistent or less than the maximum impervious surface coverage of 65%, as required under the Industrial straight zoning designation.

The proposed development would provide a total floor area ratio (FAR) of 0.33, whereas the maximum FAR allowed outside of the Community Redevelopment Area (CRA) is <1.0 FAR, pursuant to LDC Section 2.03.10 – Table 4 (Intensity of Development Lot Area and Floor Area Ratio). (See table Below)

LDC Section 2.03.10 – Table 4: Intensity of Development Lot Area and Floor Area Ratio:

Future Land Use Categories	% Low Area Residential	% Lot Area Non-Residential	Non-Residential (Max. FAR)	>2.0 FAR
Industrial	N/A	100	3.0 FAR ¹ / 1.0 FAR ²	<10 acres ¹

Footnotes:

1 = Inside community redevelopment agency (CRA) boundary

2 = Outside CRA boundary

Proposed Site Data Calculations:

	Square Footage	Acres	Percentage %
Total Site Area	63,368	1.45	100%
Impervious Area	41,031	0.94	64.75%
Total Buildings	21,445	0.49	33.8%

Osceola CPUD
Project # 54 – Rezone to Commercial Planned Unit Development

Building 1	7,480	0.17	11.8%
Building 2	13,965	0.32	22%
Vehicular Use Area	16,226	0.37	25.6%
Concrete, Walks, Ramps, etc....	3,360	0.08	5.3%
Pervious Area	22,337	0.513	35.25%
Landscape Buffers/Open Space	22,337	0.513	35.25%

Proposed On-Site Parking Calculations:

Parking	Required Spaces	Provided Spaces
1 space per 1,000 ft ² Floor Area	21	21
ADA Accessible Spaces	1	2
Bicycle Parking	4	4
Loading Spaces	1	3

Traffic

The City’s Traffic Consultant, Kimley-Horn, has reviewed the traffic statement (dated April 14, 2025) prepared by Traffic & Mobility Consultants related to the above-mentioned project. The applicant is proposing to rezone a 0.45-acre parcel from its existing R-3 Residential – Multi-Family/Office (R-3) zoning designation to Commercial Planned Unit Development (CPUD). The property is located at the southeast corner of Osceola Street & Georgia Avenue in Stuart, Florida.

It is anticipated that the proposed trip generation potential for the CPUD zoning designation is as follows: 68 daily trips, 14 (4 entering/ 10 exiting) AM peak hour trips, and 14 (10 entering/ 4 exiting) PM peak hour trips. The existing trip generation potential for the R-3 zoning designation is as follows: 110 daily trips, 8 (2 entering/ 6 exiting) AM peak hour trips, and 10 (7 entering/ 3 exiting) PM peak hour trips. The proposed CPUD zoning designation shows a minimal increase in peak hour traffic generation compared to the existing R-3 zoning designation which will not adversely affect the surrounding transportation network

Fire & Police:

The site will be served by the City of Stuart Police Department and Fire District for emergency services.

Water Wastewater:

The existing water and wastewater continue to be provided by the City of Stuart Utilities. There are no proposed changes to the existing utilities.

Solid Waste & Recycling:

The trash enclosure is a requirement of the City of Stuart Land Development Code (LDC), which requires site plan review for compliance with the LDC standards outline under Section 6.08.01. The standards and requirements for Solid Waste & Recycling Containers apply to new development or new development.

Drainage:

Onsite Runoff: The existing site currently directs runoff to adjacent roadway inlets where it is collected and ultimately discharged to the St. Lucie River. There is no proposed impervious surface, and no additional water quality is required for the site. The site does not lie within a FEMA flood zone per FEMA Flood Map 1208

Vehicular & Pedestrian Access

The proposed project identifies single vehicular access to the site via on Commerce Avenue. Proposed access will allow for full right/left in and out of the site.

Pedestrian and ADA access is provided throughout the site via stabilized paved walkways and includes pedestrian and ADA access leading from the site to the adjacent sidewalk situated along east side of Commerce Avenue.

CONSISTENCY WITH THE CITY'S COMPREHENSIVE PLAN

The proposed Industrial Planned Unit Development (*IPUD*) Rezone Petition is consistent with the following Goals, Objectives, and Policies of the City of Stuart Comprehensive Plan.

In conjunction with the parent Voluntary Annexation application, the applicant has concurrently submitted a Small-Scale Comprehensive Plan Future Land Use Map Amendment and Rezone petition. The proposed Rezone petition seeks to amend the City's Official Zoning Atlas from M-2: Industrial (*County*) to IPUD: Industrial Planned Unit Development (*City*), pursuant to Comprehensive Plan Policy 1.A.7.5.

Objective 1.A.7: *The Future Land Use Map establishes the optimum, overall distribution of land uses as well as can be established on a City-wide basis. Neither these policies nor the Future Land Use Map shall be construed to mean that every parcel in a designated area is suitable for the designated use. Other plan objectives and policies will be applied to ensure that any proposed development will be suitable for the particular site. Similarly, the Future Land Use Map does not assure the landowner the right to the most intense use allowable under the designated future land use category. The City shall have the discretion to decide that the permitted land uses shall be more limited than the maximum allowable under the discretion to decide that development is consistent with the City's plan.*

Project # 54 – Rezone to Commercial Planned Unit Development

- **Policy 1.A7.1(E) – Industrial:** Land uses, and activities predominately connected with the sale, rental, and distribution of products or performance of professional and non-professional services. Includes retail sales, service establishments, business, professional and other offices, shopping centers, financial institutions, restaurants, entertainment, hotels, motels, and other temporary lodging. A limited amount of Multi-Family Residential and indoor industrial uses is allowed. Mixed-use projects are allowed. Also, public facilities and electric distribution substations are allowed.

Policy 1.A7.5. Properties that are annexed into the City shall be given a City land use designation as soon as possible after the annexation is finalized.

Policy 11.A1.1. The city will seek to ensure the allocation of an appropriate quantity of lands that are desirable for commercial and industrial purposes to serve future growth needs of the city.

Policy 11.A1.4. The city will encourage the designation and preservation of industrial land for industrial uses.

Policy 11.A1.5. The city will encourage new commercial and industrial development.

Proposed Future Land Use (City):

Objective 1.A7. - Future Land Use Categories.

- E. Industrial:** Land uses and activities predominantly connected with manufacturing, assembly, processing, packaging, research, or storage of products. Additional permitted uses in such areas include warehousing, wholesale activity, machine repair, and limited indoor retail sales and professional and other offices. Also, public facilities and electric distribution substations are allowed.

Policy 1.A7.2. Gross densities, gross intensities and proportional use amounts for each land use category are established in the "Table of Land Use Densities and Intensities" that is adopted as part of this element. In order to promote the efficient use of land and to provide an affordable housing stock (Policy 3.A2.6. Housing opportunities), the City's Land Development code shall provide adjustment, through the urban planned unit development process, to accommodate smaller residential units and allow smaller housing units to be calculated at 0.5 density per unit for units less than 900 sq. ft and 0.75 density per units for units less than 1,100 sq. ft.

Table of Land Use Densities and Intensities
(City of Stuart Comprehensive Plan – Future Land Use Element)

FLU	Residential		Non-Residential
			UPUD, RPUD, or Major UCE

Osceola CPUD
Project # 54 – Rezone to Commercial Planned Unit Development

	In/Out CRA	General	Not ALF	ALF	% Residential	General	> 2.0 FAR	% Non- Residential
Industrial	In	None	N/A	N/A	0%	< 3.0 FAR	< 10- acres	100%
	Out	None	N/A	N/A	0%	< 1.0 FAR	N/A	100%

LOCAL PLANNING AGENCY (LPA) RECOMMENDATION

On June 8, 2023, the Local Planning Agency (*LPA*) held a public hearing and voted to forward a recommendation to the City Commission for the approval of the Comprehensive Plan Future Land Use Map Amendment, to assign the Industrial Future Land Use designation within the City of Stuart’s jurisdiction.

PUBLIC NOTIFICATION

On June 12, 2023, the applicant provided notification letters to eight (8) owners of property located within 300 feet of the subject parcel, an advertisement was displayed in TC Palm Newspaper, and one (1) public notification sign detailing the City Commission Public Hearings was posted on the subject parcel.

STAFF RECOMMENDATION

The staff has reviewed the proposed petition for Rezone to the Official Atlas and PUD Development Plan and determined they conform to the Standards of Review outlined in Land Development Code Section 11.01.09 and are consistent with the Comprehensive Plan Goals, Objectives, and Policies. Staff recommends the City Commission grant approval of the proposed IPUD Rezone with associated development plan and uses, via Ordinance No. 2519-2023.

Project Narrative 400 SE Osceola St. "Osceola Salon"

This application is for consideration of a commercial PUD of my 4100sq ft single story building on the corner of Osceola Street and Georgia Street from R3 to allow "salon services" to be permitted in the building. This building is on the immediate edge of the UG zoning district and is currently zoned R3, although no residential aspect has existed for well over 20 years on my property or any adjacent property. The natural expansion of the Urban General district over the past decades has moved East of its current zoning and encompassed at least my entire block of antiquated R3 Zoning. The proposed PUD is to allow my parcel to reflect the natural development of the area. This rezoning has zero impact on parking, traffic, drainage, living conditions, flow of light, flow of air, adjacent properties, and adjacent uses (some adjacent uses are actual salon services). The land use pattern is consistent with the area and the decades of development that have occurred. The 2 houses on the block that were previously residential in the 1950s have long been converted into professional business establishments decades ago. There is no free standing residential element adjacent to my project. The adjacent uses are chiropractor, convenience store, lawyer, dental lab, salon, and contractor. A rezone of this property will improve the esthetic character through revitalizing the landscaping which focuses on removing invasive vegetation and incorporating drought resistant low water demand landscaping and hardscape. The plan for the interior of the building would make the interior of Suite 1 fully ADA compliant by removing a few interior walls, updating a bathroom to allow wheelchair access, and providing easier ADA mobility. None of the exterior building structure will be modified. I have plenty of existing parking to accommodate such a zoning change.



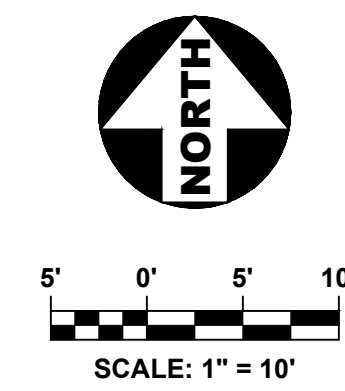
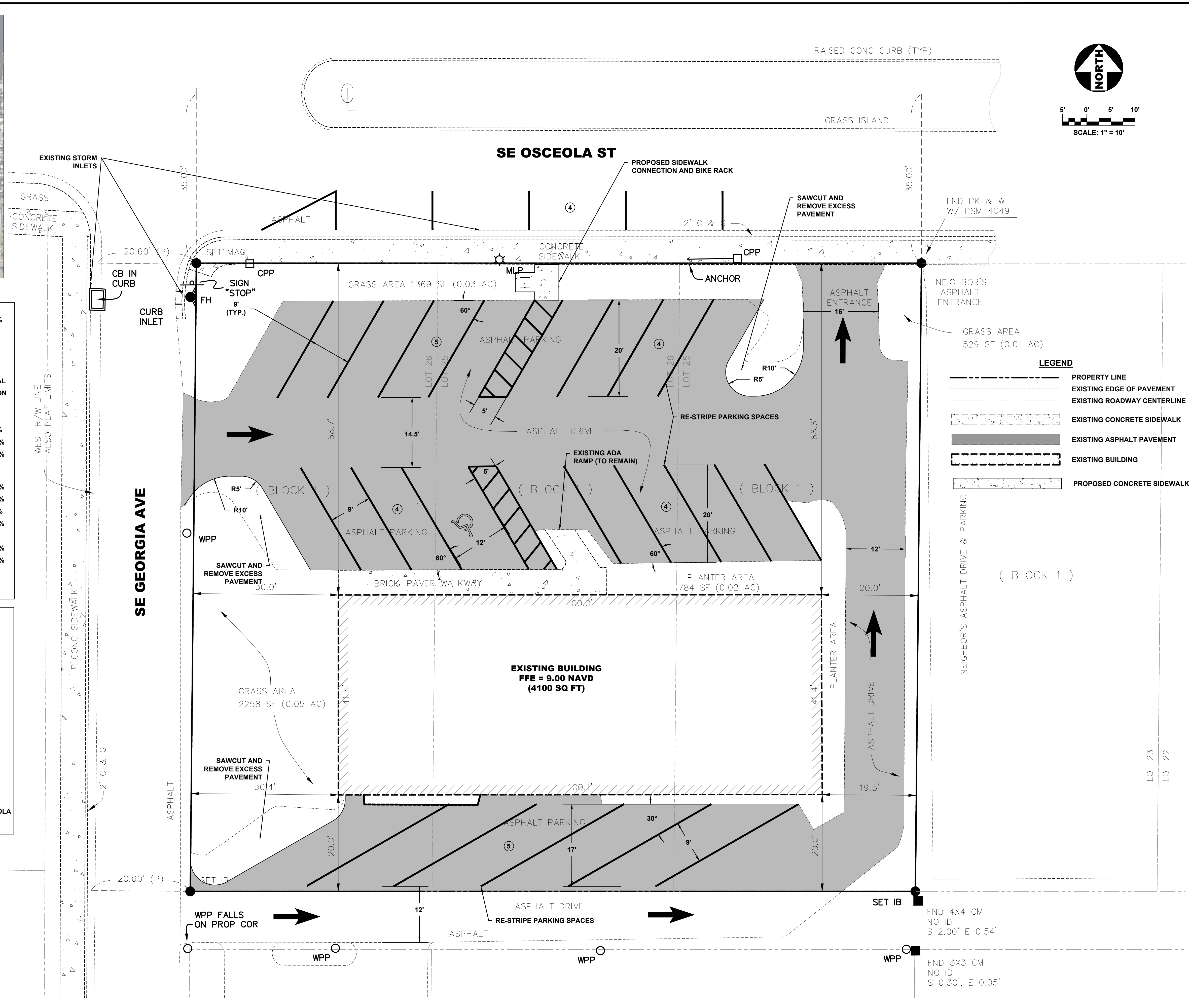
LOCATION MAP

SITE DATA			
TOTAL SITE AREA	19,515 SF	0.45 AC	100 %
EXISTING ZONING	R-3		
FUTURE LAND USE	OFFICE-RESIDENTIAL		
PARCEL CONTROL #	04-38-41-020-001-00240-0		
EXISTING USE	PROF SERV / MEDICAL OFFICES		
PROPOSED USE	PROFESSIONAL OFFICE, MEDICAL OFFICE - DENTAL, BEAUTY SALON		
PROPOSED ZONING CPUD			
OPEN SPACE			
TOTAL SITE	19,515 SF	0.45 AC	100 %
REQUIRED OPEN SPACE	4,879 SF	0.112 AC	25.0 %
PROVIDED OPEN SPACE	4,940 SF	0.113 AC	25.3 %
IMPERVIOUS AREA			
EXISTING BUILDING	4,100 SF	0.09 AC	21.0 %
PAVEMENT	10,080 SF	0.23 AC	51.7 %
SIDEWALKS	395 SF	0.01 AC	2.0 %
TOTAL	14,575 SF	0.34 AC	74.7 %
PERVIOUS AREA			
LANDSCAPE AREA	4,940 SF	0.113 AC	25.3 %
TOTAL	4,940 SF	0.113 AC	25.3 %
BUILDING DATA			
HEIGHT	1 - STORY (20')		

PARKING CALCULATIONS	
MEDICAL OFFICE - DENTAL (2100 SF)	REQUIRED SPACES = 1 SPACE PER 200 SF 2100 SF / 200 SF = 10.5 11 SPACES REQ'D
BEAUTY SALON (4 STATIONS)	REQUIRED SPACES = 2 SPACES PER STATION 2 x 4 STATIONS = 8 8 SPACES REQ'D
PROFESSIONAL OFFICE (UNOCCUPIED) (610 SF)	REQUIRED SPACES = 1 SPACE PER 300 SF 610 SF / 300 SF = 2.03 2 SPACES REQ'D
MEDICAL OFFICE (412 SE OSCEOLA ST)	REQUIRED SPACES = 3 SPACES*
TOTAL SPACES REQUIRED =	11 + 8 + 2 + 3 = 24 SPACES
TOTAL SPACES PROVIDED =	22 SPACES ON-SITE 4 SPACES OFF-SITE
*NOTE: THREE (3) PARKING SPACES LOCATED AT 400 SE OSCEOLA ST TO BE UTILIZED FOR THE REQUIRED PARKING AT 412 SE OSCEOLA ST UNDER PROJECT #Z11120001	

DRAINAGE & UTILITY STATEMENT
 THE EXISTING SITE CURRENTLY DIRECTS RUNOFF TO ADJACENT ROADWAY INLETS WHERE IT IS COLLECTED AND ULTIMATELY DISCHARGED TO THE ST. LUCIE RIVER. THERE IS NO PROPOSED IMPERVIOUS SURFACE AND NO ADDITIONAL WATER QUALITY IS REQUIRED FOR THE SITE.
 THE SITE DOES NOT LIE WITHIN A FEMA FLOOD ZONE PER FEMA FLOOD MAP 12085C0153H.
 EXISTING WATER AND SEWER TO CONTINUE TO BE PROVIDED BY CITY OF STUART UTILITIES. THERE ARE NO PROPOSED CHANGES TO THE EXISTING UTILITIES.

LEGAL DESCRIPTION
 LOTS 23, 24, AND 25, BLOCK 1, STYPMANN'S SUBDIVISION, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 5, PAGE 82, OF THE PUBLIC RECORDS OF PALM BEACH (NOW MARTIN) COUNTY, FLORIDA.



LEGEND

	PROPERTY LINE
	EXISTING EDGE OF PAVEMENT
	EXISTING ROADWAY CENTERLINE
	EXISTING CONCRETE SIDEWALK
	EXISTING ASPHALT PAVEMENT
	EXISTING BUILDING
	PROPOSED CONCRETE SIDEWALK

301 NW Flagler Ave
 Stuart, Florida 34994
 Phone: (772) 692-4344
 Fax: (772) 692-4341

CAPTEC
 Engineering, Inc.
 Civil Engineering Professionals

Engineering Business
 No. EB-007657

DATE: 08/24/23	
DRAWN BY:	TJS
DESIGNED BY:	TJS
CHECKED BY:	JWC
PROJECT NO.:	2132
HORIZ. SCALE:	1" = 10'
VERT. SCALE:	N/A
CADD FILE:	SITE PLAN

SCALE VERIFICATION	
1	SOLID BAR IS EQUAL ORIGINAL DRAWING DIMENSIONS ACCORDINGLY
2	9/25/23 TJS REV PER COS COMMENTS
1	7/14/25 JWC COS SUBMITTAL

400 SE OSCEOLA ST
 CITY OF STUART, FLORIDA

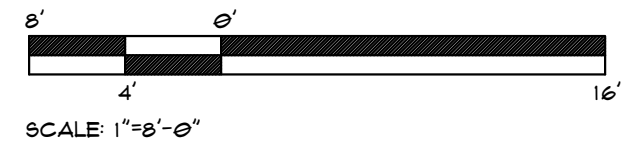
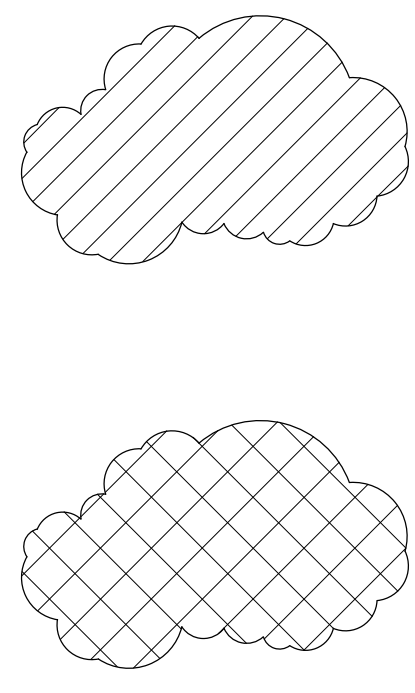
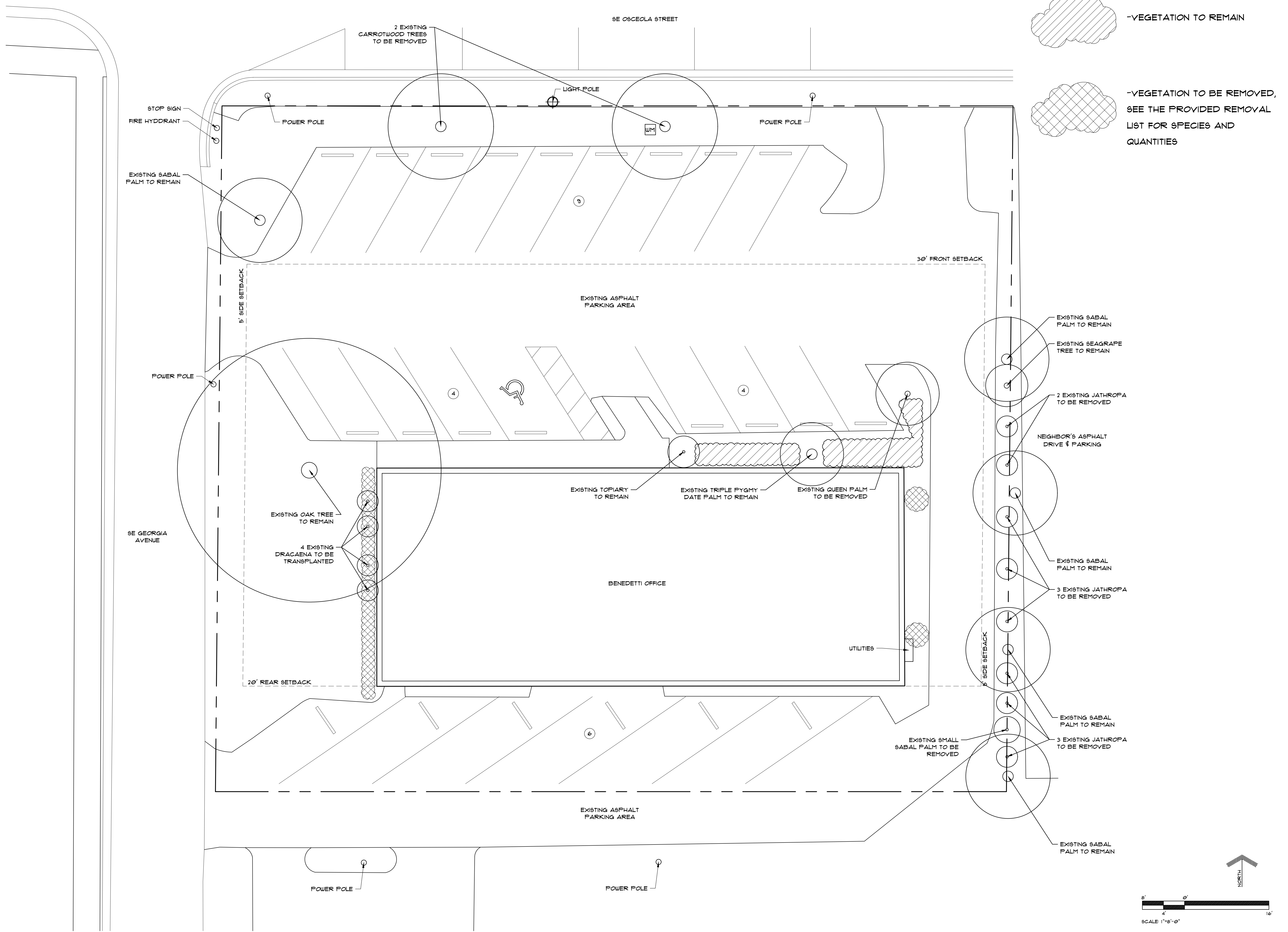
SITE PLAN

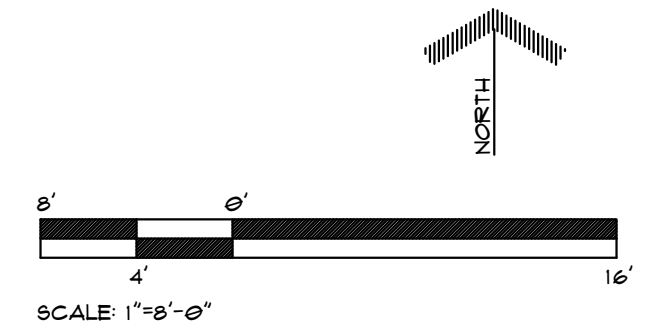
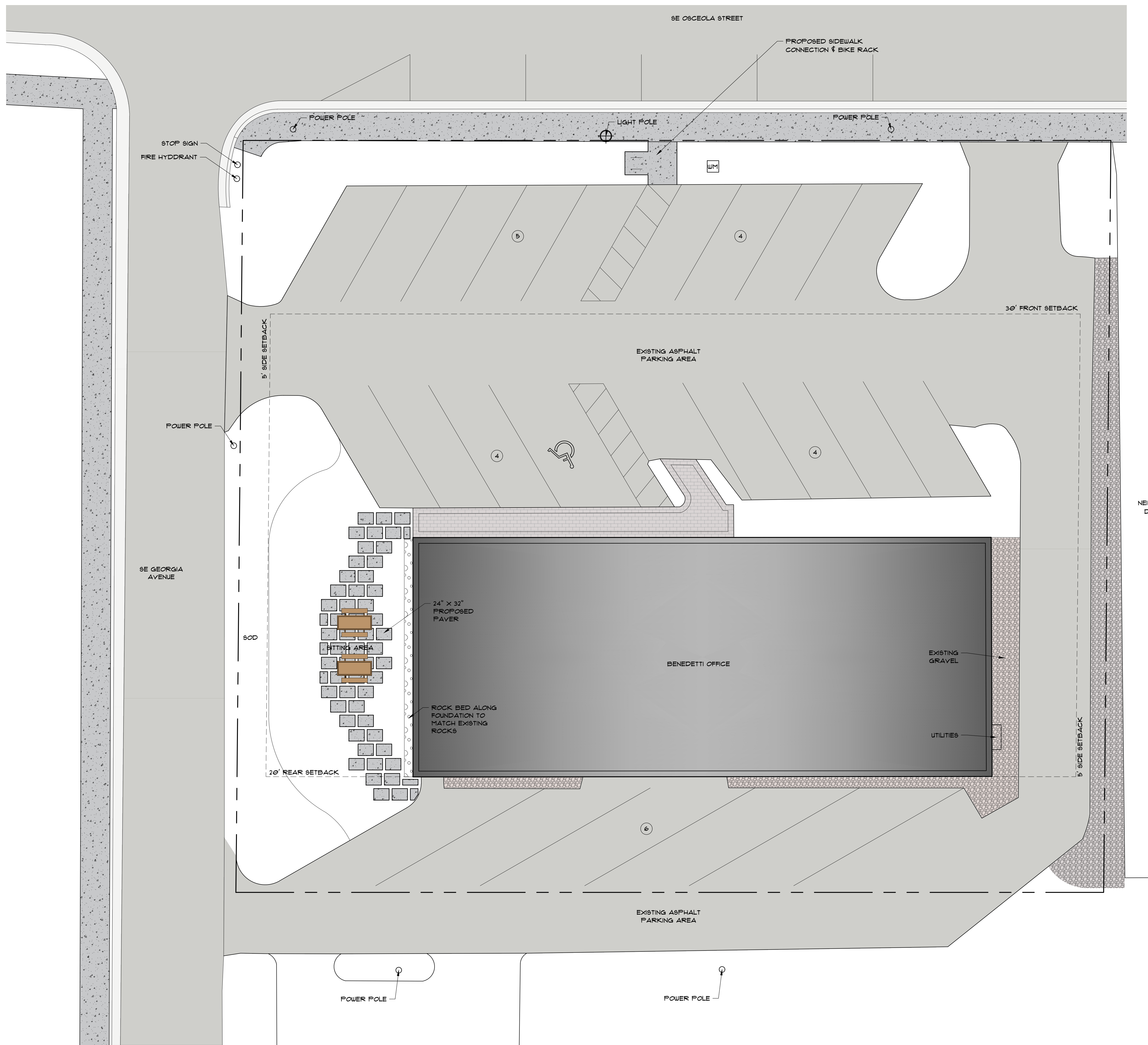
Joseph W. Capra
 301 N.W. Flagler Ave.
 Stuart, Florida 34994
 P.E. No. 37638

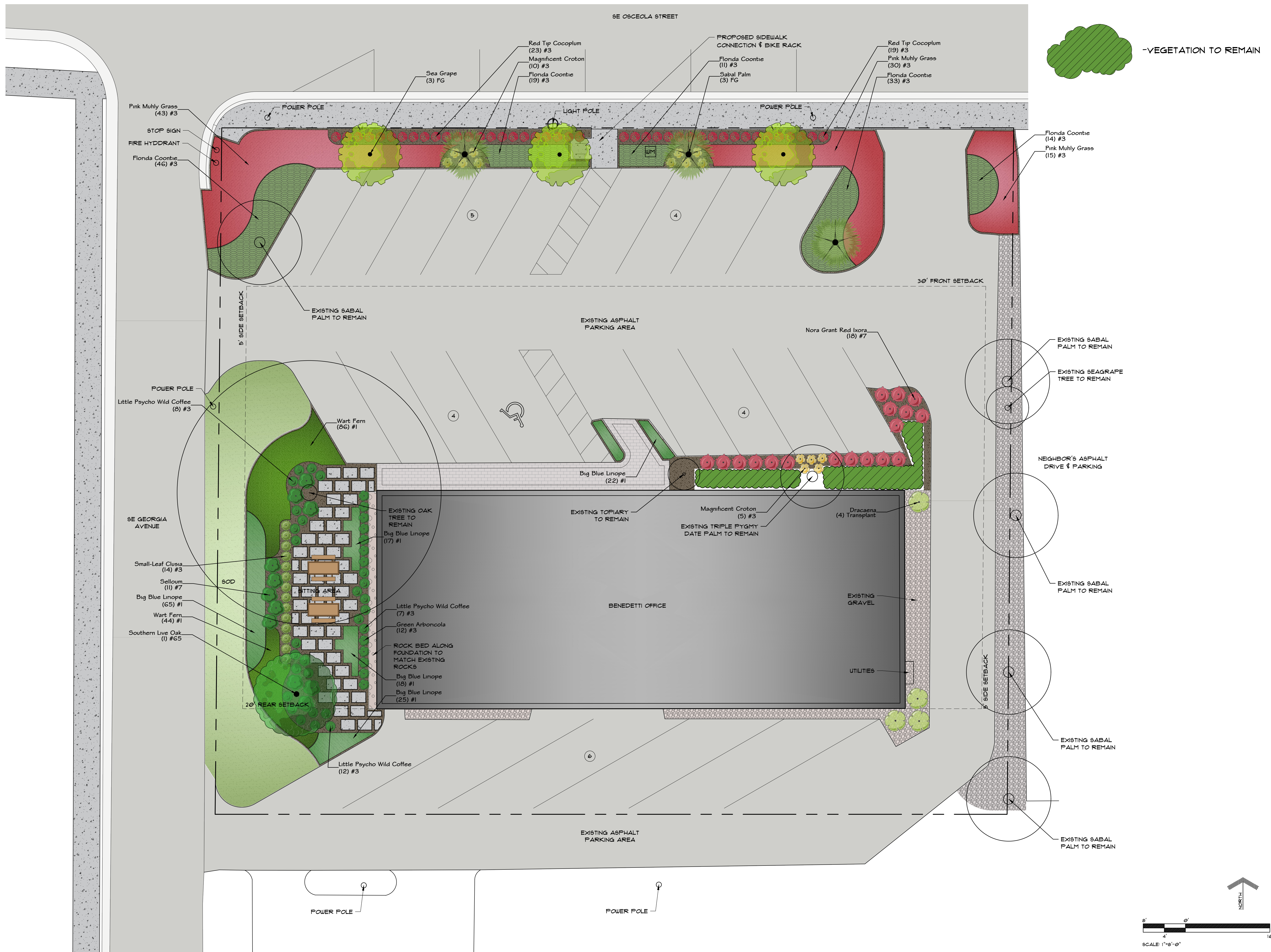
Printed Date: _____

JOB No.: **2132**
 SHEET
1 OF 1

THIS DOCUMENT, TOGETHER WITH THE CONCEPTS AND DESIGNS PRESENTED HEREIN, AS AN INSTRUMENT OF SERVICE, IS INTENDED ONLY FOR THE SPECIFIC PURPOSE AND CLIENT FOR WHICH IT WAS PREPARED. REUSE OF AND IMPROPER RELIANCE ON THIS DOCUMENT WITHOUT WRITTEN AUTHORIZATION AND ADOPTION BY CAPTEC ENGINEERING, INC. SHALL BE WITHOUT LIABILITY TO CAPTEC ENGINEERING, INC.







ENGINEER
CONTRACTOR SHOULD VERIFY ALL REQUIREMENTS AND QUANTITIES BEFORE WORK BEGINS. THE PLAN WAS CONSTRUCTED USING MULTIPLE SOURCES OF INFORMATION AND SHOULD BE CONSIDERED APPROPRIATE BEFORE IMPLEMENTATION. IT IS NOT GUARANTEED.
FOR BEST RESULTS OF THE PROJECT, USE THE NEAREST COMMON DIMENSION.

PLANT SCHEDULE PROPOSED

SYMBOL	QTY	BOTANICAL / COMMON NAME	CONTAINER	SIZE	NOTES	NATIVE	
TREES							
**	3	Coccoloba uvifera / Sea Grape	FG	12' O.A.	F, SP, Multi	Yes	
**	1	Quercus virginiana / Southern Live Oak	#65	12' x 7'	F, SP	Yes	
**	3	Sabal palmetto / Sabal Palm	FG	10'-16' CT	F, SP, HVY TRK	Yes	
SHRUBS							
**	42	Chrysobalanus icaco 'Red Tip' / Red Tip Cocoplum	#3	24' x 24"	F, SP	Yes	
**	14	Clusia guttifera / Small-Leaf Clusia	#3	24' x 18" sp.	F, SP	No	
**	15	Codiaeum variegatum 'Magnificent' / Magnificent Croton	#3	18' x 18"	F, SP	No	
**	4	Dracaena marginata / Dracaena	Transplant	5'-6' O.A.	Transplant	No	
**	18	Ixora coccinea 'Nora Grant' / Nora Grant Red Ixora	#7	36' x 36"	F, SP	No	
**	11	Philodendron selloum / Selloum	#7	36' x 36"	F, SP	No	
**	27	Psychotria nervosa 'Little Psycho' TM / Little Psycho Wild Coffee	#3	24' x 24"	Full Plants	Yes	
**	12	Schefflera arboricola 'Dwarf' / Green Arboricola	#3	18' O.A.	F, SP	No	
SYMBOL	QTY	BOTANICAL / COMMON NAME	CONTAINER	SIZE	NOTES	NATIVE	SPACING
SHRUB AREAS							
**	147	Liriope muscari 'Big Blue' / Big Blue Liriope	#1	12" OA	F, SP	No	15" o.c.
**	130	Microsorium scolopendria / Wart Fern	#1	12" OA	F, SP	No	18" o.c.
**	123	Zamia floridana / Florida Coontie	#3	20" OA	F, SP	Yes	24" o.c.
GRASSES							
**	88	Muhlenbergia capillaris / Pink Muhly Grass	#3	2' x 2'	F, SP	Yes	36" o.c.
GROUND COVERS							
	2,845 sf	Mulch / Chocolate Mulch	---				
**	923 sf	Paspalum notatum / Bahia grass	SOD			Free of Weeds and Pests	
	53 sf	Rock River Rock -- / Brown River Rock	---				

** - INDICATES PLANTS ON THE WATERWISE LIST



SEA GRAPE



SOUTHERN LIVE OAK



SABAL PALM



RED TIP COCOPLUM



CLUSIA



MAGNIFICENT CROTON



DRACAENA



NORA GRANT RED IXORA



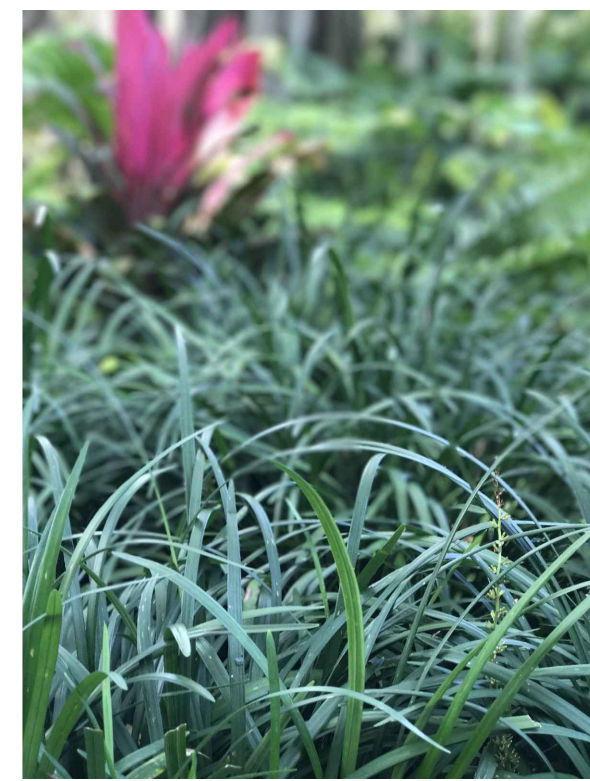
SELLOUM



LITTLE PSYCHO WILD COFFEE



GREEN ARBORICOLA



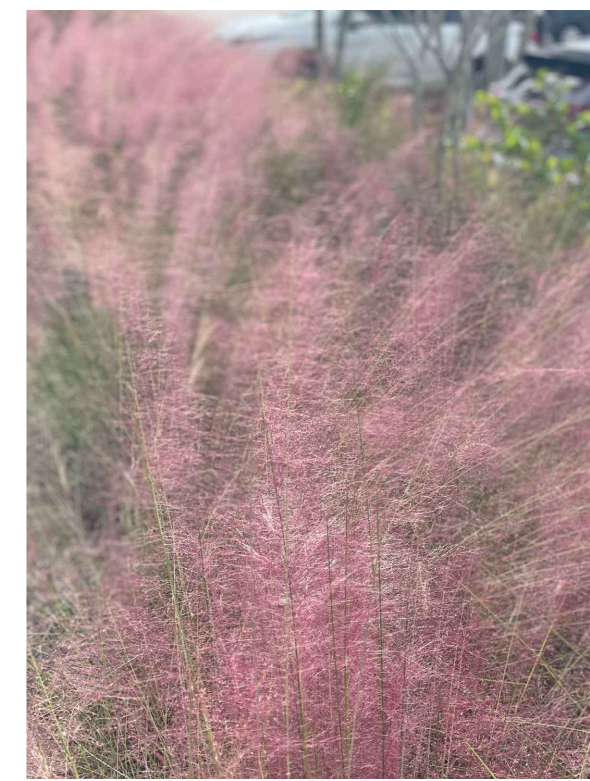
BIG BLUE LIRIOPE



WART FERN



FLORIDA COONTIE



PINK MUHLY GRASS

LANDSCAPE REQUIREMENTS TABLE					
	REQUIRED	EXISTING	PROVIDED	DROUGHT TOLERANT PERCENTAGE	POINTS
GRASSES			923 SF.	GRASSES ON WATERWISE LIST- 100%	10
TREES (1 PER 2500 SF. OF LOT AREA)	19,500 SQ. FT. = 8 TREES	0	7	13 OUT OF 13 TREES ON WATERWISE LIST- 100%	10
EXTRA SHADE TREES IN VUA		1	4	5 OUT OF 13 SHADE TREES IN VUA- 38%	5
SHRUBS			631	483 OUT OF 631 SHRUBS ON WATERWISE LIST- 77%	10
SOD ARE LESS THAN 50% OF LANDSCAPE AREA			923 SF.	19% OF SOD AREA	10
				3" OF COMPACTED MULCH IN PLANTING BEDS	10
					TOTAL POINTS- 55 POINTS

SITE DATA TABLE			
	TOTAL SITE AREA	IMPERVIOUS AREA	PERVIOUS AREA (LANDSCAPE AREA)
	19,515 SF	14,575 SF (74.7%)	4,940 SF (25.3%)



2003 NE CHRISTOPHER CT.
JENSEN BEACH, FL 34957
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LANDSCAPE PLANS
BENEDETTI OFFICE BUILDING
400 OSCEOLA STREET, STUART FL, 34994

EXPLANATION:
CONTRACTOR SHOULD VERIFY ALL REQUIREMENTS AND QUANTITIES BEFORE WORK BEGINS. THE PLAN WAS CONSTRUCTED USING MULTIPLE SOURCES OF INFORMATION AND SHOULD BE CONSIDERED APPROXIMATE BEFORE VERIFICATION. IT IS NOT GUARANTEED.
FOR THE PURPOSES OF THIS PLAN, THE PLAN WAS CONSTRUCTED USING MULTIPLE SOURCES OF INFORMATION AND SHOULD BE CONSIDERED APPROXIMATE BEFORE VERIFICATION. IT IS NOT GUARANTEED.

SHEET
L4.0
PLANT LIST/
PLANT PHOTOS/
SITE DATA TABLE

DRAWN BY: RJM
DESIGNED BY: RJM
CHECKED BY: RJM

DATE: 1/15/25
REVISED: 5/25/25

SCALE: -

LANDSCAPE SPECIFICATIONS

GENERAL CONDITIONS:

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH PLANS AND SPECIFICATIONS PREPARED BY PLANTVET, AND PER DIRECTION ON-SITE BY THE LANDSCAPE DESIGNER.

REQUIREMENTS OF THE CONTRACT INCLUDE: FURNISHING ALL LABOR, MATERIALS, SUPPLIES, EQUIPMENT, ETC. NECESSARY TO FULLY COMPLETE THE INSTALLATION OF WORK AS SHOWN ON THE DRAWINGS, SPECIFICATIONS AND PER THE LANDSCAPE DESIGNER'S DIRECTION.

THE LANDSCAPE CONTRACTOR SHALL PROVIDE A UNIT PRICE BID WITH A CONTRACT TOTAL UNIT PRICES WILL BE USED FOR ADDITIONS AND DELETIONS TO THE PLANS. SALES TAX, IF ANY, SHALL BE SHOWN.

JOB CONDITIONS:

THE LANDSCAPE CONTRACTOR SHALL CONTACT THE G.C. AND/OR AN UNDERGROUND UTILITY LOCATING COMPANY AND ARRANGE FOR ALL UTILITIES IN THE WORK AREA TO BE LOCATED. CARE SHALL BE TAKEN NOT TO DISTURB OR DAMAGE ANY UNDERGROUND CONSTRUCTION OR UTILITIES. ANY DAMAGE TO THESE FACILITIES DURING THE PLANTING OPERATIONS WILL BE REPAIRED AT THE EXPENSE OF THE LANDSCAPE CONTRACTOR IN A MANNER APPROVED BY THE OWNER. WHERE UNDERGROUND OBSTRUCTIONS WILL NOT PERMIT THE INSTALLATION OF MATERIALS IN ACCORDANCE WITH THE PLANS, NEW LOCATIONS SHALL BE APPROVED BY THE LANDSCAPE DESIGNER.

THE LANDSCAPE CONTRACTOR SHALL LAY OUT THE PLANTS ACCORDING TO THE LANDSCAPE PLANS. IF A CONFLICT ARISES AS TO THE LOCATION, SPACING, ETC., CONTACT THE LANDSCAPE DESIGNER IMMEDIATELY.

THE LANDSCAPE CONTRACTOR SHALL EXAMINE THE SITE AND FULLY ACQUAINT HIMSELF WITH ALL OF THE EXISTING CONDITIONS SO NO MISUNDERSTANDINGS MAY ARISE AS TO THE CHARACTER OR THE EXTENT OF THE WORK TO BE DONE. THE LANDSCAPE CONTRACTOR SHALL ALSO ACQUAINT HIMSELF WITH ALL PRECAUTIONS TO BE TAKEN TO AVOID INJURY TO PERSONS OR PROPERTY OF ANOTHER. NO ADDITIONAL COMPENSATION WILL BE GRANTED BECAUSE OF ANY UNUSUAL DIFFICULTIES THAT MAY BE ENCOUNTERED IN THE EXECUTION OR MAINTENANCE OF ANY PORTION OF THE WORK.

SITE PREPARATION:

PRIOR TO ANY PLANTING, THE LANDSCAPE CONTRACTOR SHALL PREPARE THE PLANTING AREAS AND COMPLETELY REMOVE ALL SOIL OR WEEDS FROM THESE AREAS.

ANY BUILDING CONSTRUCTION MATERIAL OR FOREIGN MATERIAL SUCH AS BOULDERS, TRASH, ROOTS, CONCENTRATIONS OF GRAVEL, ETC. ENCOUNTERED DURING SITE PREP OR ANY PHASE OF THE WORK, SHALL BE REMOVED FROM THE SITE AND, IF NECESSARY, REPLACED WITH ACCEPTABLE PLANTING SOIL.

THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR THE FINAL GRADING (+/- 2") DURING THE COURSE OF SITE PREPARATION SO AS TO BRING THE SOIL AND PLANTING BEDS TO THEIR PROPER ELEVATIONS AND TO ASSURE POSITIVE DRAINAGE.

FINAL SCULPTING OF BERMS, WHETHER BY MACHINE OR BY HAND, SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR. THE WORK SHALL BE PERFORMED UNDER THE DIRECTION OF THE LANDSCAPE DESIGNER.

MATERIALS LIST:

ALL QUANTITIES FOR MATERIALS SHOWN ON THE DRAWINGS SHALL BE VERIFIED BY THE LANDSCAPE CONTRACTOR PRIOR TO SUBMITTING HIS BID. QUANTITIES AND SPECIFICATIONS SHOWN ON THE DRAWINGS SHALL TAKE PRECEDENCE OVER THE PLANT LIST.

PLANT MATERIAL:

PLANT SPECIES AND SIZE SHALL CONFORM TO THOSE INDICATED ON THE DRAWINGS. NOMENCLATURE SHALL CONFORM TO STANDARDIZED PLANT NAMES, 1942 EDITION. ALL PLANT MATERIAL SHALL BE IN ACCORDANCE WITH GRADES AND STANDARDS FOR NURSERY PLANTS, PARTS I & II, LATEST EDITION PUBLISHED BY THE FLORIDA DEPARTMENT OF AGRICULTURE AND CONSUMER SERVICES. ALL PLANTS NOT OTHERWISE SPECIFIED AS FLORIDA FANCY OR SPECIMEN, SHALL BE FLORIDA GRADE NUMBER 1 OR BETTER AS DETERMINED BY THE DOCUMENTS LISTED ABOVE. SPECIMEN MEANS AN EXCEPTIONALLY HEAVY, SYMMETRICAL, TIGHTLY-KNIGHT PLANT, SO TRAINED OR FAYOURED IN ITS DEVELOPMENT THAT ITS APPEARANCE IS UNQUESTIONABLY AND OUTSTANDINGLY SUPERIOR IN FORM, NUMBER OF BRANCHES, COMPACTNESS, AND SYMMETRY. ALL PLANTS SHALL BE HARDENED OFF SOUND, HEALTHY, VIGOROUS, WELL-BRANCHED, AND FREE OF DISEASE, INSECTS, INSECT EGGS AND LARVAE, AND SHALL HAVE VIGOROUS ROOT SYSTEMS. MATCHED PLANTS OR TREES FOR PLANTING IN ROWS SHALL BE UNIFORM IN SIZE AND SHAPE. ALL MATERIALS SHALL BE SUBJECT TO APPROVAL BY THE LANDSCAPE DESIGNER. WHERE ANY REQUIREMENTS ARE OMITTED FROM THE PLANT LIST, THE PLANTS FURNISHED SHALL BE NORMAL FOR THE VARIETY.

ALL CONTAINER GROWN MATERIAL SHALL BE HEALTHY, VIGOROUS, WELL-ROOTED PLANTS AND ESTABLISHED IN THE CONTAINER. THE PLANTS SHALL HAVE FOLIAGE AND BRANCHES WHICH ARE GOOD QUALITY AND IN A HEALTHY GROWING CONDITION. ESTABLISHED CONTAINER GROWN PLANTS TRANSPLANTED INTO ANOTHER CONTAINER SHALL BE GROWN IN THE SECOND CONTAINER LONG ENOUGH FOR THE NEW FIBROUS ROOTS TO HAVE DEVELOPED TO HOLD THE ROOT MASS TOGETHER WHEN REMOVED FROM THE CONTAINER. ROOT-BOUND PLANTS WILL NOT BE ACCEPTED.

MEASUREMENTS:

CANOPY TREES: HEIGHT SHALL BE MEASURED FROM THE GROUND TO THE AVERAGE HEIGHT OF CANOPY. SPREAD SHALL BE MEASURED TO THE END OF BRANCHINGS EQUALLY AROUND THE CROWN ACROSS THE CENTER OF THE TRUNK.

SHRUBS: HEIGHT SHALL BE MEASURED FROM THE GROUND. SPREAD SHALL BE MEASURED TO THE END OF THE BRANCHING EQUALLY AROUND THE SHRUB MASS.

PALMS: GRAY WOOD (G.W.) SHALL BE MEASURED FROM THE GROUND TO THE POINT WHERE THE SMOOTH AGED TRUNK JOINS ANY REMAINING PETIOLES.

OVERALL HEIGHT (O.A.) SHALL BE MEASURED FROM THE GROUND TO THE TIP OF THE UNOPENED BUD.

CLEAR TRUNK (C.T.) SHALL BE MEASURED FROM THE GROUND TO THE BASE OF THE LOWEST GREEN FRONDS.

QUANTITIES, LOCATIONS, AND SUBSTITUTIONS:
NO SUBSTITUTIONS SHALL BE MADE WITHOUT APPROVAL FROM THE LANDSCAPE DESIGNER AND THE OWNER. THE LANDSCAPE DESIGNER RESERVES THE RIGHT TO ADJUST THE NUMBER AND LOCATIONS OF PLANTS. THE OWNER SHALL RECEIVE A CREDIT OR DEBIT BASED ON THE UNIT PRICE OF THE PLANT MATERIAL DELETED OR ADDED.

TRANSPLANTS:

ALL TRANSPLANTS WITH A 2" CALIPER TRUNK OR GREATER ARE TO BE ROOT-PRUNED A MINIMUM OF 6 WEEKS BEFORE MOVING.

FERTILIZER:

ALL TREES (EXCLUDING PALM TREES), SHRUBS AND GROUND COVERS TO BE FERTILIZED WITH 'MILORGANITE' OR AN APPROVED COMPLETE FERTILIZER AT THE TIME OF PLANTING. ALL PALM TREES SHALL BE FERTILIZED WITH A 100% CONTROLLED RELEASE PALM SPECIAL FERTILIZER WITH MICRO NUTRIENTS MANUFACTURED BY LESCO, ORNOCOTE OR FLORIKAN OR APPROVED EQUAL APPLIED AT THE MANUFACTURER'S RECOMMENDED RATES. UNIT PRICES OF PLANTS ARE TO INCLUDE THIS COST. APPLY 'MILORGANITE' AT THE FOLLOWING RATES:

- 3.00 LBS. OR 8.10 CUPS PER 12'-16" PLANT
- 2.00 LBS. OR 5.40 CUPS PER 8'-12" PLANT
- 0.10 LBS. OR 2.00 CUPS PER 6'-8" PLANT
- 0.20 LBS. OR 1/2 CUP PER 3 GALLON PLANT
- 0.10 LBS. OR 1/4 CUP PER 1 GALLON PLANT

IRRIGATION:

ALL PLANT MATERIAL AND SOIL WILL HAVE 100% AUTOMATIC IRRIGATION COVERAGE WITH A RAIN SENSOR.

STAKING AND GUYING:

THE LANDSCAPE DESIGNER RESERVES THE RIGHT TO REQUEST ANY OR ALL TREES AND PALMS TO BE STAKED.

ALL HIPICUS TREES SHALL BE STAKED AT ALL TIMES. STAKE THEM WITH A MIN. OF 1" #5 (5/8") STEEL REBAR, 1' LONG AND PLACED A MIN. 3" INTO THE GROUND. THE REBAR SHALL BE PLACED SINGLY AGAINST THE TREE TRUNK AND TIED WITH 3 WRAPS OF 3/4" WIDE GREEN PLASTIC NURSERY TIE TAPE IN 3-4 LOCATIONS ALONG THE TRUNK.

TREES SHALL BE GUYED IN THREE DIRECTIONS WITH 2 TWISTED STRANDS OF NO. 12 GALVANIZED WIRE ATTACHED TO SECURE BELOW GRADE ANCHORS. WIRES IN CONTACT WITH THE TREE SHALL BE ENCASED IN RUBBER. LARGE TREES AND PALMS SHALL BE BANNED AND STAKED WITH CYPRESS POLES OR CLEAN LUMBER AND ANCHORED SECURELY. NO NAILS ARE TO BE DRIVEN INTO TREES OR PALMS. UNLESS DIRECTED, STAKING AND GUYING SHALL BE LEFT TO THE DISCRETION OF THE LANDSCAPE CONTRACTOR, HOWEVER THIS DOES NOT RELIEVE HIM OF THE RESPONSIBILITY OF RE-SETTING OR REPLACING TREES WHICH HAVE BLOWN OVER.

ESPALET PLANTS:

ONLY LEAD OR PLASTIC ANCHORS WITH ROUND HEAD STAINLESS STEEL SCREWS OF APPROPRIATE SIZE SHALL BE USED BY THE LANDSCAPE CONTRACTOR TO ESPALLET PLANTS. PLANTS SHALL BE ATTACHED TO THE STAINLESS STEEL SCREWS WITH BLACK PLASTIC COATED SOLID COPPER #14 ELECTRICAL WIRE. THE LANDSCAPE DESIGNER WILL DIRECT THE SHAPE OF THE ESPALLETED PLANTS.

PLANTING SOIL:

ALL PLANTING PITS TO BE BACKFILLED WITH A PLANTING MIX OF 50% SAND, 25% HUMUS, AND 25% TOPSOIL. GARDENIAS AND AZALEAS SHALL HAVE A PLANT PIT 3 TIMES THE DIAMETER OF THE ROOT BALL AND SHALL BE BACKFILLED WITH A PLANTING MIX OF 50% SAND AND 50% HUMUS.

SOIL:

SOIL TO BE WEED-FREE ST. AUGUSTINE FLORITAM SOLID SOIL UNLESS OTHERWISE SPECIFIED. LOOSE SUBGRADE BY HAND RAKING TO A DEPTH OF 4" BEFORE LAYING SOIL. SOIL TO BE LAID WITH TIGHT JOINTS, NOT OVERLAPPING.

MULCH:

ALL PLANT BEDS TO BE MULCHED WITH A 2" LAYER OF PINE BARK, MIXED HARDWOOD, OR EUCALYPTUS MULCH UNLESS OTHERWISE SPECIFIED.

GUARANTEE:

ALL NEW PLANT MATERIALS SHALL BE GUARANTEED FROM THE TIME OF ACCEPTANCE AND SHALL BE ALIVE AND IN SATISFACTORY GROWTH FOR EACH SPECIFIC KIND OF PLANT AT THE END OF THE GUARANTEE PERIOD. PALMS ARE TO BE GUARANTEED FOR 6 MONTHS, OTHER PLANTS ARE TO BE GUARANTEED FOR 90 DAYS. REPLACEMENT SHALL OCCUR WITHIN TWO WEEKS OF REJECTION AND RE-GUARANTEE FROM DATE OF INSTALLATION. LANDSCAPE CONTRACTOR SHALL REPAIR DAMAGE TO PLANTS, LAWNS, UTILITIES, ETC., CAUSED BY PLANT REPLACEMENT WORK AT NO ADDITIONAL COST. THE LANDSCAPE CONTRACTOR SHALL NOT BE RESPONSIBLE FOR DAMAGE CAUSED BY VANDALISM, WIND STORMS, FLOODING, OR OTHER ACTS OF GOD BEYOND CONTROL.

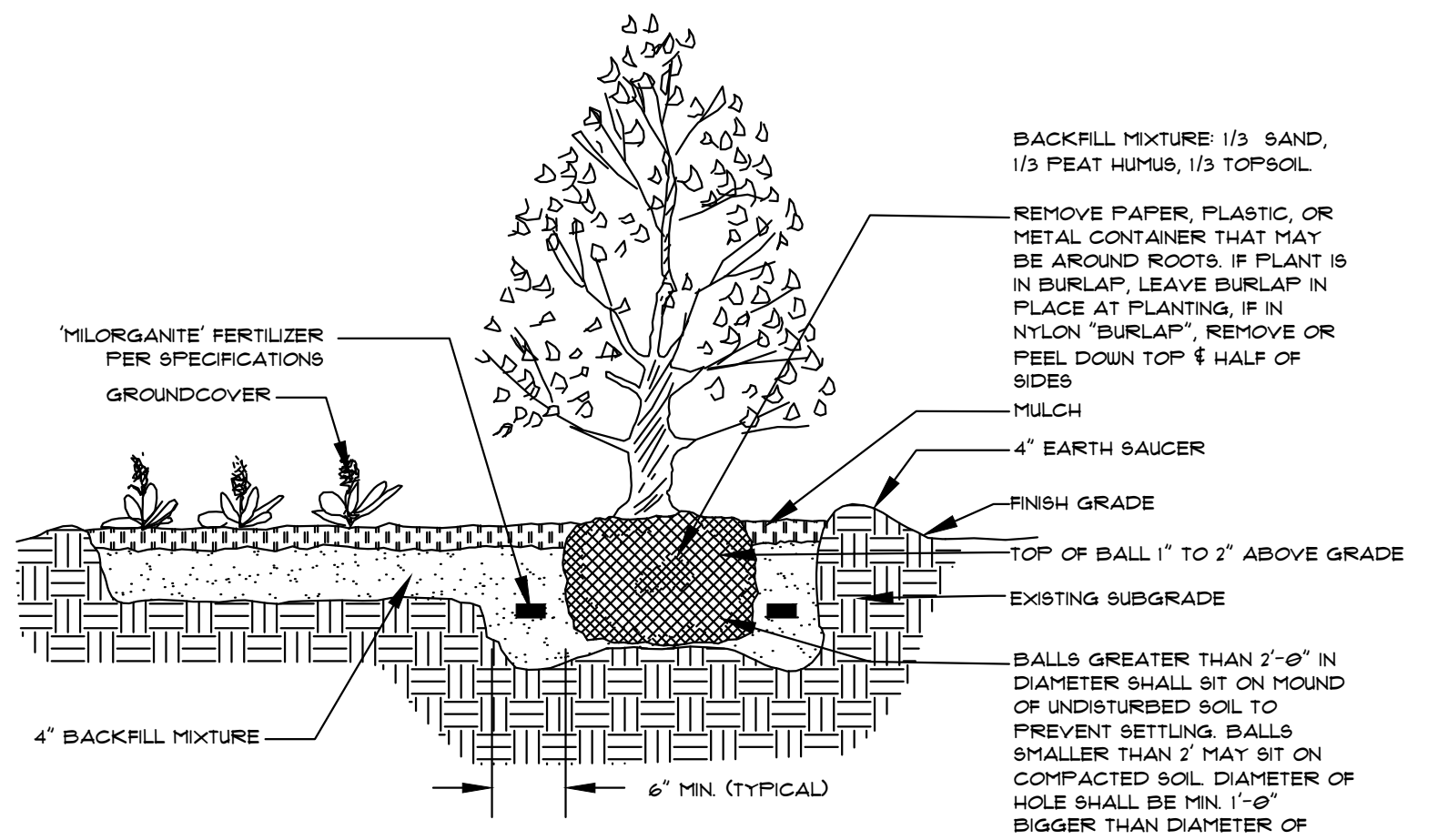
MAINTENANCE:

NEWLY INSTALLED PLANTS AND PLANT BEDS ARE TO BE KEPT WATERED, WEED FREE AND INSECT FREE UNTIL THE LANDSCAPE CONTRACTOR IS COMPLETE.

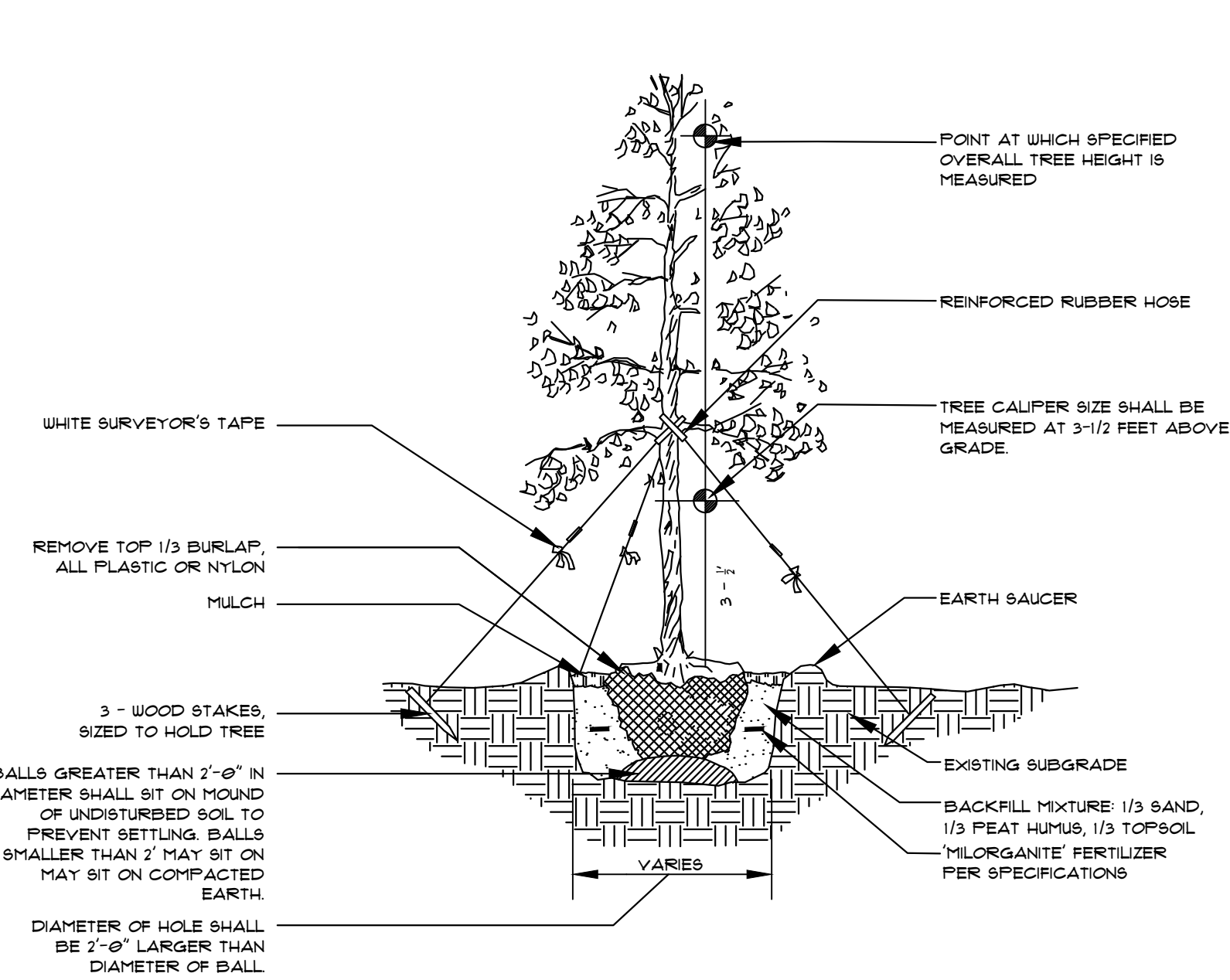
ALL SABAL PALMS WITH TIED HEADS ARE TO BE OPENED AND TRIMMED 6-8 WEEKS AFTER INSTALLATION AT NO ADDITIONAL COST.

CLEANUP:

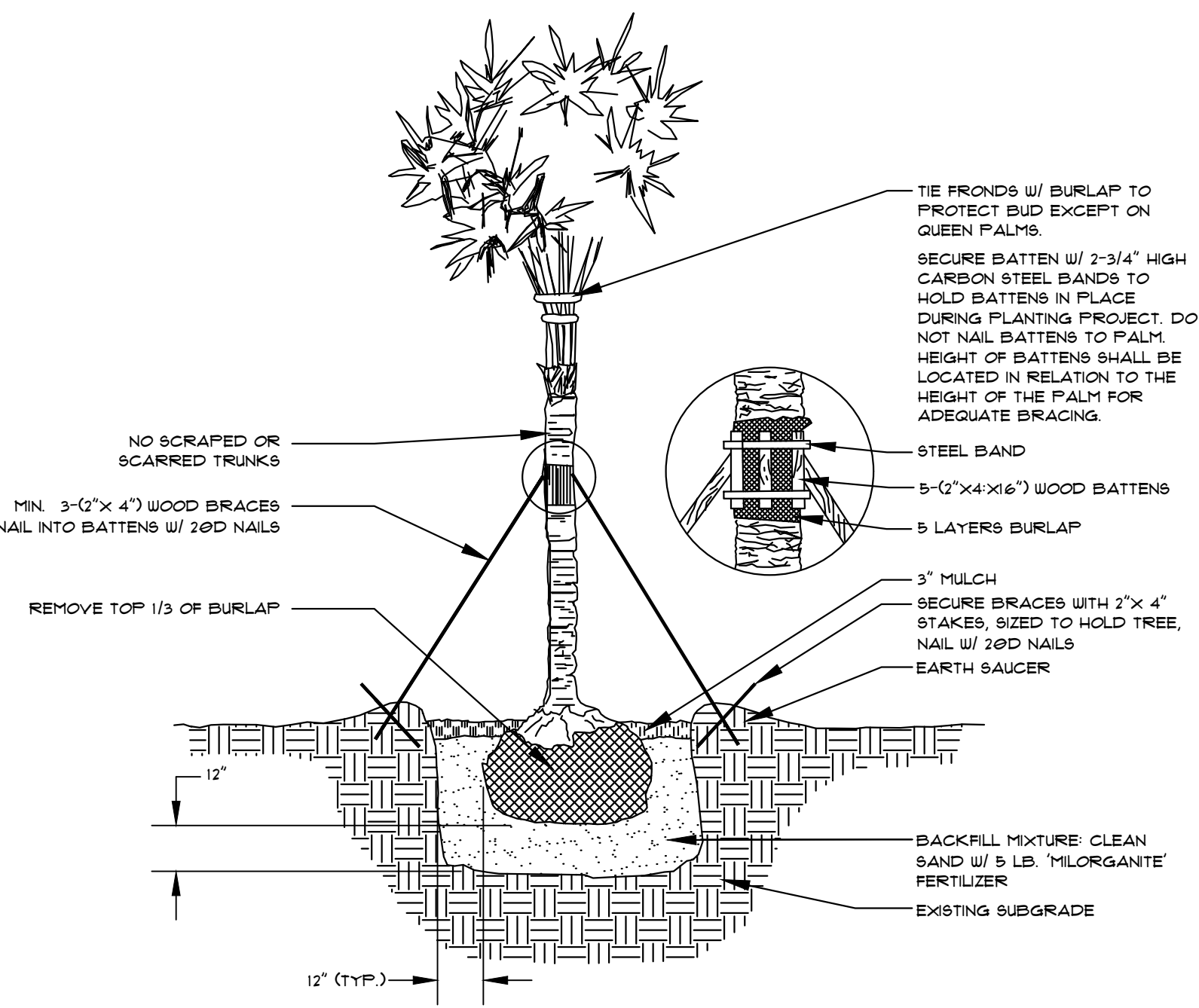
LANDSCAPE CONTRACTOR SHALL, AT ALL TIMES, KEEP JOB SITE CLEAN AND FREE FROM ACCUMULATION OF WASTE MATERIAL, DEBRIS, AND RUBBISH.



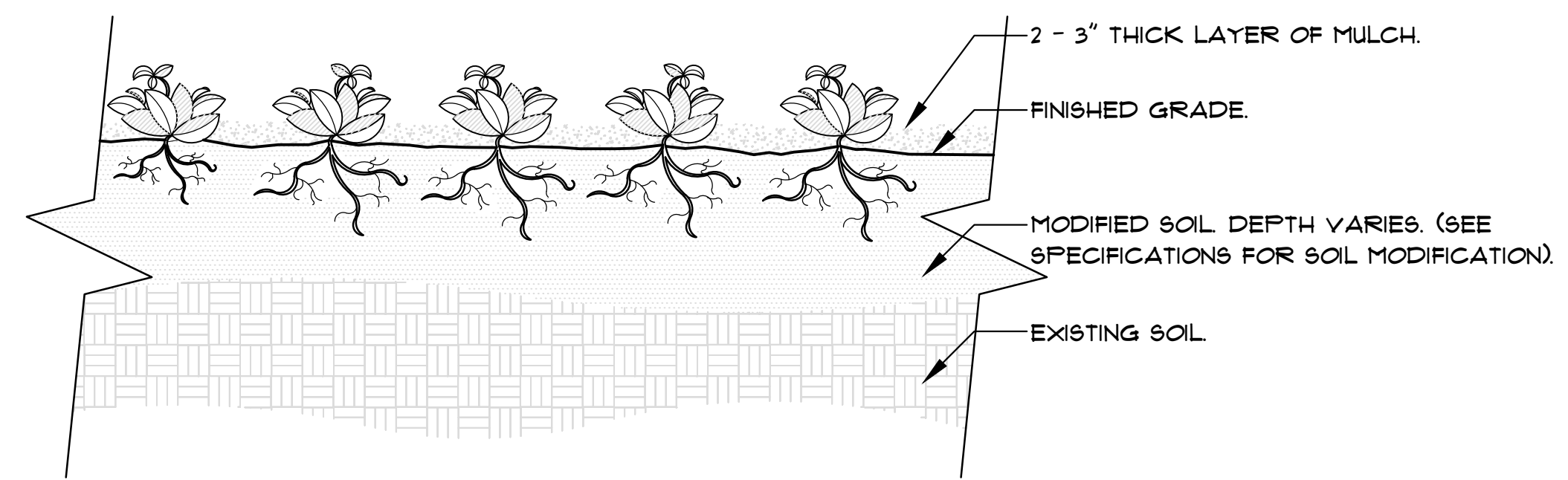
SHRUB & GROUNDCOVER PLANTING DETAIL



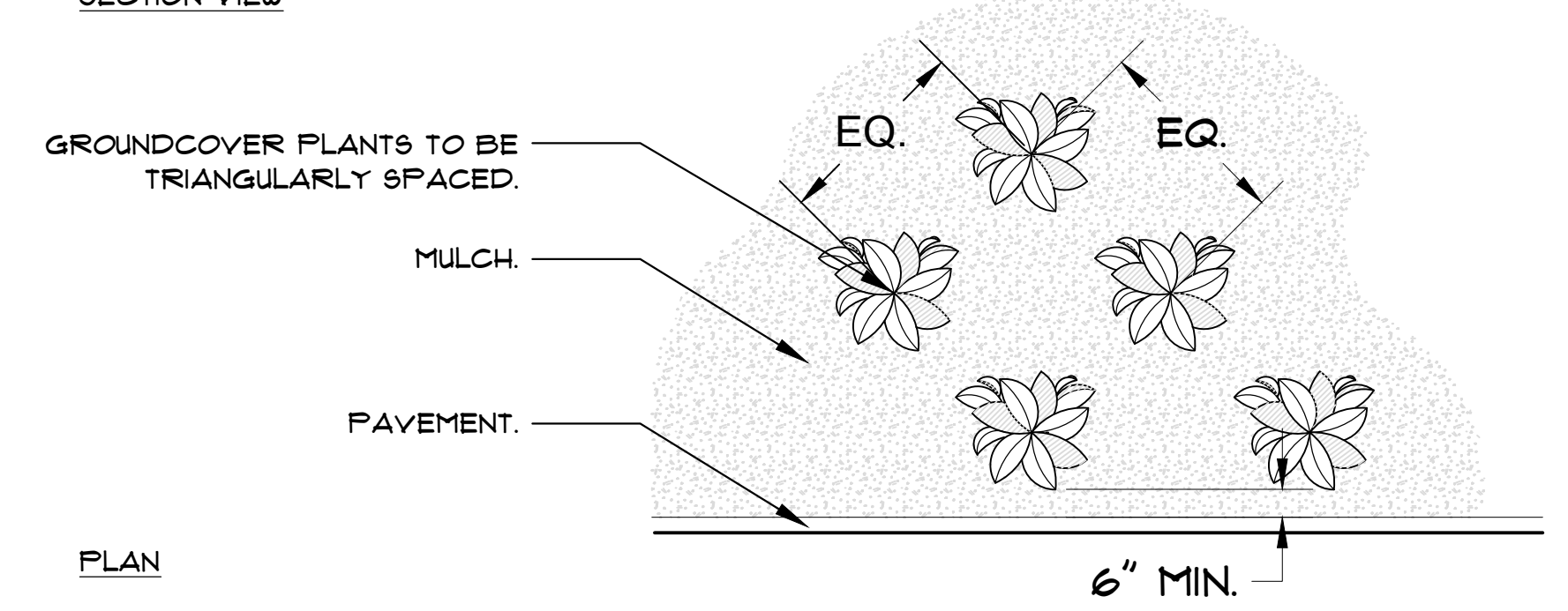
TREE PLANTING DETAIL



SABAL PALM PLANTING DETAIL



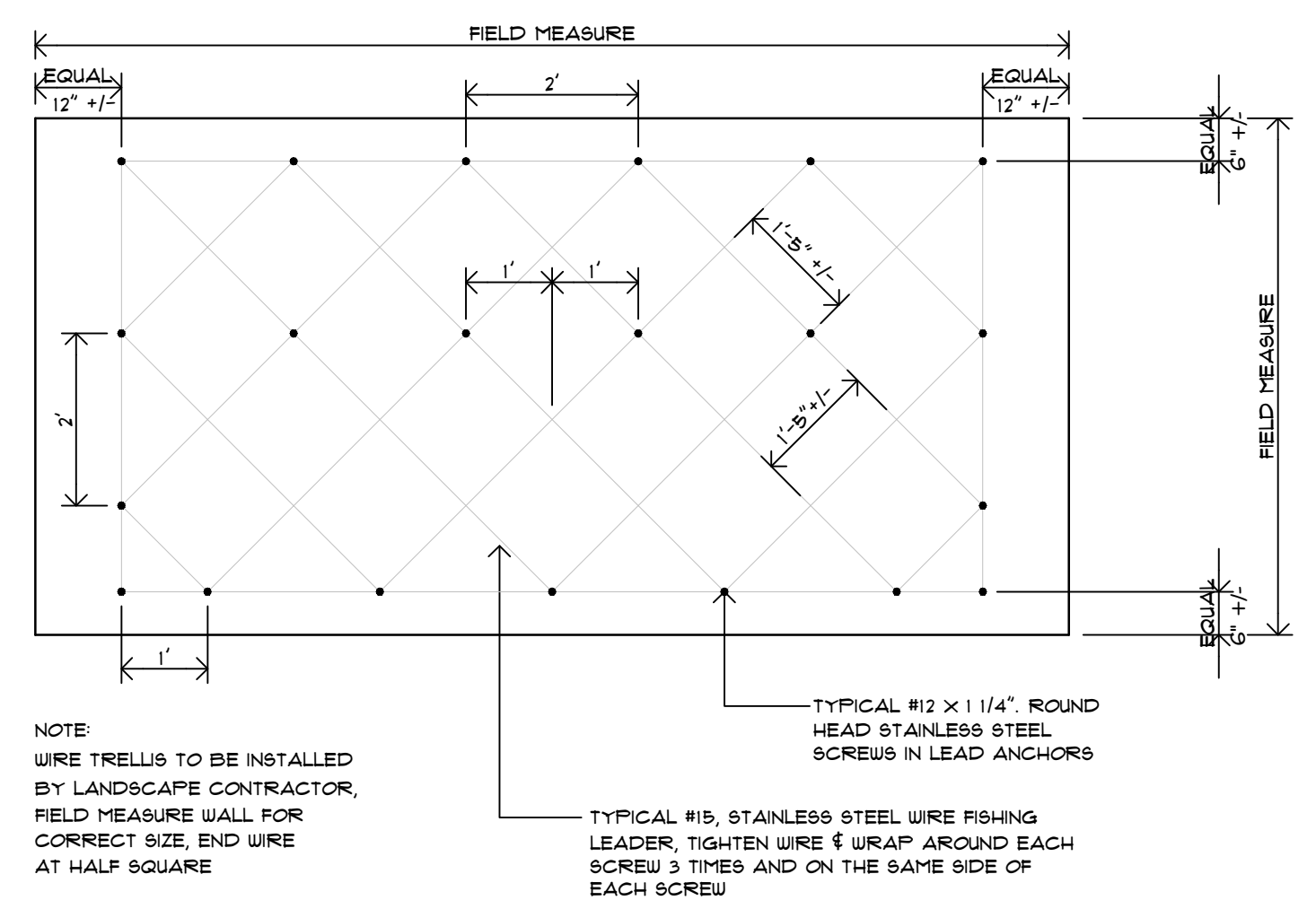
SECTION VIEW



PLAN

- NOTES:**
 1- SEE PLANTING LEGEND FOR GROUNDCOVER SPECIES, SIZE, AND SPACING DIMENSION.
 2- SMALL ROOTS (1/4" OR LESS) THAT GROW AROUND, UP, OR DOWN THE ROOT BALL PERIPHERY ARE CONSIDERED A NORMAL CONDITION IN CONTAINER PRODUCTION AND ARE ACCEPTABLE HOWEVER THEY SHOULD BE ELIMINATED AT THE TIME OF PLANTING. ROOTS ON THE PERIPHERY CAN BE REMOVED AT THE TIME OF PLANTING. (SEE ROOT BALL SHAVING CONTAINER DETAIL).
 3- SETTLE SOIL AROUND ROOT BALL OF EACH GROUNDCOVER PRIOR TO MULCHING.

GROUNDCOVER DETAIL



NOTE:
 WIRE TRELLIS TO BE INSTALLED BY LANDSCAPE CONTRACTOR, FIELD MEASURE WALL FOR CORRECT SIZE. END WIRE AT HALF SQUARE

WIRE TRELLIS DETAIL



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LANDSCAPE PLANS
BENEDETTI OFFICE BUILDING
 400 OSCEOLA STREET, STUART FL, 34994

SHEET
L5.0
 DETAILS &
 SPECIFICATIONS

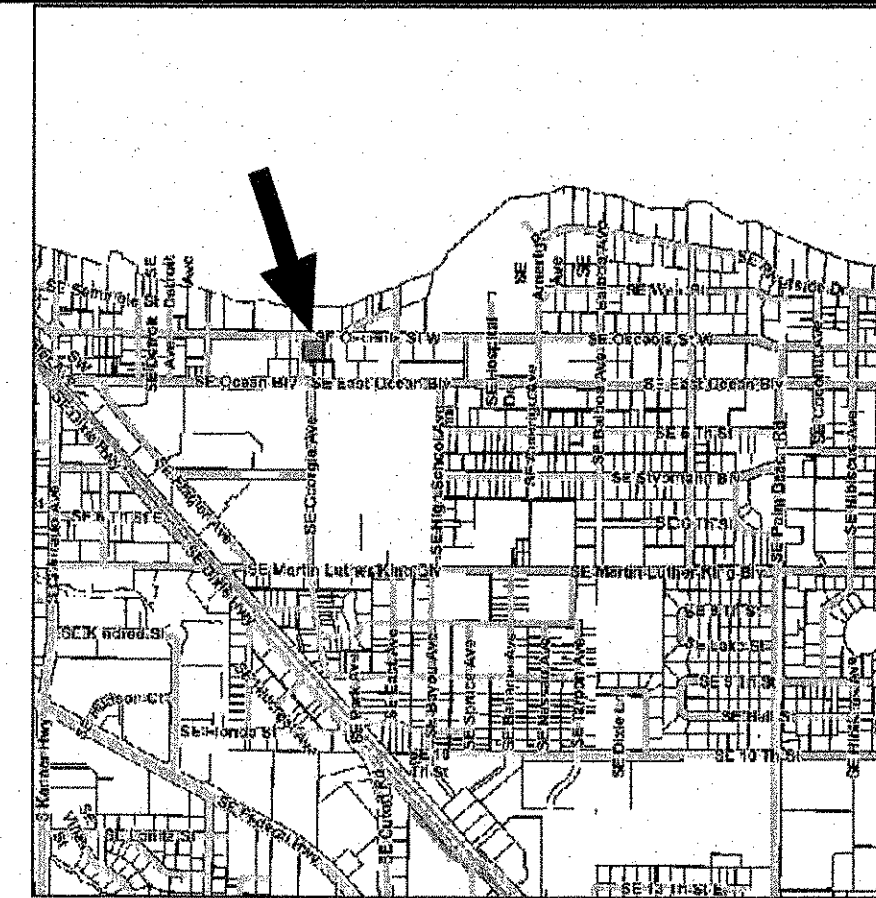
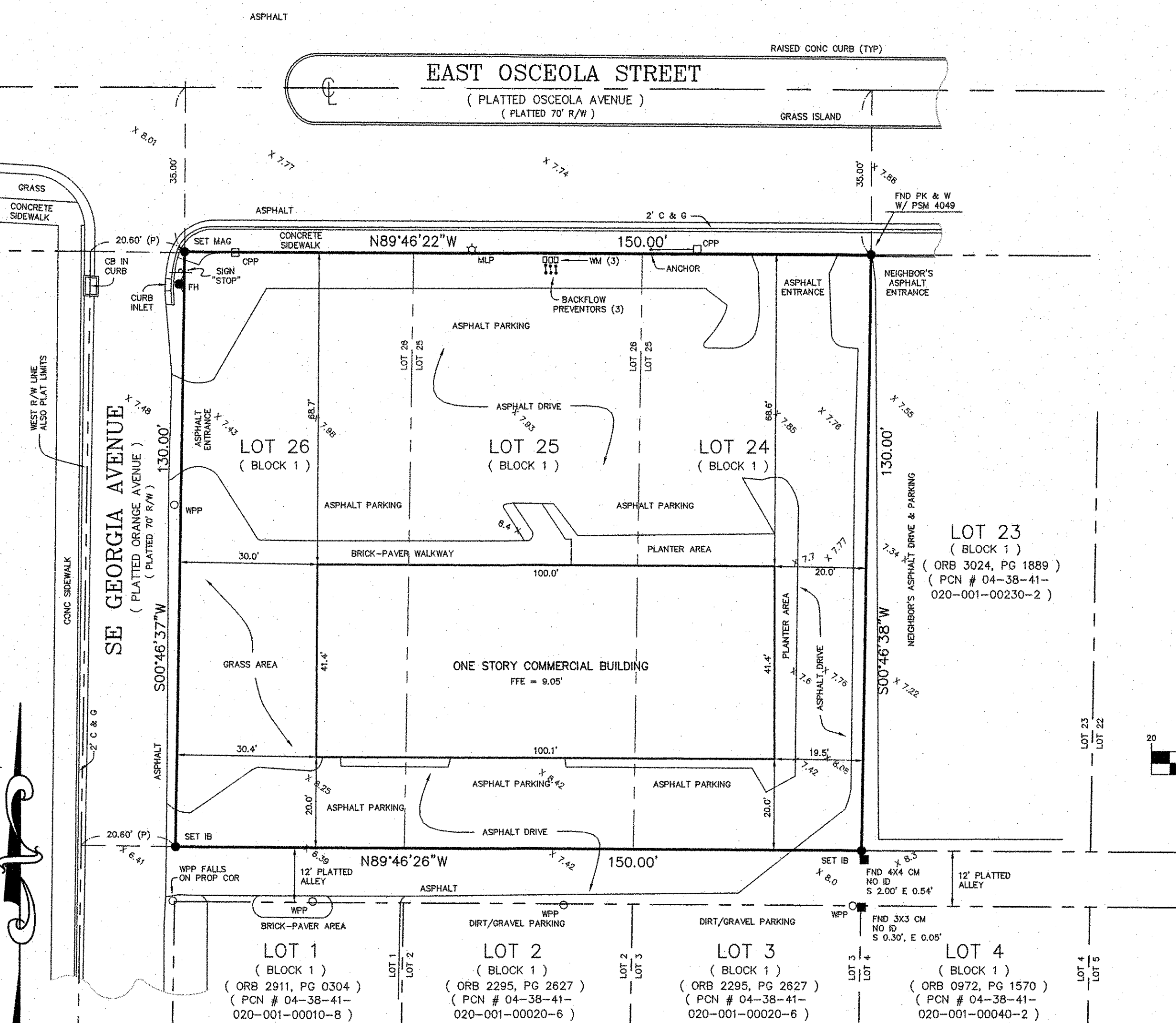
DRAWN BY: RJM
 DESIGNED BY: RJM
 CHECKED BY: RJM

DATE: 1/5/25
 REVISED: 5/25/25

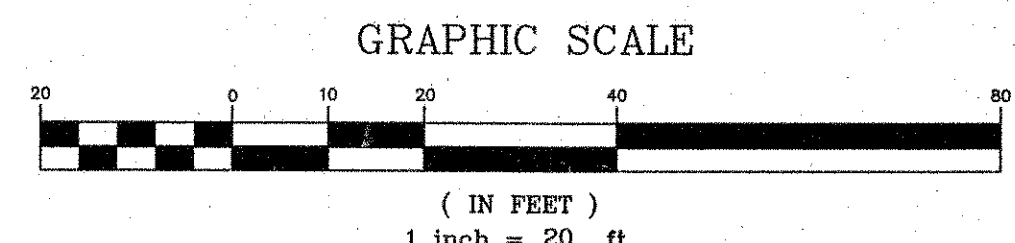


LEGEND:

AC	ACRE
A/C	AIR CONDITIONER
ALUM	ALUMINUM
ANC	ANCHOR
APPROX	APPROXIMATE
AVE	AVENUE
BRG	BEARING
BLK	BLOCK
BLVD	BOULEVARD
BLDG	BUILDING
BM	BENCHMARK
CB	CABLE TELEVISION BOX
CATV	CABLE TELEVISION
C	CALCULATED
CB	CATCH BASIN
Z	CENTRAL ANGLE
CLF	CHAINLINK FENCE
CHD	CHORD
CONC	CONCRETE
CBS	CONC. BLOCK STRUCTURE
CM	CONCRETE MONUMENT
CNR	COULD NOT READ
CPP	CORNER
COR	CORNER POWER POLE
COV	COVERED
CMP	CORRUGATED METAL PIPE
CVG	CONCRETE VALLEY GUTTER
D	DEED
D/F	DEED
DE	DEED
DF	DEED
ESMT	EASEMENT
EOC	EDGE OF CONCRETE
EOP	EDGE OF PAVEMENT
EOW	EDGE OF WATER
EM	ELECTRIC METER
ES	ELECTRIC SERVICE
ELEV	ELEVATION
EQUIP	EQUIPMENT
X 17.00	EXISTING ELEVATION
FT	FEET
FNC	FIELD MEASUREMENT
FTE	FENCE
FE	FINISHED FLOOR ELEVATION
FH	FIRE HYDRANT
FPL	FLOUIDA POWER & LIGHT
FND	FOUND
GOVT	GOVERNMENT
HSE	HOUSE
I & E	INGRESS & EGRESS EASEMENT
INV	INVERT
IP	IRON BAR
IB & C	IRON BAR & CAP
IP & C	IRON PIPE & CAP
LE	LANDSCAPE EASEMENT
LB	LICENSED BUSINESS NUMBER
MLP	METAL LIGHT POLE
M & W	MAG NAIL & WASHER
MAG	MAG NAIL
ME	MAINTENANCE EASEMENT
MH	MANHOLE
MHL	MEAN HIGH WATER LINE
M/F	METAL FENCE
N & TT	NAIL & TIN TAB
N & W	NAIL & WASHER
NAV D	NORTH AMERICAN VERTICAL DATUM
NO ID	NO IDENTIFICATION
NTS	NOT TO SCALE
NAD	NORTH AMERICAN DATUM
ORB	OFFICIAL RECORD BOOK
O/S	OFFSET
OH	OVERHANG
OHW	OVERHEAD WIRE
PC	PAGE
PK	PARKER-KALON NAIL
PK & TT	PARKER-KALON NAIL & TIN TAB
PK & W	PARKER-KALON NAIL & WASHER
PVMT	PAVEMENT
PRM	PERMANENT REFERENCE MONUMENT
P	PLAT
PB	PLATBOOK
PCN	PARCEL CONTROL NUMBER
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
POB	POINT OF BEGINNING
POC	POINT OF COMMENCEMENT
POE	POINT OF ENCROACHMENT
PRC	POINT OF REVERSE CURVATURE
PT	POINT OF TANGENCY
PF	POWER POLE
PUE	PRIVATE UTILITY EASEMENT
PLS	PROFESSIONAL LAND SURVEYOR
PL	PROPERTY LINE
PROPOSED	PROPOSED
PROPOSED ELEVATION	PROPOSED ELEVATION
PRO	PRORATED
R	RADIUS
RAD	RADIAL
RRS	RAILROAD SPIKE
RNG	RANGE
RLS	REGISTERED LAND SURVEYOR
RCP	REINFORCED CONCRETE PIPE
R/W	RIGHT OF WAY
SS	SANITARY SEWER
SECT	SECTION
SET IB	SET 5/8 IRON BAR & CAP #4049
SET MAG	SET MAG NAIL & WASHER #4049
S/T	SEPTIC TANK
S/W	SIDEWALK
SET	SOUTHERN BELL TELEPHONE BOX
S/F	SQUARE FEET
S/D	SUBDIVISION
T	TANGENT
TOE	TEMPORARY CONSTRUCTION EASEMENT
TOB	TOP OF BANK
TOE	TOE OF SLOPE
TWP	TOWNSHIP
TRANS	TRANSFORMER
TYP	TYPICAL
UDE	UTILITY & DRAINAGE EASEMENT
UE	UTILITY EASEMENT
V/F	VINYL FENCE
W/F	WOOD FENCE
WM	WATER METER
WV	WATER VALVE
WPP	WOOD POWER POLE
W	WATER
Δ	SURVEY WORK POINT



LEGAL DESCRIPTION
LOTS 23, 24, AND 25, BLOCK 1, STYPMANN'S SUBDIVISION, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 5, PAGE 82, OF THE PUBLIC RECORDS OF PALM BEACH (NOW MARTIN) COUNTY, FLORIDA.
(ALSO RECORDED IN ORB 3024, PAGE 1889)
PCN # 04-38-41-020-001-00240-0



Date of Original Field Survey: 07/02/1992
Date of Updated Boundary Survey: 05/28/2025

NOTES:

- Survey of description as furnished by Client.
- Lands shown hereon were not abstracted for easements and/or rights-of-way of record.
- All Survey Measurements are in Accordance with The United States Standard, in Feet.
- This is a Boundary & Topographic Survey as Defined in Chapter 5J-17, Florida Administrative Code.
- There are no above ground encroachments, unless otherwise shown.
- Bearings, Coordinates, and Distances as shown on this survey are referenced to Grid North, Based on State Plane Coordinates, U.S. Survey Feet, North American Datum of NAD 83/11, (2011 Adjustment), Florida East Zone, and The Elevations Shown Hereon Are Based on The North American Vertical Datum of 1988 and Are Referenced to The LENGEMANN of Florida's L-NET GPS Network Developed Which Meets or Exceeds The Federal Geodetic Control Committee (FGCC) Guidelines For "Geometric Geodetic Accuracy Standards and Specifications For Using GPS Relative Positioning Techniques", As Well as The Standards of Practice Set Forth By The Florida Board of Land Surveyors in Chapter 5J-17, Florida Administrative Code, Pursuant to Chapter 472.027, Florida Statutes.
- Elevations as Shown Hereon are in Feet and Decimals, and Reference the North American Vertical Datum of 1988, (NAVD 88), And Are Based On Benchmark, "OC-1", Having an Elevation = 6.16'. The "Site or Temporary Benchmarks" as Shown on this Survey are Based on a Closed Level Loop From Said Benchmark "OC-1" to "OC-1", With a Verticle Closure of 0.01'. Elevations as Shown Have Been Measured to an Estimated Verticle Positional Accuracy of +/- 0.05'.
- The Expected Use of the Subject Property is for Future Construction and Engineering Design and Falls within the Commercial High Risk Category, as Classified in Florida Administrative Code. All Field-Measured Control Measurements Exceeded 1:10,000 Feet Accuracy Requirements for This Classification, And, On This Particular Survey, The Scale Factor Was Measured at 1.000018633.
- The National Flood Insurance Program designation as indicated on the F.E.M.A. Map No. 12085C0153H, dated 02/19/2020, locates the parcel in Zone X, base flood elevation N/A feet; subject to any scaling and interpolation factors associated with mapping of this accuracy. This data is an interpretation by the surveyor and is provided as a courtesy. The flood zone(s) should be verified by a determination agency.
- Underground foundations & utilities not located unless shown.
- Additions or deletions to survey maps or reports by other than the signing party or parties is prohibited without written consent of the signing party or parties.

1. **PROPERTY ADDRESS:**
-400 EAST OSCEOLA STREET

2. **CERTIFIED TO:**
-MICHAEL BENEDETTI

NOT VALID WITHOUT THE SIGNATURE AND THE ORIGINAL RAISED SEAL OF A FLORIDA LICENSED SURVEYOR AND MAPPER.

STEPHEN J. BROWN, INC.

STEPHEN J. BROWN, PROFESSIONAL SURVEYOR AND MAPPER
REGISTRATION NO. 4049, STATE OF FLORIDA

REVISIONS	BY
CONVERT TO AUTOCAD 05/28/2025	S.J.B.
TOPO ADDED 09/22/2025	S.J.B.
CITY COMMENTS REVISION 10/13/2025	S.J.B.

BOUNDARY SURVEY PREPARED FOR: BENEDETTI

STEPHEN J. BROWN, INC.
LICENSED BUSINESS NUMBER: 6484
SURVEYORS • DESIGNERS • LANDPLANNERS • CONSULTANTS
619 EAST 5TH STREET, STUART, FLORIDA 34994 EMAIL: STEVE@SUBSTUART.COM PHONE: (772)-288-7176

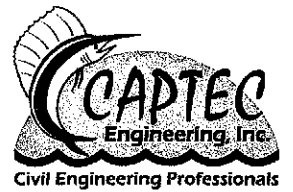
DRAWN	S.J.B.
CHECKED	S.J.B.
DATE	07/17/1992
SCALE	1" = 20'
JOB NO.	1239-02-01A
SHEET	ONE
OF ONE	SHEETS

Uniform Signage Plan for CPUD 400 SE Osceola St.

Proposed signage Plan:

- Maintain Aesthetic attributes of City of Stuart Municode Sec. 6.11.05.
- Ground Signs (monument or freestanding)
 - Propose setback is adjusted from 10' to 2' from the sidewalk (right-of-way) along the North boundary due to the parking lot starting 8' from the sidewalk.
 - Propose setback from the West (Georgia St) 6' from right-of-way to not encroach upon landscape beauty.
- Wall signs:
 - Must be anchored to concrete block
 - On building face
 - For Suite 3:
 - 1 sign for the practice name not to exceed 8 sq. ft.
 - 1 sign per occupant not to exceed 4 sq. ft. each
 - For Suite 2:
 - 1 sign not to exceed 8 sq. ft.
 - For Suite 1:
 - Max 9 sq. ft. per occupant
 - Max 2 signs per occupant

**400 SE OSCEOLA ST
DRAINAGE STATEMENT
STUART, FLORIDA**



INTRODUCTION

The following drainage statement is for the existing office building at 400 SE Osceola St project located in the City of Stuart within Martin County, Florida. The intent of the proposed project is to re-purpose one of the three office spaces for a Hair Salon.

SITE LOCATION AND DESCRIPTION

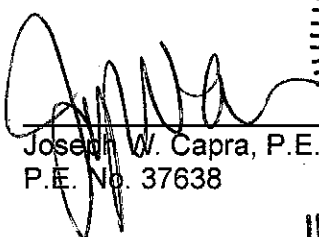
The project site is approximately 0.45 acres and is located on East Osceola St, at the intersection of SE Georgia Ave. The site is currently developed with an existing asphalt parking lot, office building, and green area.

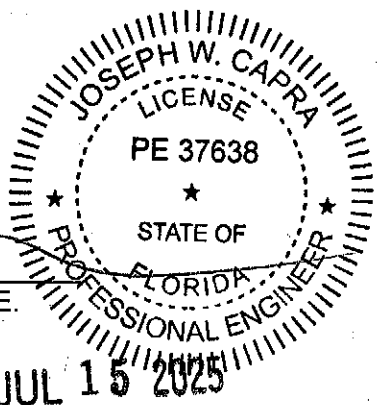
DRAINAGE

Runoff from the existing site currently drains offsite to existing storm inlets located on SE Georgia Ave and SE Osceola St. There are no proposed exterior improvements as it relates to the pervious and impervious areas of the site, and therefore, no water quality improvements proposed with the site.

The existing finished floor elevation is 9.00 ft.-NAVD.

The Site lies within FEMA Flood Zone X as shown on FEMA Flood Map 12085C0151H on the attached Exhibit.


Joseph W. Capra, P.E.
P.E. No. 37638


JUL 15 2025

**400 SE OSCEOLA ST
DRAINAGE STATEMENT
STUART, FLORIDA**



INTRODUCTION

The following drainage statement is for the existing office building at 400 SE Osceola St project located in the City of Stuart within Martin County, Florida. The intent of the proposed project is to re-purpose one of the three office spaces for a Hair Salon.

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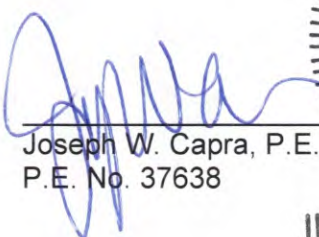
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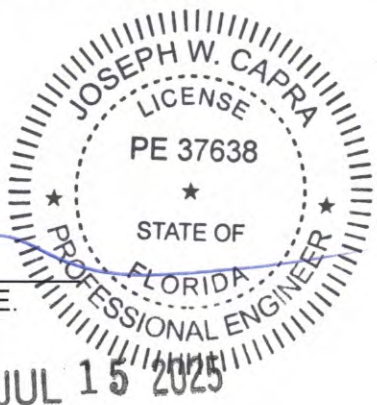
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Joseph W. Capra, P.E.
P.E. No. 37638



MEMORANDUM

April 14, 2025

Re: 400 Osceola Street
 Traffic Statement
 Stuart, Florida
 Project № 25055.00.03

Introduction

Traffic & Mobility Consultants was retained to provide a traffic analysis for 400 Osceola Street which is proposing interior building modifications to offer salon services; the City of Stuart is requiring a rezoning from R-3 to Commercial Planned Unit Development. **Attachment 1** provides the application narrative.

Existing Trip Generation Analysis

400 Osceola Street is an existing 4,100 square feet of medical/dental office space on approximately 0.45 acres; existing access to SE Georgia Avenue and SE Osceola Avenue will remain as is.

Medical-Dental Office Building rates from the ITE Trip Generation 11th Edition were applied; Tables 1a, 1b, and 1c summarize trip generation.

Table 1a- Daily

Description	Land Use Code	Intensity	Units	Daily Trip Generation	Directional Split		Gross Trips			Daily Net Trips		
					In	Out	In	Out	Total	In	Out	Total
Medical-Dental Office Building	720	4.10	KSF	$T = 42.97(X) - 108.01$	50%	50%	34	34	68	34	34	68

$R^2=.92$

Table 1b- Adjacent Street AM

Description	Land Use Code	Intensity	Units	AM Peak Hour Trip Generation	Directional Split		Gross Trips			AM Net Trips		
					In	Out	In	Out	Total	In	Out	Total
Medical-Dental Office Building	720	4.10	KSF	$\ln(T) = 0.90$ $\ln(X) + 1.34$	79%	21%	11	3	14	11	3	14

$R^2=.80$

Table 1c- Adjacent Street PM

Description	Land Use Code	Intensity	Units	PM Peak Hour Trip Generation	Directional Split		Gross Trips			PM Net Trips		
					In	Out	In	Out	Total	In	Out	Total
Medical-Dental Office Building	720	4.10	KSF	$T = 4.07(X) - 3.17$	30%	70%	4	10	14	4	10	14

Source: ITE Trip Generation 11th Edition

$R^2=.77$

PUD Trip Generation Analysis

The change from R-3 to Commercial PUD increases the maximum dwelling units per acre from 10 units per acre to 30 units per acre when comparing *Sec.2.03.08 Tables 3a to 3b*. On the 0.45 acre parcel, this equates to currently allowable 4 dwelling units to a potential 13 dwelling units; or an increase in 9 potential units.

Single-Family Detached Housing rates from the ITE Trip Generation 11th Edition were applied; Tables 2a, 2b, and 2c summarize trip generation.

Table 2a- Daily

Description	Land Use Code	Intensity	Units	Daily Trip Generation	Directional Split		Gross Trips			Daily Net Trips		
					In	Out	In	Out	Total	In	Out	Total
Single-Family Detached Housing	210	9	Dwelling	$\ln(T) = 0.92$ $\ln(X) + 2.68$	50%	50%	55	55	110	55	55	110

$R^2=.95$

Table 2b- Adjacent Street AM

Description	Land Use Code	Intensity	Units	AM Peak Hour Trip Generation	Directional Split		Gross Trips			AM Net Trips		
					In	Out	In	Out	Total	In	Out	Total
Single-Family Detached Housing	210	9	Dwelling	$\ln(T) = 0.91$ $\ln(X) + 0.12$	25%	75%	2	6	8	2	6	8

$R^2=.90$

Table 2c- Adjacent Street PM

Description	Land Use Code	Intensity	Units	PM Peak Hour Trip Generation	Directional Split		Gross Trips			PM Net Trips		
					In	Out	In	Out	Total	In	Out	Total
Single-Family Detached Housing	210	9	Dwelling	$\ln(T) = 0.94$ $\ln(X) + 0.27$	63%	37%	7	3	10	7	3	10

Source: ITE Trip Generation 11th Edition

$R^2=.92$

400 Osceola Street
Traffic Statement
Project № 25055.00.03
April 14, 2025
Page 3 of 3

Conclusion

Medical-Dental Office Building represents the worst-case trip generation scenario under the existing zoning district of R-3, as well as the proposed Commercial PUD zoning district. The worst case trip generation is demonstrated to result in no net change, and the change in allowable dwelling units is demonstrated to generate less than 20 net peak hour trips; the project does not require additional analysis and is deemed to be consistent with city transportation concurrency requirements.

I remain available should you have any questions or comments.

Respectfully submitted,
TRAFFIC & MOBILITY CONSULTANTS

THIS ITEM HAS BEEN DIGITALLY SIGNED AND SEALED BY

ON THE DATE ADJACENT TO THE SEAL

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED
SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

TRAFFIC & MOBILITY CONSULTANTS LLC
3101 MAGUIRE BOULEVARD, SUITE 265
ORLANDO, FLORIDA 32803

CERTIFICATE OF AUTHORIZATION CA-30024
Ayman As-Saidi, P.E. № 56849



To: Kristopher McCrain
Principal Planner
City of Stuart Development Department

From: Alex Memering, P.E.
Kimley-Horn and Associates, Inc. ARM

Date: September 8, 2025

Re: 400 Osceola Street
Review of Rezoning Traffic Statement

Kimley-Horn has reviewed the traffic statement (dated April 14, 2025) prepared by Traffic & Mobility Consultants related to the above-mentioned project. The applicant is proposing to rezone a 0.45-acre parcel from its existing R-3 Residential – Multi-Family/Office (R-3) zoning designation to Commercial Planned Unit Development (CPUD). The property is located at the southeast corner of Osceola Street & Georgia Avenue in Stuart, Florida.

It is anticipated that the proposed trip generation potential for the CPUD zoning designation is as follows: 68 daily trips, 14 (4 entering/ 10 exiting) AM peak hour trips, and 14 (10 entering/ 4 exiting) PM peak hour trips. The existing trip generation potential for the R-3 zoning designation is as follows: 110 daily trips, 8 (2 entering/ 6 exiting) AM peak hour trips, and 10 (7 entering/ 3 exiting) PM peak hour trips. The proposed CPUD zoning designation shows a minimal increase in peak hour traffic generation compared to the existing R-3 zoning designation which will not adversely affect the surrounding transportation network.

Thank you for the opportunity to assist the City of Stuart in reviewing this project. Please contact us if you have any questions or need additional information.

**CITY OF STUART, FLORIDA
AGENDA ITEM REQUEST
Community Redevelopment Board**

Meeting Date: 2/3/2026

Prepared by: Jodi Nentwick

Title of Item:

AVONLEA COMMERCIAL CORNERS EAST MAJOR AMENDMENT TO THE NEW AVONLEA PUD FOR PARCEL D (LOT 11)

ORDINANCE No. 2550-2026; AN ORDINANCE OF THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA, PROVIDING FOR THE THIRD AMENDMENT OF THE "NEW AVONLEA PLANNED UNIT DEVELOPMENT" CONSISTING OF 12.40 ACRES ZONED RESIDENTIAL PLANNED UNIT DEVELOPMENT, 16.24 ACRES ZONED COMMERCIAL PLANNED UNIT DEVELOPMENT, AND 20.4 ACRES OF WETLAND/UPLAND PRESERVE ZONED RESIDENTIAL PLANNED UNIT DEVELOPMENT, BEING GENERALLY LOCATED BETWEEN NE DIXIE HIGHWAY TO THE SOUTH, LAND ON BOTH SIDES OF GREEN RIVER PARKWAY ON THE WEST, LAND ON BOTH SIDES OF CARDINAL AVENUE ON THE EAST AND THE CITY BOUNDARY TO THE NORTH, FOR PROPERTY LEGALLY DESCRIBED BY EXHIBIT "A" ATTACHED; AMENDING THE COMMERCIAL PLANNED UNIT DEVELOPMENT TO PROVIDE FOR THE APPROVAL OF THE MASTER PARCEL SITE PLAN FOR THE AVONLEA COMMERCIAL EAST PARCEL D (LOT 11) TO BE IDENTIFIED AS AVONLEA COMMERCIAL CORNERS EAST; PROVIDING FOR THE APPROVAL OF THE MASTER SITE OFF-SITE INFRASTRUCTURE PLAN FOR THE ENTIRE PLANNED UNIT DEVELOPMENT; PROVIDING FOR REVISED AND NEW DEVELOPMENT CONDITIONS APPROVAL FOR PARCEL D (LOT 11); PROVIDING FOR AMENDED AND NEW TIMETABLES FOR DEVELOPMENT; DECLARING THE PLAN TO BE CONSISTENT WITH THE CITY'S COMPREHENSIVE PLAN; PROVIDING DIRECTIONS TO THE CITY CLERK; PROVIDING FOR REPEAL OF ALL ORDINANCES IN CONFLICT; PROVIDING FOR SEVERABILITY; PROVIDING FOR AN EFFECTIVE DATE, AND FOR OTHER PURPOSES.

Summary Explanation/Background Information on Agenda Request:

On November 18, 2022, an application was submitted by Cotleur & Hearing ("Applicant") on behalf of Avonlea Commercial Corners LLC ("Property Owner") for a major amendment to New Avonlea PUD seeking authorization to develop the +/- 0.90 acre-parcel constructing a 6,827 square foot mixed-use, consisting of office, medical, personal services, and restaurants.

The 0.9 acre development area has been established as a development area by the Master PUD plan and is to be considered in conjunction with the over 20-acres of preserve area that has been established, restored and protected as a part of the PUD. It is important to note that the development acreage is "net" of wetland setbacks as memorialized by the Conservation Easement with South Florida Water Mmanagement District (SFWMD), the New Avonlea PUD Plat and the land use amendment of the preserve areas and setbacks to Conservation from PUD.

Project History:

Originally approved by Ordinance No. 2307-2015, the Avonlea Master plan has had several minor changes specifically for the residential areas of the development. The intent of the original New Avonlea PUD (2307-2015) was to amend the previous Avonlea PUD and provide platted boundaries for development, preserve and associated setbacks and reduce the intensity of the project to a more realistic and more appropriate for Stuart density. Presently, Parcels 2 and 12 are nearing build-out with 62 townhome units. On Parcel 16 there are 69 units currently under construction and parcel 10 and 13 are proposed for the development of 203 apartment units. In total, the PUD permits 40,000 square feet of development on the parcels subject to this application, and we are further reducing that density/intensity

to 6,827 square feet, which is over a quarter of the permitted density. The specific project area is vacant and wooded and has not been developed previously. The most recent amendment to the Avonlea development was approved and adopted in April 2020 through Ordinance No. 2434-2020.

Funding Source:

N/A

Recommended Action:

Staff recommends the Board forwards a recommendation to the City Commission to approve the major amendment to the New Avonlea PUD to develop Parcel D (Lot 11) to be known as Avonlea Commercial Corners East.

ATTACHMENTS:

- 1. Ordinance 2550-2025 Avonlea Lot 11 Major Amendment CPUD
- 2. CRB PUD Amendment Application
- 3. CRB Avonlea Commercial Corners East Narrative
- 4. CRB Avonlea Master Site Plan Final
- 5. CRB Avonlea Lot 11 Site Plan
- 6. CRB Avonlea East Plans and Elevations
- 7. CRB Avonlea_East_01 Colored Rendering
- 8. CRB Avonlea_East_02 Colored Rendering
- 9. CRB Boundary & Topographic Survey
- 10. CRB Landscape Plans
- 11. CRB Avonlea Lot 11 Tree Disposition Plan
- 12. CRB Applicant's Response to Tree Mitigation
- 13. CRB Avonlea Lot 11 Irrigation Plan
- 14. CRB Traffic Impact Analysis
- 15. CRB City's Traffic Consultant Approval Letter



**BEFORE THE CITY COMMISSION
CITY OF STUART, FLORIDA**

ORDINANCE NUMBER 2550-2026

AN ORDINANCE OF THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA, PROVIDING FOR THE THIRD AMENDMENT OF THE “NEW AVONLEA PLANNED UNIT DEVELOPMENT” CONSISTING OF 12.40 ACRES ZONED RESIDENTIAL PLANNED UNIT DEVELOPMENT, 16.24 ACRES ZONED COMMERCIAL PLANNED UNIT DEVELOPMENT, AND 20.4 ACRES OF WETLAND/UPLAND PRESERVE ZONED RESIDENTIAL PLANNED UNIT DEVELOPMENT, BEING GENERALLY LOCATED BETWEEN NE DIXIE HIGHWAY TO THE SOUTH, LAND ON BOTH SIDES OF GREEN RIVER PARKWAY ON THE WEST, LAND ON BOTH SIDES OF CARDINAL AVENUE ON THE EAST AND THE CITY BOUNDARY TO THE NORTH, FOR PROPERTY LEGALLY DESCRIBED BY EXHIBIT “A” ATTACHED; AMENDING THE COMMERCIAL PLANNED UNIT DEVELOPMENT TO PROVIDE FOR THE APPROVAL OF THE MASTER PARCEL SITE PLAN FOR THE AVONLEA COMMERCIAL EAST PARCEL D (LOT 11) TO BE IDENTIFIED AS AVONLEA COMMERCIAL CORNERS EAST; PROVIDING FOR THE APPROVAL OF THE MASTER SITE OFF-SITE INFRASTRUCTURE PLAN FOR THE ENTIRE PLANNED UNIT DEVELOPMENT; PROVIDING FOR REVISED AND NEW DEVELOPMENT CONDITIONS APPROVAL FOR PARCEL D (LOT 11); PROVIDING FOR AMENDED AND

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* * * * *

WHEREAS, Florida Statutes § 163.3202 requires the City of Stuart to adopt, amend, and enforce land development regulations that are consistent with and serve to implement the City’s Comprehensive Plan; and

WHEREAS, pursuant to the Florida Local Government Development Agreement Act, Florida Statutes § 163.3220 *et. Seq.*, the City of Stuart has the authority to hold public hearings in order to enter into, amend or revoke a development agreement; and

WHEREAS, on June 8, 2015, the City Commission adopted Ordinance No. 2307-2015, whereby establishing the “New Avonlea PUD” which consists of a Master Parcel Plan and provides that any future development activities on any of the project’s multiple parcels (with the exception of Phase II of an existing Avonlea Commerce Center Parcel) shall require a master site plan approval through a major Planned Unit Development (PUD) amendment, further requiring that an off-site infrastructure plan be added to the Master Parcel Plan at the time of first amendment.

WHEREAS, on June 11, 2018, the City Commission adopted Ordinance No. 2266-2018, amending the PUD for the approval of the Master parcel site plan for the Avonlea Preserve A (Parcel 2) and B (Parcel 12) Residential Planned Unit Development (RPUD) developments, including the New Avonlea Off-Site Infrastructure Plan are consistent with the PUD and anticipated development; and

WHEREAS, on December 9, 2019, the City Commission adopted Ordinance No. 2420-2019,

Ordinance No. 2550-2026 – New Avonlea PUD – Major PUD Amendment No. 4 and Master Site Plan Approval for Avonlea Commercial Corners East - Parcel D (Lot 11)

amending the PUD providing approval of the Master Parcel Site Plan for the Avonlea Crossing A (Parcel 1), to be known as Kindig Professional Offices, providing for a Master Off-Site Infrastructure Plan for the entire PUD, revised and additional development conditions of approval, and provided for amended and new timetables for development; and

WHEREAS, on June 8, 2020, the City Commission adopted Ordinance No. 2438-2020 approving a minor amendment to the “New Avonlea PUD” amending Exhibit Number 11 of the New Avonlea PUD, entitled the Off-Site Infrastructure Plan by HJA Design Studio, and amending Condition No. 25; and

WHEREAS, on April 13, 2020, the City Commission adopted Ordinance 2434-2020, amending the Commercial Planned Unit Development (CPUD) of the Master Parcel Site plan for the Avonlea Crossings Parcel B (Parcel 16) to rezone from CPUD to a RPUD, amending the Master Off-Site Infrastructure Plan for the entire PUD, revised and additional development conditions of approval, and amended timetables for development; and

WHEREAS, on November 18, 2022, an application was submitted by Cotleur & Hearing (“Applicant”) on behalf of Avonlea Commercial Corners LLC (“Property Owner”) for a major amendment to New Avonlea PUD seeking authorization to develop the +/- 0.90 acre-parcel constructing a 6,827 square foot mixed-use, consisting of office, medical, personal services; and

WHEREAS, the Community Redevelopment Board (“CRB”) held a properly noticed hearing on February 3, 2026, to consider the Applicant’s request and unanimously voted to recommend (*APPROVAL/DENY*) subject to certain conditions; and

WHEREAS, the City Commission held two properly noticed public hearings with the First Reading of the ordinance on XXXXX, 2026, and the Second Reading of the ordinance on XXXXX, 2026.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF STUART, FLORIDA that:

SECTION 1: The foregoing recitals shall be and are hereby incorporated as if fully set forth herein.

SECTION 2: The legal description of the subject property, reflecting the entire New Avonlea PUD parcels, is set forth on **Exhibit “A”** attached hereto and made a part hereof by reference. A map depicting the subject property is attached hereto as **Exhibit “B”** and made part of reference.

SECTION 3: In consideration of the CRB recommendation, all written and oral comments at the public hearing, the analysis by the City’s Development Department and the evidence and testimony presented by the parties at the public hearing, the City Commission has determined that the Applicant/Property Owner showed substantial competent evidence that the application is consistent with the City’s Comprehensive Plan and Land Development Code of the City along with being compliant with the procedural requirements of the law. The City Commission hereby approves the Major Amendment to the “New Avonlea PUD” subject to certain Conditions of Approval. The Conditions of Approval for the subject development are attached hereto attached as **Exhibit “C”** and made part hereof by reference.

SECTION 4: All ordinances or parts of ordinances and resolutions in conflict with this ordinance or any part thereof are hereby repealed to the extent of such conflict. If any provision of this ordinance conflicts with any prior contractual provision between the City and the Applicant, Property Owner, and/or successor in interest of the site, this ordinance shall prevail.

SECTION 5: Following the adoption of this ordinance and any action for failure to complete development or otherwise comply with the Development Documents, the City Development Director may obtain a hearing before the City Commission, and shall thereupon give at least five (5) days written notice of the time, date and location of the hearing, along with specific notice of

the alleged breach. At the hearing before the City Commission the Applicant, Property Owner or successor in interest may appear and may contest the allegation of breach or explain the reason or reasons for the breach. Upon a finding of a material breach of the Development Documents and therefore, the Ordinance(s) and Resolution(s) adopting the same, the City Commission may impose or do any or all the following:

- a. Initiate the process to amend or repeal this or any other ordinance or resolution pertaining to the development.
- b. Direct the City Development Director to initiate the process to rezone the CPUD property or any portion of the CPUD property.
- c. Impose an administrative penalty of up to \$1,000.00 for each violation, and up to \$5,000.00 for each repeat violation that occurs, along with all reasonable costs, including attorney's fees incurred by the City.

Any breach of any provision or condition of this CPUD ordinance by the Applicant, Property Owner or successor in interest shall be considered a zoning violation subject to any remedies provided herein, or as otherwise provided by law. In the event a violation continues from day to day, each day the violation is found to continue shall be deemed a separate violation.

SECTION 6: If any provision of this ordinance or the application thereof to any person or circumstance is held invalid; the invalidity shall not affect other provisions or applications which can be given effect without the invalid provision or application, and to this end the provisions of this ordinance are declared severable.

SECTION 7: This ordinance shall become effective immediately upon adoption by the City Commission.

SECTION 8: If any provision of this ordinance or the application thereof to any person or circumstance is held invalid, the invalidity shall not affect other provisions or applications which

Ordinance No. 2550-2026 – New Avonlea PUD – Major PUD Amendment No. 4 and Master Site Plan Approval for Avonlea Commercial Corners East - Parcel D (Lot 11)

can be given effect without the invalid provision or application, and to this end, the provisions of this ordinance are declared severable.

SECTION 9: The following documents (hereinafter “**Approved Development Documents**” are attached hereto attached as (**Exhibit “D”**) on file as public records of the City of Stuart, at the City Clerk’s office in City Hall, shall be deemed as part of the conditions applicable to the subject development:

The following are General New Avonlea PUD Plans and Documents (reinstated by Ordinance 2420-2019):

1. The Master Parcel Plan, Sheet 1 of 1 by Lucido and Associates, dated 5.27.18 (existing)
2. South Florida Water Management District Permit #43-01387-P, issued 6.9.16. (existing)
3. Department of the Army Permit #SAJ-1998-03203. (existing)
4. Drainage Statement, by Grzelka Engineering, Inc. and as built plans for master drainage. (existing)
5. Master Drainage Plan, by DLS Environmental Services, Inc. dated March 2016 (revised)
6. Environmental Assessment Report, prepared by DLS Environmental Services, Inc. dated July 2014 (existing)
7. Preserve Area Management Plan, prepared by DLS Environmental Services, Inc., dated March 2017 (existing)
8. Traffic Impact Analysis prepared by MacKenzie Engineering and Planning, dated October 2014 (existing)
9. 2015 Pattern Book for Avonlea Commerce Center Phase II (existing)
10. PUD Boundary Survey prepared by Velcon Group, Inc., dated 01.08.14 (existing)
11. Avonlea PUD Off-Site Infrastructure Plan, by HJA Design Studio, Sheet 1 and 2 of 2, dated 1.11.16 (existing)
12. Avonlea Typical Cross Section, by Cotleur and Hearing, Inc., dated 4.19.18 (existing)
13. Amended and Restated Declaration of Covenants and Restrictions for the New Avonlea P.U.D., dated 11.30.16. (existing)

Master Parcel Plans for Avonlea Crossings Parcel A (Parcel 1)

14. Site Plan for Avonlea Crossing A, by Kuoppala & Associates, dated 9.30.19 (existing)
15. Landscape Plan for Avonlea Crossing A, by Litterick Landscape Architecture, dated 9.30.19 (existing)
16. Architectural Elevations and Floor Plans for Avonlea Crossing A, by Kuoppala & Associates, dated 9.30.19 (existing)
17. Boundary Survey for Avonlea Crossing A, by Velcon Engineering and Surveying, dated 4.30.19 (existing)

The following documents are established by Ordinance 2366-2018 and are not being replaced:

Master Parcel Plans for Avonlea Preserve Parcel A (Parcel 2) and B (Parcel 12)

18. Site Plan for Avonlea Preserve A, by Cotleur and Hearing, Inc, dated 11.20.17 (existing)
19. Site Plan for Avonlea Preserve B, by Cotleur and Hearing, Inc., dated 11.20.17 (existing)
20. Lot Plan for Avonlea Preserve A and B, by Cotleur and Hearing, Inc., dated 4.24.18 (existing)
21. Landscape Plan for Avonlea Preserve A, by Cotleur and Hearing, Inc., dated 5.25.18 (existing)
22. Landscape Plan for Avonlea Preserve B, by Cotleur and Hearing, Inc., dated 5.25.18 (existing)
23. Landscape Plan details for Avonlea Preserve A and B, by Cotleur and Hearing, Inc., dated 5.25.18 (existing)
24. Boundary Survey for Avonlea Preserve A, by Velcon Group, Inc., dated 1.27.17 (existing)
25. Boundary Survey for Avonlea Preserve B, by Velcon Group, Inc., dated 2.6.17 (existing)
26. Amenity Package, dated 2.2.18 (existing)

The following documents are established by Ordinance 2550-2026:

Master Parcel Plans for Avonlea Commercial Corners East – Parcel D (Lot 11)

27. To be populated at First Reading

Ordinance No. 2550-2026 – New Avonlea PUD – Major PUD Amendment No. 4 and Master Site Plan Approval for Avonlea Commercial Corners East - Parcel D (Lot 11)

SECTION 10: The complete execution and recording of this Ordinance by the City Clerk shall occur no later than 60 days from the date of this approval, failing which this Ordinance shall be void.

SECTION 11: Upon complete execution of this ordinance, the City Clerk is directed to record this ordinance in the Public Records of Martin County, Florida.

First read on the ____ day of _____, 2026.

Commissioner _____ offered the foregoing Ordinance and moved its adoption.

The motion was seconded by Commissioner _____ and upon being put to a roll call vote, the vote was as follows:

CHRISTOPHER COLLINS, MAYOR
 SEAN REED, VICE MAYOR
 EULA CLARKE, COMMISSIONER
 LAURA GIOBBI, COMMISSIONER
 CAMPBELL RICH, COMMISSIONER

	YES	NO	ABSENT	ABSTAIN

ADOPTED on second and final reading this day of _____, 2026.

ATTEST:

 MARY R. KINDEL
 CITY CLERK

 CHRISTOPHER COLLINS
 MAYOR

APPROVED AS TO FORM
 AND CORRECTNESS:

 LEE J. BAGGETT, ESQ.
 CITY ATTORNEY

EXHIBIT "A" Legal Description

AVONLEA PUD LEGAL DESCRIPTION:

BEGINNING ON THE SOUTH RIGHT-OF-WAY LINE OF BAKER ROAD 333 FEET EAST OF THE WEST LINE OF THE OF THE SOUTHEAST 1/4 OF THE SOUTHEAST 1/4, SECTION 29, TOWNSHIP 37 SOUTH, RANGE 41 EAST, THENCE RUN SOUTH 100 FEET TO THE POINT OF BEGINNING; THENCE RUN SOUTH 107 FEET; THENCE RUN EAST 177 FEET; THENCE RUN SOUTH 10 FEET; THENCE RUN EAST 330 FEET; THENCE RUN SOUTH 650 FEET; THENCE RUN EAST 500 FEET TO THE SOUTHEAST CORNER OF SECTION 29; THENCE RUN NORTH 1261 FEET, MORE OR LESS, TO THE SOUTHEASTERLY RIGHT-OF-WAY OF BAKER ROAD; THENCE RUN SOUTHWESTERLY TO THE SOUTH RIGHT-OF-WAY, THENCE RUN WEST 375 FEET; THENCE RUN SOUTH 100 FEET; THENCE RUN WEST 200 FEET; THENCE RUN NORTH 100 FEET; THENCE RUN WEST 100 FEET; THENCE RUN SOUTH 100 FEET AND WEST 100 FEET TO THE POINT OF BEGINNING (LESS THE WEST 50 FEET OF THE EAST 420 FEET OF THE NORTH 100 FEET OF THE SOUTH 133 FEET) AND (LESS ROAD RIGHT-OF-WAY). SECTION 29, TOWNSHIP 37 SOUTH, RANGE 41 EAST, MARTIN COUNTY, FLORIDA.

LESS, HOWEVER,

A STRIP OF LAND 50.00 FEET IN WIDTH IN THE SOUTHEAST ONE QUARTER OF THE SOUTHEAST ONE QUARTER OF SECTION 29, TOWNSHIP 37 SOUTH, RANGE 41 EAST, MARTIN COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT THE SOUTHEAST CORNER OF SAID SECTION 29, BEAR NORTH 88° 49' 00" WEST ALONG THE SOUTH LINE OF SAID SECTION A DISTANCE OF 272.50 FEET; THENCE NORTH 0° 15' 00" EAST ALONG A LINE PARALLEL TO THE EAST LINE OF SAID SECTION A DISTANCE OF 874.94 FEET TO THE SOUTH RIGHT-OF-WAY LINE OF BAKER ROAD; THENCE NORTH 89° 04' 50" WEST ALONG SAID LINE A DISTANCE OF 282.26 FEET TO THE POINT OF BEGINNING; THENCE CONTINUE NORTH 89° 04' 50" WEST A DISTANCE OF 50.00 FEET; THENCE SOUTH 0° 55' 10" WEST A DISTANCE OF 223.36 FEET; THENCE SOUTH 88° 49' 00" EAST A DISTANCE OF 50.00 FEET; THENCE NORTH 0° 55' 10" EAST A DISTANCE OF 223.59 FEET TO THE POINT OF BEGINNING.

ALSO LESS,

COMMENCE ON THE SOUTH RIGHT-OF-WAY LINE OF BAKER ROAD, 333 FEET FROM THE WEST LINE OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 29, TOWNSHIP 37 SOUTH, RANGE 41 EAST; THENCE RUN SOUTH 100 FEET TO THE POINT OF BEGINNING; THENCE RUN SOUTH 107 FEET; THENCE RUN EAST 177 FEET; THENCE RUN SOUTH 10 FEET, THENCE RUN EAST 273 FEET; THENCE RUN NORTH 217 FEET TO A POINT IN THE SOUTH RIGHT-OF-WAY LINE OF BAKER ROAD (50 FEET WIDE); THENCE RUN WEST 50 FEET; THENCE RUN SOUTH 100 FEET; THENCE RUN WEST 200 FEET; THENCE RUN NORTH 100 FEET TO A POINT IN THE SOUTH RIGHT-OF-WAY LINE OF BAKER ROAD (50 FEET WIDE); THENCE RUN WEST 100 FEET; THENCE RUN SOUTH 100 FEET; THENCE RUN WEST 100 FEET TO THE POINT OF BEGINNING.

LESS, HOWEVER, THE WEST 50 FEET OF THE EAST 420 FEET OF THE NORTH 100 FEET OF THE SOUTH 133 FEET.

PARCEL 2:

THE SOUTH 1/2 OF THE SOUTHWEST 1/4 OF SECTION 28, TOWNSHIP 37 SOUTH, RANGE 41 EAST, MARTIN COUNTY, FLORIDA.

LESS, HOWEVER, BEGINNING AT THE SOUTHWESTERLY CORNER OF SAID SECTION 28, RUN NORTH ON SAID SECTION LINE 330 FEET; THENCE RUN EAST 396 FEET; THENCE RUN SOUTH 330 FEET; THENCE RUN WEST 396 FEET TO THE POINT OF BEGINNING LESS HIGHWAY RIGHT-OF-WAY (CONTAINING ACRES, MORE OR LESS).

AND LESS, HOWEVER, BEGINNING 396 FEET EAST OF THE SOUTHWESTERLY CORNER OF SAID SECTION 28 RUN NORTH ON SAID SECTION LINE 165 FEET; THENCE RUN EAST 792 FEET; THENCE RUN SOUTH 165 FEET TO STATE ROAD 707 AND WEST TO THE POINT OF BEGINNING LESS HIGHWAY RIGHT-OF-WAY AND LESS RAILROAD RIGHT-OF-WAY.

Ordinance No. 2550-2026 – New Avonlea PUD – Major PUD Amendment No. 4 and Master Site Plan Approval for Avonlea Commercial Corners East - Parcel D (Lot 11)

AND LESS, HOWEVER, THAT PART THAT LIES SOUTH OF THE SOUTHERLY RIGHT-OF-WAY OF STATE ROAD #707 AND NE SAVANNAH ROAD.

AND LESS, HOWEVER, THAT PART OF THE NE 1/4 OF THE SE 1/4 OF THE SW 1/4 IN SECTION 28, TOWNSHIP 37 SOUTH, RANGE 41 EAST LYING NORTH OF THE NORTHERLY RIGHT-OF-WAY OF NE SAVANNAH ROAD AND THE EXTENSION OF BAKER ROAD.

AND LESS, HOWEVER, THE EAST 202.00 FEET OF THE NORTHWEST ONE-QUARTER (NW 1/4) OF THE SOUTHEAST ONE-QUARTER (SE 1/4) OF THE SOUTHWEST ONE-QUARTER (SW 1/4) OF SECTION 28, TOWNSHIP 37 SOUTH, RANGE 41 EAST, MARTIN COUNTY, FLORIDA, LYING NORTH ON THE NORTH RIGHT-OF-WAY LINE OF NE BAKER ROAD ACCORDING TO THE PLANS FOR BAKER ROAD EXTENSION (MARTIN COUNTY PROJECT NO. 55-R-5). THE AREA OF THIS PARCEL IS 1.65 ACRES, MORE OR LESS.

SUBJECT TO EASEMENTS, RESTRICTIONS, RESERVATIONS AND RIGHT-OF-WAYS OF RECORD, IF ANY.

TOGETHER WITH

AVONLEA COMMERCIAL CENTER

A PARCEL OF LAND LYING IN SECTION 28, T37S, R41E, MARTIN COUNTY, FLORIDA BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGIN AT THE POINT OF INTERSECTION OF THE WEST LINE OF THE EAST 202.00' OF THE NORTHWEST 1/4 OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 SAID SECTION 28, T37S, R41E OF PARCEL 6 AS DESCRIBED IN OFFICIAL RECORDS BOOK 895, PAGE 19, PUBLIC RECORDS OF MARTIN COUNTY, FLORIDA AND THE NORTHERLY RIGHT OF WAY LINE OF BAKER ROAD EXTENTION AS DESCRIBED IN OFFICIAL RECORDS BOOK 726, PAGE 1921, PUBLIC RECORDS OF MARTIN COUNTY, FLORIDA FOR THE POINT OF BEGINNING; THENCE N89°28'19"W ALONG SAID NORTHERLY RIGHT OF WAY LINE OF BAKER ROAD EXTENTION, 348.92' TO THE POINT OF INTERSECTION WITH THE EASTERLY RIGHT OF WAY LINE OF N.E.CARDINAL WAY BEING A 60.00' RIGHT OF WAY AS NOW LAID OUT AND IN USE; THENCE N10°56'26"W ALONG SAID EASTERLY RIGHT OF WAY LINE OF N.E.CARDINAL WAY, 381.26' TO THE POINT OF INTERSECTION WITH THE SOUTH LINE OF A PARCEL DESCRIBED IN OFFICIAL RECORDS BOOK 895, PAGE 25 AND 19, PUBLIC RECORDS OF MARTIN COUNTY, FLORIDA AND PHYSICALLY DESCRIBED BY MONUMENTS AND OCCUPATION; THENCE S89°15'48"E ALONG SAID OCCUPIED LINE, 426.24' TO THE POINT OF INTERSECTION WITH THE AFORESAID PARCEL 6; THENCE S00°45'39"W ALONG SAID WEST LINE OF THE EAST 202.00' OF PARCEL 6 A DISTANCE OF 372.11' TO THE POINT OF BEGINNING.

TOGETHER WITH

JENSEN INDUSTRIAL PARK SITE

A PARCEL OF LAND LYING IN SECTION 28, TOWNSHIP 37 SOUTH, RANGE 41 EAST, MARTIN COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID SECTION 28, THENCE, NORTH 00° 15' 38" EAST, ALONG THE WEST LINE OF SAID SECTION 28, A DISTANCE 861.35 FEET, MORE OR LESS, TO THE INTERSECTION THEREOF WITH SOUTHERLY RIGHT-OF-WAY LINE OF BAKER ROAD EXTENSION; THENCE SOUTH 89° 58' 55" EAST, ALONG SAID SOUTHERLY RIGHT-OF-WAY LINE, A DISTANCE 1545.39 FEET FOR A POINT OF BEGINNING (P.O.B.);

THENCE, SOUTH 89° 58' 55" EAST, CONTINUING ALONG SAID SOUTHERLY RIGHT-OF-WAY LINE, A DISTANCE OF 520.27 FEET; THENCE, SOUTH 20° 06' 54" EAST CONTINUING ALONG SAID RIGHT-OF-WAY LINE, A DISTANCE OF 142.51 FEET MORE OR LESS, TO THE INTERSECTION THEREOF WITH THE NORTHERLY RIGHT-OF-WAY LINE FOR SAVANNAH ROAD; THENCE, SOUTH 49° 53' 54" WEST, ALONG NORTHERLY RIGHT-OF-WAY LINE, A DISTANCE OF 412.74 FEET, MORE OR LESS, TO THE POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF CARDINAL AVENUE, SAID POINT BEING THE POINT OF CURVATURE OF A CURVE TO THE RIGHT HAVING A RADIUS OF 75.00 FEET, THENCE, SOUTHERLY, WESTERLY, AND NORTHERLY ALONG SAID CURVE AND SAID EASTERLY RIGHT-OF-WAY LINE, THROUGH A CENTRAL ANGLE OF 78° 51' 33", A DISTANCE OF 103.23 FEET TO A POINT OF COMPOUND CURVATURE OF A CURVE CONCAVE EASTERLY HAVING A RADIUS OF 585.00 FEET AND WHOSE RADIUS POINT BEARS NORTH 38° 45' 27" EAST, THENCE, NORTHWESTERLY ALONG SAID CURVE AND CONTINUING ALONG SAID EASTERLY RIGHT-OF-WAY LINE, THROUGH A CENTRAL ANGLE OF 39° 47' 31", A DISTANCE OF 406.28 FEET TO THE POINT OF

Ordinance No. 2550-2026 – New Avonlea PUD – Major PUD Amendment No. 4 and Master Site Plan Approval for Avonlea Commercial Corners East - Parcel D (Lot 11)

TANGENCY, THENCE, NORTH 11° 27' 12" WEST, CONTINUING ALONG SAID EASTERLY RIGHT-OF-WAY LINE, A DISTANCE OF 1.01 FEET TO THE POINT OF CURVATURE OF A CURVE TO THE RIGHT HAVING A RADIUS OF 50.00 FEET; THENCE, NORTHEASTERLY ALONG SAID CURVE AND CONTINUING ALONG SAID EASTERLY RIGHT-OF-WAY LINE, THROUGH A CENTRAL ANGLE OF 101° 28' 07", A DISTANCE OF 88.55 FEET TO THE POINT OF BEGINNING.

CONTAINING; 4.06 ACRES, MORE OR LESS.

SUBJECT TO EASEMENTS, RESERVATIONS, AND RIGHTS-OF-WAY OF RECORD.

THE BEARINGS AS STATED HEREIN ARE BASED ON AN ASSUMED BEARING OF NORTH 00° 15' 38" EAST ALONG THE WEST LINE OF SAID SECTION 28.

LESS, HOWEVER,

LEGAL DESCRIPTION OF A PORTION OF PROPOSED GREEN RIVER PARKWAY RIGHT-OF-WAY

A PARCEL OF LAND LYING IN SECTION 28 AND SECTION 29, TOWNSHIP 37 SOUTH, RANGE 41 EAST, MARTIN COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID SECTION 29; THENCE, NORTH 88° 48' 22" WEST, ALONG THE SOUTH LINE OF SAID SECTION 29, A DISTANCE OF 500.00 FEET; THENCE, NORTH 00° 15' 52" EAST DEPARTING SAID SECTION LINE, A DISTANCE OF 55.02 FEET; THENCE, NORTH 33° 12' 44" EAST, A DISTANCE OF 172.02 FEET TO THE POINT OF CURVATURE OF A CURVE TO THE LEFT, HAVING A RADIUS OF 2028.75 FEET; THENCE, NORTHERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 27° 30' 11", A DISTANCE OF 973.84 FEET, MORE OR LESS, TO THE INTERSECTION THEREOF WITH THE EASTERLY RIGHT-OF-WAY LINE OF BASELINE ROAD; THENCE, NORTH 30° 40' 07" EAST, ALONG SAID EASTERLY RIGHT-OF-WAY LINE, A DISTANCE OF 238.43 FEET, MORE OR LESS, TO THE INTERSECTION THEREOF WITH THE SOUTH PLAT BOUNDARY LINE OF NORTHWOOD SUBDIVISION, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN THE PLAT BOOK 1, PAGE 55, PUBLIC RECORDS, MARTIN COUNTY, FLORIDA; THENCE, SOUTH 89° 58' 55" EAST, ALONG SAID SOUTH PLAT BOUNDARY LINE, A DISTANCE OF 10.00 FEET, MORE OR LESS, TO A POINT 40.00 FEET EASTERLY OF THE SOUTHWEST CORNER OF SAID PLAT OF NORTHWOOD SUBDIVISION, THENCE, SOUTH 00° 15' 15" WEST, DEPARTING SAID SOUTH PLAT BOUNDARY LINE, A DISTANCE OF 10.21 FEET TO THE POINT OF CURVATURE OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 2148.75 FEET; THENCE, SOUTHERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 28° 34' 54", A DISTANCE OF 1071.89 FEET TO A POINT OF REVERSE CURVATURE OF A CURVE CONCAVE NORTHEASTERLY, HAVING A RADIUS OF 180.00 FEET; THENCE, SOUTHEASTERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 117° 38' 31", A DISTANCE OF 307.99 FEET TO A POINT OF TANGENCY; THENCE, SOUTH 88° 48' 22" EAST, ALONG A LINE 60.00 FEET NORTH OF, AS MEASURED AT RIGHT ANGLES TO, THE SOUTH LINE OF SAID SECTION 29, A DISTANCE OF 92.47 FEET TO THE INTERSECTION THEREOF WITH THE EAST LINE OF SAID SECTION 29; THENCE, SOUTH 00° 15' 38" WEST ALONG SAID EAST SECTION LINE, A DISTANCE OF 60.00 FEET TO THE SOUTHEAST CORNER OF SAID SECTION 29 AND THE POINT OF BEGINNING.

EXCEPTING THE WEST 50 FEET OF THE EAST 420 FEET OF THE NORTH 100 FEET OF THE SOUTH 133 FEET OF SAID SECTION 29 AND EXCEPTING THE RIGHTS-OF-WAY FOR BAKER ROAD EXTENSION AND STATE ROAD NO. 707.

SUBJECT TO EASEMENTS, RESTRICTIONS, RESERVATIONS AND RIGHTS OF WAY OF RECORD

PARCEL I.D.: 28-37-41-013-000-00110-0

EXHIBIT "B"

EXHIBIT "C" – DEVELOPMENT CONDITIONS

1. Any modifications to the Master CPUD Site Plan that is less than 10% of the approved building footprint, building setbacks, gross square footage, building location, parking size, location and number of parking, drainage areas, and location of landscaping may be approved by the City's Development Director, provided however, the Development Director may refer the matter to the City Commission for approval via a public hearing.
2. Prior to certificate of occupancy of the last building, the applicant shall provide an engineer's written confirmation attesting that the CPUD's storm water system is in place and functioning in compliance with all approved plans and specifications.
3. Erosion and dust control measures to be implemented during construction shall be provided on the civil plans and submitted during site permit review. Water trucks shall be provided by the applicant as necessary during construction in order to reduce dust generated on-site.
4. All proposed dumpster enclosures must be constructed in accordance with the City of Stuarts Dumpster Enclosure Const. Details.
5. If any of the proposed sanitation receptacles per use/unit require more than twice a week pickup, the owner/developer will be required to install vert-i-pack(s) with 8-yard receptacle(s).
6. All Construction pertinent to the Utilities and Engineering Department shall be installed, inspected and tested in accordance with the City of Stuart Minimum Design and Construction Standards latest edition and the City of Stuart Specifications and Ordinances where applicable. In case of discrepancies between the construction plans and aforementioned manuals, the most restrictive shall apply.
7. Approval by the Utilities and Engineering Department shall not be construed to be a license to proceed with work and shall not be construed as authority to violate, cancel, alter or set aside any of the provisions of the City Code. Approval shall not prevent this department from thereafter requiring a correction of errors in plans, construction or violation of City Code.

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8. The Applicant, Property Owner or successor in interest must provide approval from Martin County for the proposed dumpster location within their easement prior to issuance of site permit (infrastructure).
9. The approval granted shall become null and void unless a Building and/or Site Permit is obtained within five (5) years of the Major Amendment to the CPUD approval, or an extension is granted in accordance with Section 8.05.08 of the City of Stuart Land Development Code.
10. Any changes to this approved site plan will require an application for amendment, in accordance with Section 11.01.00 of the Land Development Code.
11. Prior to final zoning inspection for the required site permit and issuance of the Building Certificate of Occupancy, the Applicant, Property Owner or successor in interest will be required to submit the following documentation to the City of Stuart Development Department:
 - a. As-built landscape plan to confirm the landscaping was installed per the approval plan
 - b. A signed and sealed letter from the landscape architect attesting to that all landscaping has been installed according to the approved plans.
 - c. A one-year warranty certificate from the plant nursery
12. Construction activity shall be limited from 7:00am to 6:00 pm Monday – Saturday.
13. All regulatory agency permits, including but not limited to the South Florida Water Management District and Army Corp of Engineers, shall be obtained by the applicant and copies provided to the City prior to the commencement of any development activities.
14. The Applicant, Property Owner or successor in interest shall construct the project's infrastructure in one phase and complete all site and infrastructure prior to the certificate of occupancy.
15. In the event of any conflict in the provisions of **Exhibit “D” Development Conditions** and attached exhibits, **Exhibit “D”** shall prevail.

16. All utility improvements constructed by the developer within Florida's Department of Transportation's right-of-way, shall be constructed in a manner and form acceptable to the FDOT. The developer shall grant any such utility easements necessary for this site as may be required by the FDOT.
17. Temporary or freestanding storage units are prohibited on the property once construction is complete.

Tree Mitigation

20. A Tree Removal permit is required prior to work commencing.

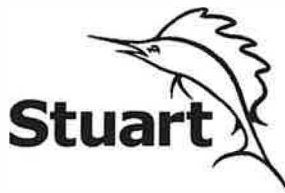
Signage

21. All signage within the CPUD must maintain consistency in quantity, placement, dimensions, height, illumination, and materials, reflecting a unified design theme throughout the Planned Unit Development.

EXHIBIT “D” - Approved Plans and Documents

The Avonlea Commercial Corners East – Parcel D (Lot 11) CPUD project and use thereof shall comply with the following drawings:

To be populated at First Reading



City of Stuart
121 SW Flagler Ave.
Stuart, FL 34994
development@ci.stuart.fl.us
(772) 288-5326

Received by: _____

Reviewed by: _____

Planned Unit Development (PUD) Amendment Application

Project ID# _____
(Staff Entry)

Pre-App Conference Date: <small>1-27-2022</small>	Application Date:
--	-------------------

SITE INFORMATION

Project Name: <small>Avonlea Commercial Corners</small>	Parcel ID#: <small>283741013000001100</small>
Site Address:	
Subdivision: <small>Avonlea CPUD</small>	Lot(s): <small>11</small>
Site Acreage: <small>0.90</small>	Flood Zone/Base Flood Elevation: <small>X</small>
Existing Zoning District / CRA Subdistrict (if applicable): <small>CPUD</small>	
Proposed Zoning District / CRA Subdistrict (if applicable): <small>CPUD</small>	
Current Comprehensive Plan Future Land Use Designation: <small>Neighborhood/ Special District</small>	
Proposed Comprehensive Plan Future Land Use Designation: <small>Neighborhood/ Special District</small>	
Existing Land Use: <small>Vacant/Wooded</small>	Proposed Land Use: <small>Commercial</small>
Proposed Square Footage (if applicable): <small>6,827 SF</small>	Proposed Density (if applicable):

PETITIONER INFORMATION

Property Owner: <small>Avonlea Commercial Corners LLC</small>	Phone Number / Email Address: <small>772-286-6292/ nikschoth@naisouthcoast.com</small>
Property Owner's Mailing Address: <small>100 SW Albany Avenue, Suite #200, Stuart, FL 34994</small>	
Applicant (if not Owner):	Phone Number / Email Address:
Applicant's Mailing Address:	
Agent/Contact Person: <small>George Missimer</small>	Phone Number / Email Address: <small>561-747-6336 / gmissimer@colleur-hearing.com</small>
Agent's Mailing Address: <small>1934 Commerce Lane, Suite 1, Jupiter, FL 34997</small>	
Architect: <small>Dominick Ranieri</small>	Engineer: <small>Darren Guettler, Velcon</small>
Planner: <small>George Missimer</small>	Landscape Architect: <small>Donaldson Hearing</small>

Statement of Ownership and Designation of Authorized Agent

(Please Print or Type)

Before me, the undersigned authority, personally appeared Nikolaus M. Schroth

Who, being by me first duly sworn, on oath deposed and says:

1. That he/she is the fee simple title owner of the property described in the attached Legal Description.
2. That he/she is requesting approval of a PUD Amendment in the City of Stuart, FL.
3. That he/she has appointed Cotleur & Hearing to act as an authorized agent on his/her behalf to accomplish the above project.

Name of Owner: Nikolaus M. Schroth, Avonlea Commercial Corners, LLC

Signature of Owner: [Signature]

100 SW Albany Ave, 200
Street Address

By: Name/Title Manager/Registered Agent.

Stuart, FL 34994
City, State, Zip Code

P.O. Box

772 403 3750

City, State, Zip Code

Telephone Number

Fax Number

nikschroth@NAI SouthCoast.com
Email Address:

STATE OF FLORIDA, COUNTY OF Martin

Sworn and subscribed before me by means of physical presence or online notarization, this

20th day of September, 2022 By Nikolaus Schroth

Personally Known OR Produced Identification

Type of Identification Produced:

[Signature]
Notary Public

My Commission expires:



Financial Responsibility Form

(Please Print or Type)

The Undersigned, as the Property Owner, Lessee, Contract Purchaser, or Applicant (circle one), acknowledges responsibility for all City expenses associated with the referenced application (s) including time spent by the City's consultants and further acknowledges that payment of consultant fees will be made prior to the receipt of the consultant comments.

Name: Nikolaus M. Schroth
Title: Registered Agent
Company: Avonlea Commercial Corners, LLC
Company Address: 100 SW Albany Avenue, Suite #200,

City/State/Zip Code: Stuart, FL, 34994
Telephone Number: 772-286-6292
Facsimile Number:
Email Address (optional):

I hereby certify that all information contained herein is true and correct.

1. Signed this 20th day of September, 2022



Signature of Property Owner, Lessee, Contract Purchaser or Applicant (circle one)

Application Requirements

Fees:

- Major PUD Amendment - \$3,195.00; or
- Minor PUD Amendment: \$2,130.00; or
- PUD Agreement Amendment (text change only): \$1,065.00

(This does not include fees that may be charged as a result of application review by the City's consultants or any required recording fees)

A Major Planned Unit Development Amendment is one which shall include any one of the following;

- A change of two (2) percent or more in the area of any land use designations shown on the site plan;
- Any change in the list of proposed uses;
- An increase in residential density of five (5) percent or more;
- An increase in nonresidential Building square footage of ten (10) percent or more;
- A change in the boundary of the PUD district;
- A change in the site plan or approval regarding any area(s) set aside and designated for future development;
- Any other change determined by the City Development Director to have a potentially significant impact on City services or the surrounding neighborhood;
- An amendment of greater than twelve (12) months to an originally approved timetable of development. Such an amendment may only be approved upon good cause shown to the City Commission. Any contributions conditioned as part of the original PUD agreement shall be revisited upon application for timetable extension. A timetable extension greater than twelve (12) months will require a full concurrency review.

(A Minor Planned Unit Development Amendment is any amendment that is not a Major Amendment.)

Submittal Requirements: A completed application form, the payment of fees, a site plan, one (1) copy of all documents on a PDF formatted disc electronically signed and sealed, and any other information as may be required by the City Development Director in order to do a thorough review of the request. (Note: A concept plan may, at the discretion of the applicant, be submitted instead of a site plan if a site plan has not previously been approved. However, in doing so the applicant acknowledges that a site plan will need to be submitted for City Commission approval prior to making application for a development permit.) *(The data requirements for a site plan and a concept plan are available at the Development Department)*

Approving Authority: The Development Director is required to prepare a staff report and recommendation concerning this application. For a Major PUD amendment, the Local Planning Agency (LPA) is required to hold an advertised public hearing and formulate a recommendation to the City Commission. For both types of applications, the City Commission is required to hold an advertised public hearing after which it may approve, approve with conditions, or deny the application.

Justification: Written justification supporting the application and demonstrating how the application remains:
(a) consistent with the relevant components of the City of Stuart Comprehensive Plan including concurrency with adopted levels-of-service for utilities/facilities and compatibility with existing/planned uses; and
(b) complies with the relevant development standards of the City of Stuart Land Development Code.

(over)

Avonlea Commercial East
CPUD Site Plan
November 18, 2022

Introduction:

Avonlea Commercial Corners LLC, referred to here after as the “Property Owner”, owns several parcels of land in the northern portion of the City of Stuart municipal boundaries. The subject parcel is part of the Avonlea Master Plan and has been planned as a commercial corner within the Avonlea development area. The property will bring new convenient commercial services to the area to support the new residential units currently under construction. The proposed commercial developments intend to provide a mixture of retail, office, medical, personal services, and restaurant uses. The proposed site plan complies with the approved master plan and offsite improvement plan for the Avonlea community and is an important part of the overall development success. The densities, intensities and boundaries of development have been established and it is our intention for them to remain; however, as required with all site plans within the Avonlea Master Plan, a Master Plan revision must be submitted and approved for site specific development review.

Project Contact:**Agent/Planner - Cotleur & Hearing**

Donaldson Hearing / George Missimer

1934 Commerce Lane, Suite 1

Jupiter, FL 33458

Phone: (561) 747- 6336 ext. 110

Fax: (561) 747-1377

Email: dhearing@cotleur-hearing.comgmissimer@cotleur-hearing.com**Developer/Builder**[Nikolaus M. Schroth](#)[100 SW Albany Ave, Suite 200](#)[Stuart FL 34994](#)[772-403-3752](#)Nikschroth@naisouthcoast.com**Location:**

283741013000001100 (East)

The subject property lies North of Dixie Highway and West of Cardinal Avenue. The parcel totals approximately 0.90 acres and anchors a prominent intersection.

Surrounding Zoning:

	Zoning	Future Land Use
Subject Property	CPUD	NEIGHBORHOOD/ SPECIAL DISTRICT
North	CPUD	NEIGHBORHOOD/ SPECIAL DISTRICT
South	CPUD, UG	DOWNTOWN
East	CPUD, UG	NEIGHBORHOOD/ SPECIAL DISTRICT
West	CPUD	NEIGHBORHOOD/ SPECIAL DISTRICT

Project History:

Originally approved by Ordinance No. 2307-2015, the Avonlea Master plan has had several minor changes specifically for the residential areas of the development area. The intent of the original New Avonlea PUD (2307-2015) was to amend the previous Avonlea PUD and provide platted boundaries for development, preserve and associated preserve setbacks and reduce the intensity of the project to a more realistic and more appropriate for Stuart density. Presently, Parcels 2 and 12 are nearing build out with 62 townhome units, on Parcel 16 there are 69 units currently under construction and parcel 10 and 13 are proposed for the development of 203 apartment units. In total the PUD permits 40,000 square feet of development on the parcels subject to this application and we are further reducing that density/intensity to 6,827 square feet, which is over a quarter of the permitted density. The specific project area is vacant and wooded and has not been developed previously. The most recent amendment to the Avonlea development was approved and adopted in April of 2020 through Ordinance No. 2434-2020.

Project Description:

The property owner is proposing to construct a commercial building on the subject parcel of land totaling 0.90 acres. This one-story building is proposed to have several different uses, including but not limited to retail, office, medical, personal services, and restaurants. The Avonlea Commercial Corner east is vital to the new residential units being constructed within the RPUD. The proposed building on the subject property has been designed to engage the street and provide an anchor for the corner of the RPUD.

Beyond the benefits the proposed uses will bring to the area, the physical character of the proposed development enhances and reinforces the architectural styles of the area. The building, while engaging the street, provide a wide pedestrian plaza and outside gathering area. The concept of utilizing the outdoor spaces to enhance developments is an important feature of the proposed site plans.

The 0.9 acre development area has been established as development area by the Master PUD plan and is to be considered in conjunction with the over 20-acres of preserve area that has been established, restored and protected as a part of the PUD. It is important to note that the development acreage is “net” of wetland setbacks as memorialized by the Conservation Easement with SFWMD, the New Avonlea PUD Plat and the land use amendment of the preserve areas and setbacks to Conservation from PUD.

The site plan and building has been planned with these preserve areas in mind and are designed to highlight the unique setting within which Avonlea is being developed.

Site Plan:

Located to the east side of the Avonlea master plan, the site plan is located at the corner of NE Dixie Hwy and Cardinal Ave. One building of about 6,827 SF is proposed at the corner of the property with direct engagement of the street. An outdoor covered seating area is proposed on the rear of the building to take advantage of views of the neighboring wetland area. Similar to other commercial projects by the applicant, a mixture of uses is envisioned for the building. Parking is provided on site for the proposed building.

This parcel will fill a gap in services for the surrounding properties. Supporting the local community and providing convenience are hallmarks of successful commercial developments and is one example of how these two proposed site plans enhance the area and the Avonlea development.

Site Circulation:

As mentioned, the subject parcel is located at a prominent and existing street intersection. Where possible, internal connections to surrounding parcels have been identified on the site plans. Safe and efficient means of access are shown for the parcel. The parcel does present challenges to site plan due to odd configuration, but ultimately the site design overcomes these obstacles through the utilization of some urban principals and logical sustainable design.

Parking:

The proposed site plans provide parking on site to support the proposed building and uses. The parking area is designed with a logical progression of traffic as vehicles enter the subject site. The site plan includes street parking consistent with the approved Avonlea Offsite Improvement plan. Parking dimensions are provided consistent with the City of Stuart design requirements.

Landscaping:

The provided area for landscaping and open space exceeds the minimum requirements and provides the opportunity for the integration of landscape design with site design as well as with the building design. The integration of the natural environment is an important piece to the creation of a sustainable development and also for maximizing the success of the proposed development. When considering the open space of these development sites, staff and the city must consider the open spaces set aside by the New Avonlea PUD which totals over 20 acres more than 40% of the total project area.

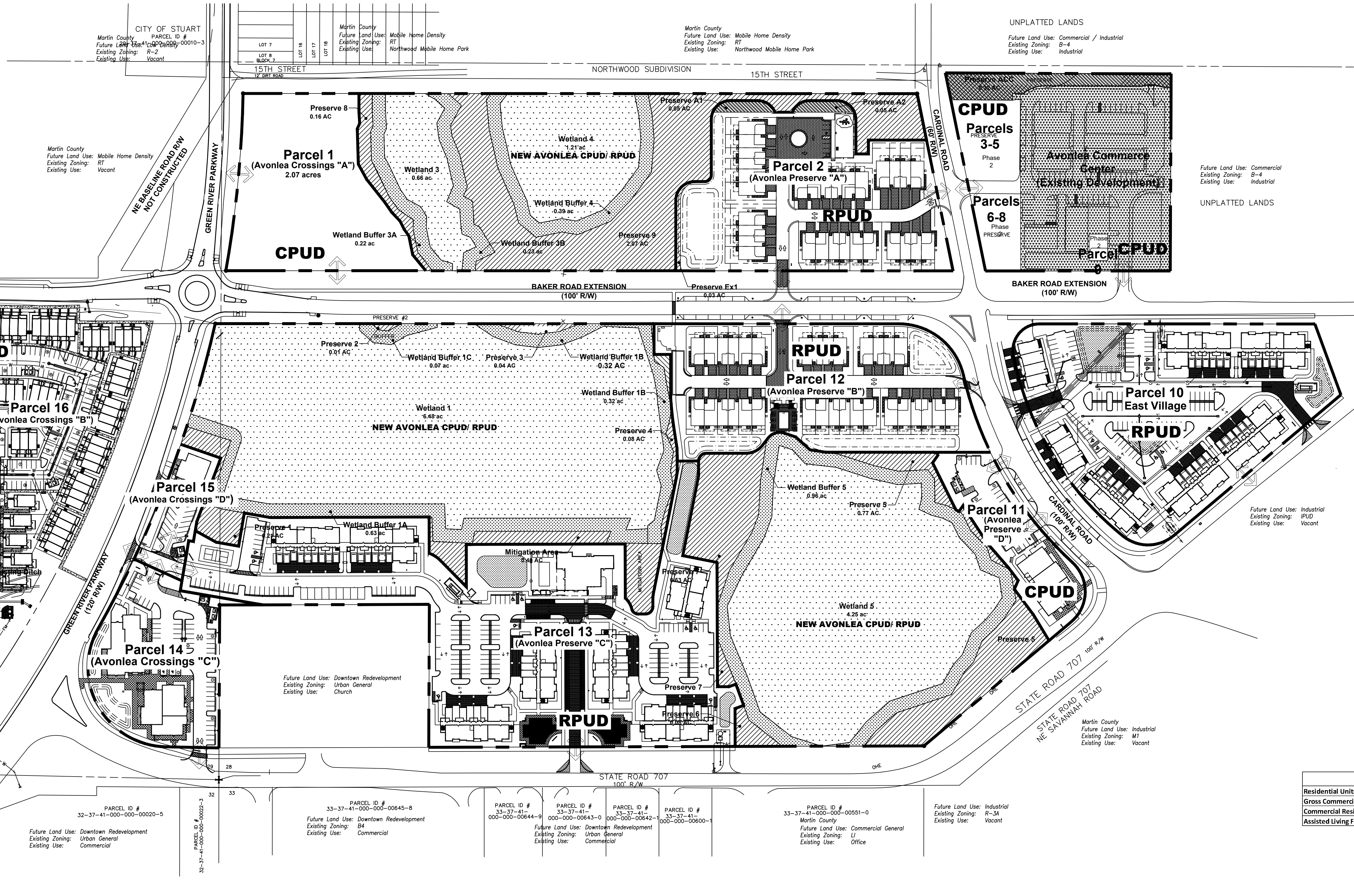
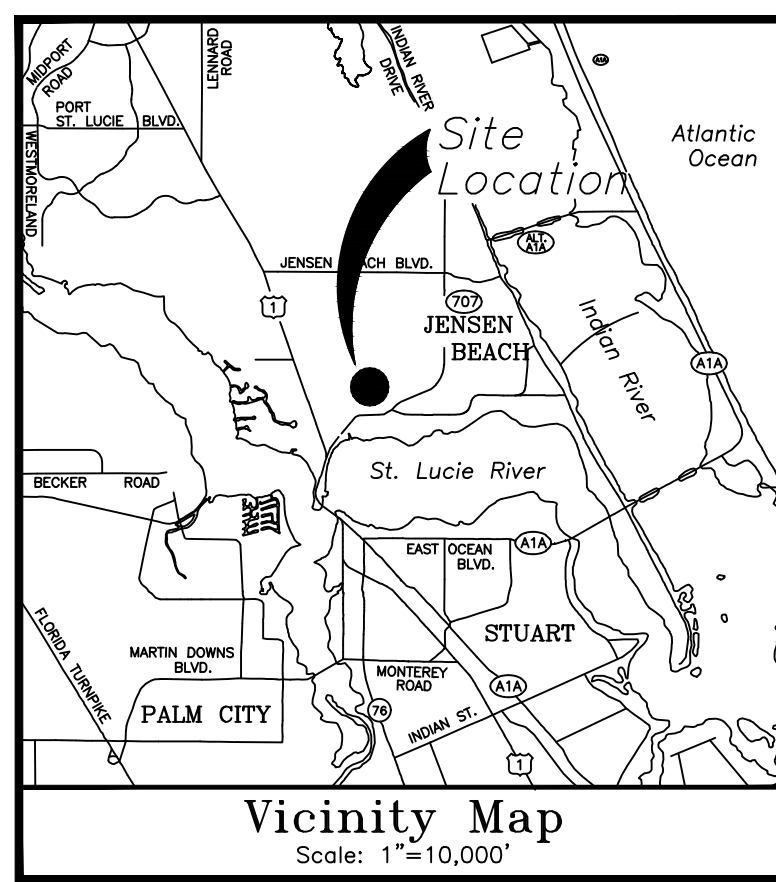
Tree disposition plans have been created for both parcels of land. Consistent with the City of Stuart Policy the Applicant proposes to balance the mitigation requirements across the parcel. The existing trees on the subject parcel have a variety of failing conditions that will ultimately keep them from being relocated and significantly reduce their survival rating if attempted. Additionally, the aesthetics of the existing trees do not lend themselves to being good relocated trees. The Applicant proposes to provide onsite mitigation for the removed trees.

Architecture:

The architecture of the building pertains directly to the Florida vernacular and is consistent with the surrounding area and historical context of Stuart. However, it also brings improved building methods and understated elegance to complement the new residential developments.

Conclusion:

The property owner proposes to construct a building on a commercial corner within the Avonlea Master Plan. The parcels total approximately 0.90 acres, and the total building area is approximately 6,827 square feet. The building has been positioned to engage the street corner to provide anchors for the prominent street intersection. The building uses are intended to provide convenience to the residential units currently under construction in the way of personal services, restaurants, office, retail, and other uses. The proposed site plan centers around sustainable design principles and the utilization and respect of the outdoors. The timing of the project is predicated upon the completion and stabilization of the six residential projects surrounding these sites.



Preservation Data:		
	Acres	%
Total Site Area	49.04 ac	100.0%
Created Upland	5.28 ac	10.8%
Proposed Wetland Preserve	12.60 ac	25.7%
Existing Native Upland Habitat	31.16 ac	63.5%
Required Upland Preserve Area (25% of Existing Native Upland Habitat)	7.79 ac	
Proposed Upland Preserve	7.79 ac	
Total Preserve Area (Includes Wetland and Upland Preserves)	20.40 ac	41.6%
Total Development Area	28.64 ac	58.4%
Total Site Area:	49.04 ac	100%

Open Space Calculations:*		Acres
Total Site Area:		49.04 ac
(Less) Wetlands:		12.60 ac
Area Subject To Open Space Requirement:		36.44 ac
Minimum Open Space Required (30%):		10.93 ac
Open Space Provided By Upland Preserve:		7.79 ac
Open Space To Be Provided Within Development:		3.14 ac
Open Space To Be Provided In RPUD:		1.48 ac
Open Space To Be Provided In CPUD:		1.66 ac
Total Open Space Provided:		10.93 ac

General Notes
 -Sidewalks along Cardinal Road shall be a minimum of 8'
 -Sidewalks along Baker Road shall be a minimum of 6' and have a minimum average width of 8' for Avonlea East Village. Street front plazas and divided sidewalks may count towards this requirement.

Legal Description
 See Exhibit 'A' in PUD Agreement

Legend

- Existing Wetlands under South Florida Water Management Conservation Easement
- Existing Wetland Buffer under South Florida Water Management Conservation Easement
- Existing Upland Preserve under South Florida Water Management Conservation Easement
- Proposed Upland Preserve
- Existing Development

Arrows represent conceptual driveway connections and interconnectivity between parcels.

Maximum Density per CPUD and RPUD	
Residential Units	325 units / 6.62 upa
Gross Commercial Floor Area	117,000
Commercial Residential Units/Live Work	19
Assisted Living Facility	200 units

RPUD	Parcel	Site Area	Maximum Building Coverage	Developable Area	Max. Impervious	Maximum Density	Maximum Units	Maximum ALF Units	Permitted Uses:
	Avonlea Preserve A (Complete)	2	3.11 ac	2.98 ac	75%	15 upa	30 units		200 Units Combined w/ CPUD Multifamily, Attached, Single Family, ALF, Commercial Residential/ Live Work
	Avonlea Preserve B (Complete)	12	3.07 ac	3.07 ac	75%	15 upa	32 units		
	Avonlea Preserve C	13	6.35 ac	6.35 ac	75%	19.37 upa	123 units		
	Avonlea Crossings B (Complete)	16	4.22 ac	4.16 ac	75%	16.35 upa	69 units		
	Total		16.75 ac	16.56 ac		15.16 upa	254 units		

Notes:
 (1) The development mix in the CPUD and RPUD meet the minimum 10% commercial and 30% residential of the Neighborhood Special District land use.
 (2) Maximum building coverage within the RPUD shall not exceed 30% of the total site area of the RPUD including a pro-rata share of the preserve area (43.3%)

CPUD	Parcel	Site Area	Developable Area	Maximum Gross Commercial Floor Area (2)	Max. Commercial Residential/Live Work Units	Maximum ALF Units
	Avonlea Commerce Phase 1	N/A	2.20 ac	23,700 sf (Completed)	5 units (completed)	
	Avonlea Commerce Phase 2	3-9	0.79 ac	33,300 sf	5 units	
	Avonlea Crossings A	1	2.07 ac	20,000 sf	Not Applicable	
	Avonlea Crossings C	14	1.60 ac	20,000 sf	Not Applicable	
	Avonlea Crossings D	15	0.45 ac	5,000 sf	Not Applicable	
	Avonlea Preserve D	11	0.90 ac	15,000 sf	Not Applicable	
	East Village	10	4.07 ac	0 sf	9 units	
	Total		12.08 ac	117,000 sf	19 units	

(1) The development mix in the CPUD and RPUD meet the minimum 10% commercial and 30% residential of the Neighborhood Special District land use.
 (2) Allowable commercial uses listed in Avonlea PUD Agreement.

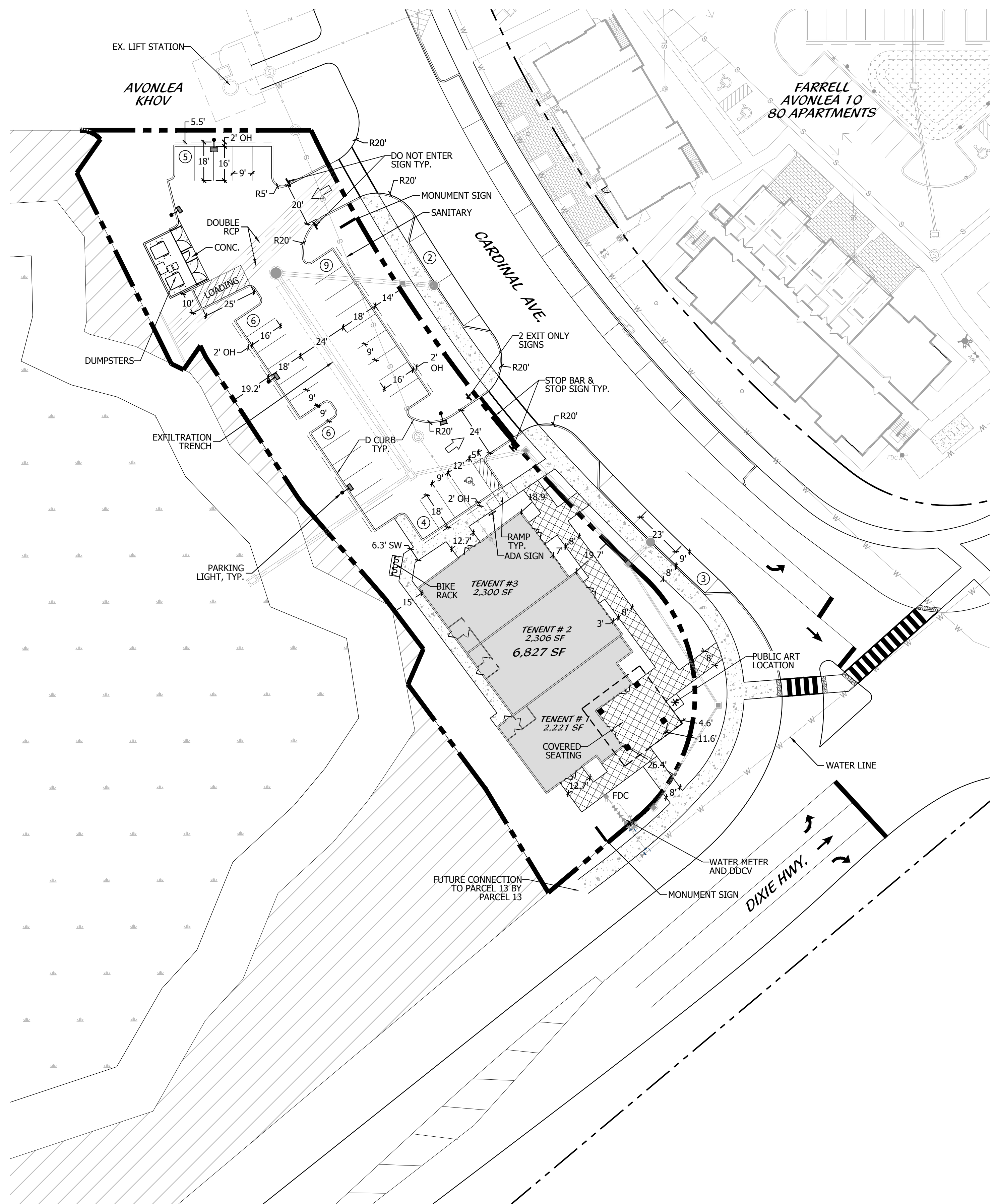
SCALE
 0 50' 100' 200'

North

New Avonlea PUD

Master Parcel Plan

City of Stuart, Martin County, Florida



SITE DATA

NAME OF PROJECT AVONLEA LOT 11
 PROPERTY CONTROL NUMBER 28-37-41-013-000-00110-0

FEMA FLOOD ZONE X
 LAND USE DESIGNATION NEIGHBORHOOD/SPECIAL
 ZONING COMMERCIAL PLANNED UNIT
 OVERLAY DISTRICT COMMUNITY REDEVELOPMENT AREA

MAXIMUM BUILDING HEIGHT 35'
 NUMBER OF STORIES 1
 NUMBER OF BUILDINGS 1

PROPOSED USES RETAIL SALES AND SERVICE
 OFFICE (PROFESSIONAL, MEDICAL)
 RESTAURANT (GENERAL) 80 SEATS

TOTAL SITE AREA 0.90 AC
 39,049.97 SF

BUILDING DATA
 OFFICE/RETAIL 4,500 SF
 RESTAURANT 2,327 SF
 GROSS SQUARE FOOTAGE 6,827 SF
TOTAL SQUARE FOOTAGE 6,827 SF

FAR 1.00

LAND USE

	SF	AC	%
BUILDING LOT COVERAGE	6,827	0.16	17.48%
VEHICULAR USE AREA	12,379	0.28	31.70%
OPEN SPACE	19,844	0.46	50.82%
TOTAL	39,050	0.90	100.00%

LOT COVERAGE

	SF	AC	%
BUILDING LOT COVERAGE	6,827	0.16	17.48%
VEHICULAR USE AREA	12,379	0.28	31.70%
SIDEWALKS & HARDSCAPE	3,894	0.09	9.97%
OUTDOOR SEATING (INCLUDED IN HARDSCAPE)	1,120		
TOTAL IMPERVIOUS AREA	23,100	0.53	59.15%

PERVIOUS AREA

	SF	AC	%
GREEN SPACE	15,950	0.37	40.85%
TOTAL PERVIOUS AREA	15,950	0.37	40.85%

TOTAL SITE AREA 39,050 0.90 100.00%

PARKING DATA

	REQ	PROV
OFFICE/RETAIL (1/300 SF)	15	35
RESTAURANT, GENERAL (1/4 SEATS)	20	
TOTAL	35	35

HANDICAP SPACES (INCLUDED IN TOTAL) 1 1

GENERAL NOTES

REFUSE SERVICE SHALL BE ACCOMPLISHED BY WAY OF INDIVIDUAL DUMPSTER AND RECYCLING AS SHOWN ON THE SITE PLAN.

10' X 30' SAFE SIGHT TRIANGLE SHALL BE PROVIDED AT THE PROJECTS ENTRY ROADS.

TYPE D OR F RAISED CONCRETE CURBING SHALL BE PROVIDED AROUND ALL PLANTING ISLANDS WITHIN VEHICULAR USE AREAS.

ON STREET PARKING IS UTILIZED IN THE CALCULATION FOR REQUIRED PARKING IN ACCORDANCE WITH THE PROVISIONS OF THE MXD CODE.

RAMPS SHALL BE PROVIDED AT ALL PLAZA, SIDEWALK AND STREET INTERSECTIONS IN ACCORDANCE WITH ADA AND THE FLORIDA HANDICAP ACCESSIBILITY CODE.

ALL A/C UNITS SHALL BE LOCATED TO THE REAR OR SIDE OF ALL BUILDINGS AND FULLY SCREENED ON THREE SIDES WITH LANDSCAPING.

THE BUILDING, LANDSCAPE AND LIGHTING DESIGN SHALL MAXIMIZE THE USE OF OPTED DESIGN PRINCIPLES.

MINIMUM TYPE CONSTRUCTION FOR ALLOWABLE HEIGHT & BUILDING AREA MUST BE COMPLIANCE WITH THE FLORIDA BUILDING CODE T-500 (AFTER OCT. 1, 2003 FBC 2004).

ALL SIGNAGE SHALL CONFORM TO THE REQUIREMENTS OF THE TOWN OF JUPITER LDR'S.

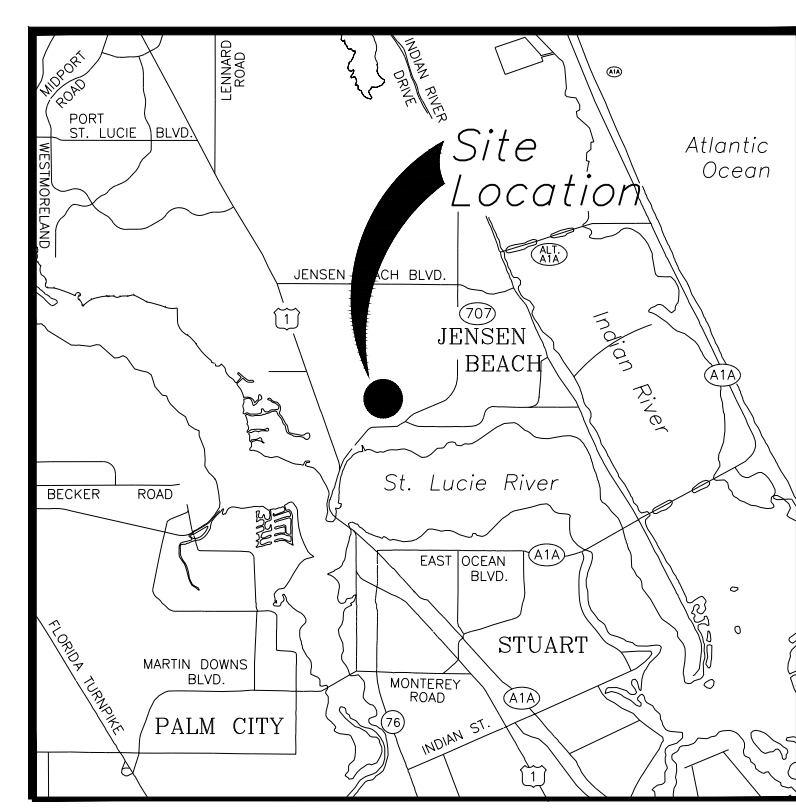
"PEDESTRIAN XING" SIGNS SHALL BE PROVIDED FOR ALL MID-BLOCK CROSSINGS.

PROJECT ID SIGNS SHALL BE ILLUMINATED WITH GROUND MOUNTED FIXTURES ON PHOTOCELLS.

THE PHOTOMETRIC PLAN REFLECTS MAINTAINED LIGHTING VALUES IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN OF JUPITER LDR'S.

STREET LIGHTS TO BE FPL COACH LIGHTS MOUNTED ON 12' TALL POLES. LIGHT LEVELS SHALL NOT EXCEED THE MAXIMUM TO MINIMUM ESTABLISHED IN TABLE 1 OF ORDINANCE NO. 66-98.

LOCATION MAP



KEY MAP



PROJECT TEAM

OWNER/CLIENT:
 AVONLEA COMMERCIAL CORNERS, LLC
 100 SW ALBANY AVENUE, SUITE 200
 STUART, FL 34994
 772.286.6292
 CONTACT: NIKOLAUS SCHROTH

CIVIL ENGINEER:
 VELCON GROUP
 702 SW PORT ST. LUCIE BOULEVARD
 PORT ST. LUCIS, FL 34953
 772.879.0477
 CONTACT: BOB KEMMERSON, PE

LANDSCAPE ARCHITECT/PLANNER:
 COTLEUR & HEARING, INC.
 1934 COMMERCE LANE, SUITE 1
 JUPITER, FL 33458
 561.747.6336
 CONTACT: DON HEARING

SURVEYOR:
 VELCON GROUP
 702 SW PORT ST. LUCIE BOULEVARD
 PORT ST. LUCIS, FL 34953
 772.879.0477
 CONTACT: DARREN GUETTLER

ARCHITECT:
 TI ARCHITECTS
 3000 HIGH RIDGE ROAD, BAY 4
 BOYNTON BEACH, FL 33426
 561.860.2905
 CONTACT: HAROLD TUTTLE AIA

TRAFFIC CONSULTANT:
 MACKENZIE ENGINEERING AND PLANNING, INC.
 1172 SW 30TH STREET, SUITE 500
 PALM CITY, FL 34990
 772.286.8030
 CONTACT: SHAUN G. MACKENZIE, PE

LEGEND

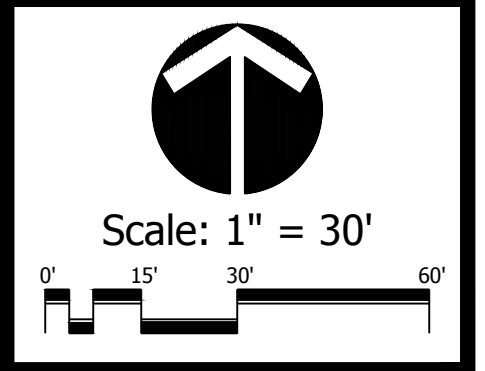
ADA	AMERICANS WITH DISABILITIES ACT	HC SIGN
LB	LANDSCAPE BUFFER	STOP SIGN
R	RADIUS	DO NOT ENTER
SB	SETBACK	PARKING LIGHT
SW	SIDEWALK	
TYP	TYPICAL	

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 Landscape Architects
 Land Planners
 Environmental Consultants
 1934 Commerce Lane
 Suite 1
 Jupiter, Florida 33458
 561.747.6336 · Fax 747.1377
 www.cotleurhearing.com
 Lic# LC-26000535

Avonlea Commercial Corners

Lot 11 Site Plan
 Stuart, Florida

DESIGNED	DEH
DRAWN	RO
APPROVED	DEH
JOB NUMBER	21-0401
DATE	08-29-22
REVISIONS	11-11-22
	03-01-24
	04-21-25

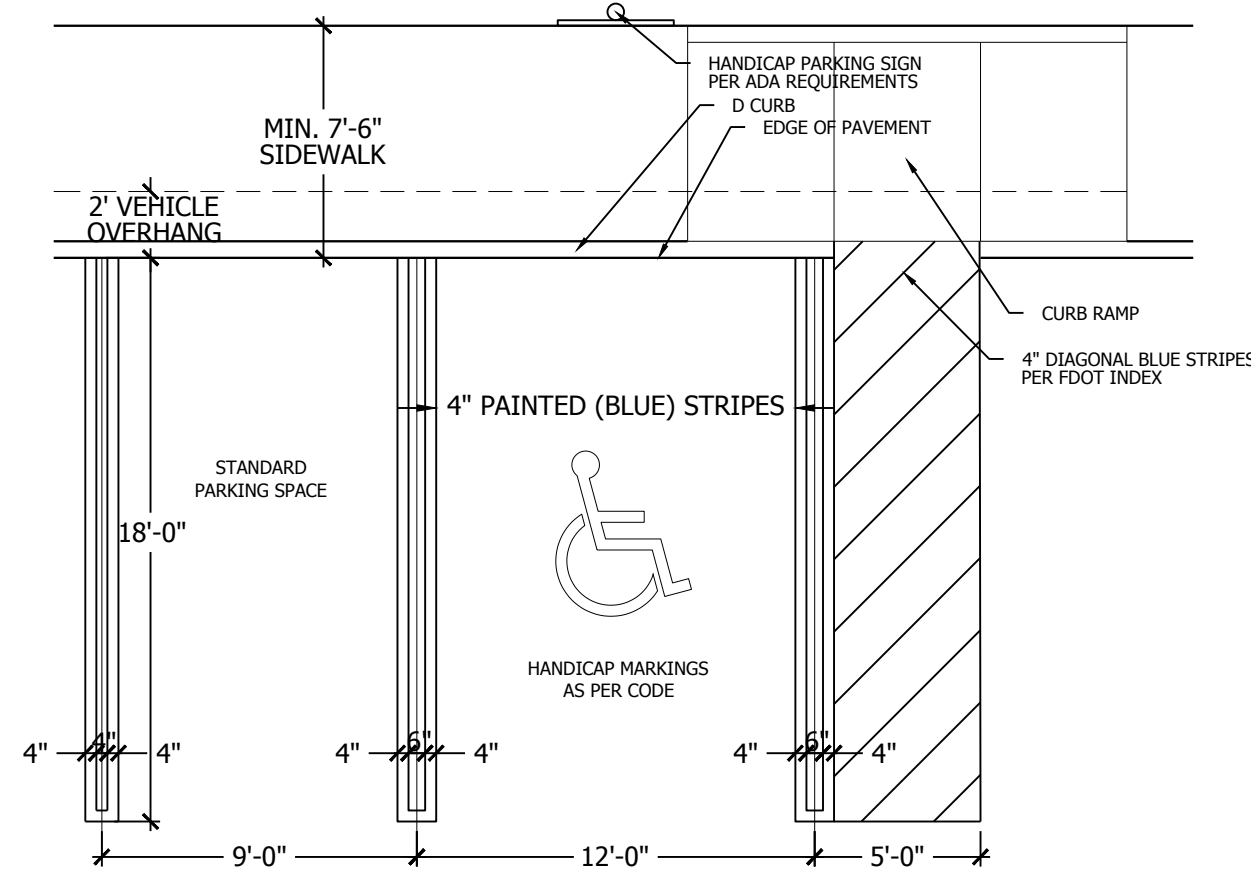


April 21, 2025 7:50:48 a.m.
 Drawing: 21-0401 SP.DWG

SHEET 1 OF 2
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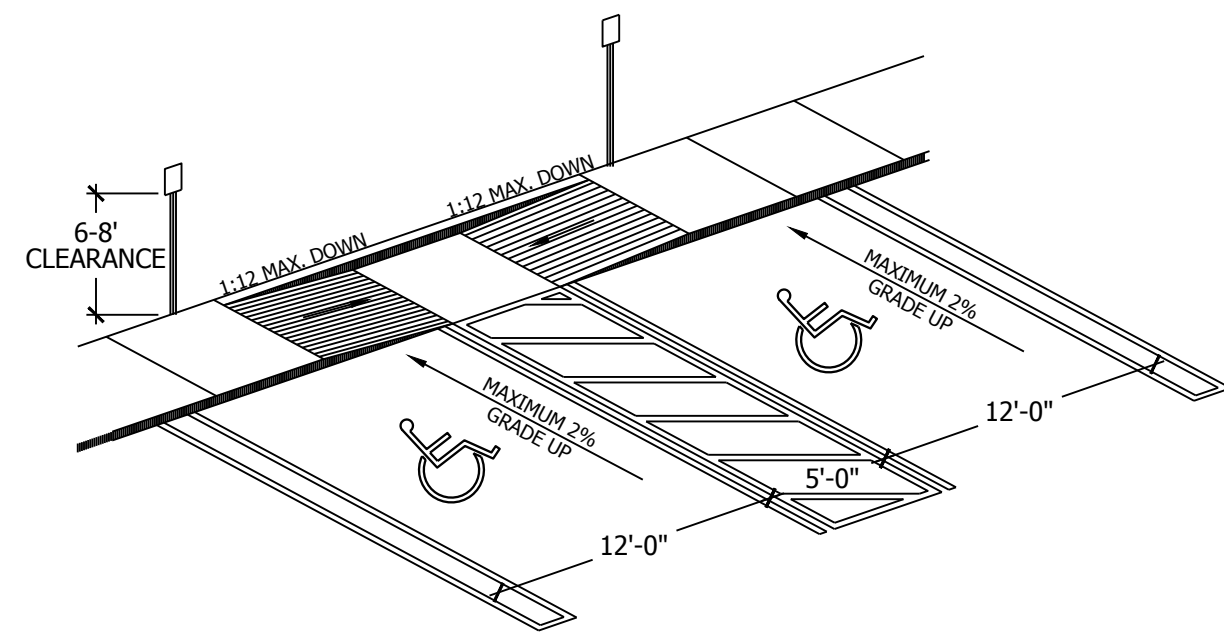
STANDARD AND HANDICAP PARKING DETAIL

NTS



HANDICAP RAMP DETAIL

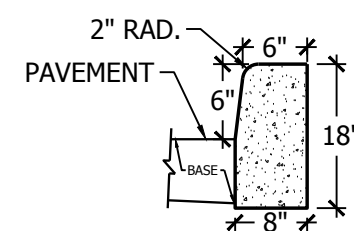
NTS



NOTE: IN THIS APPLICATION RAMPS CAN BE SEPARATED AND SIDEWALK CAN BE FLUSH ACROSS ALL HC SPACES
NOTE: HCR#3-MOD MEANS ONLY 1 RAMP BUILT TO REQUIRED SIDE OF SIDEWALK. OR RAMPS SEP. TO ENDS OF HC SPACES

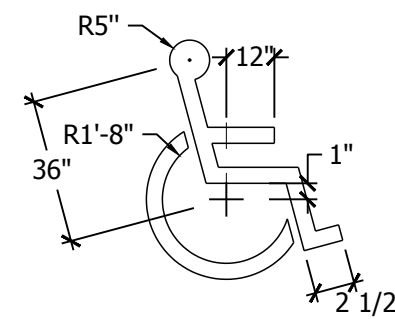
D CURB DETAIL

NTS



HANDICAP SYMBOL DETAIL

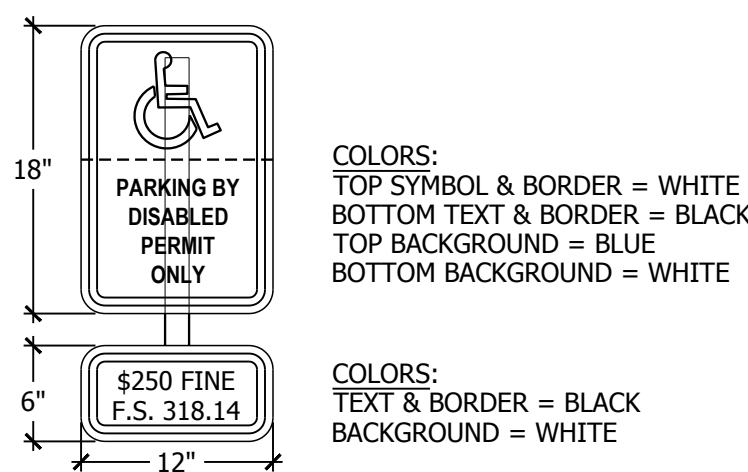
NTS



NOTE: SYMBOL TO BE 4" WIDE WHITE TRAFFIC PAINT ON PAVEMENT OR COLORED PAVERS. TYPICAL @ ALL HANDICAP STALLS

HANDICAP SIGN DETAIL

NTS

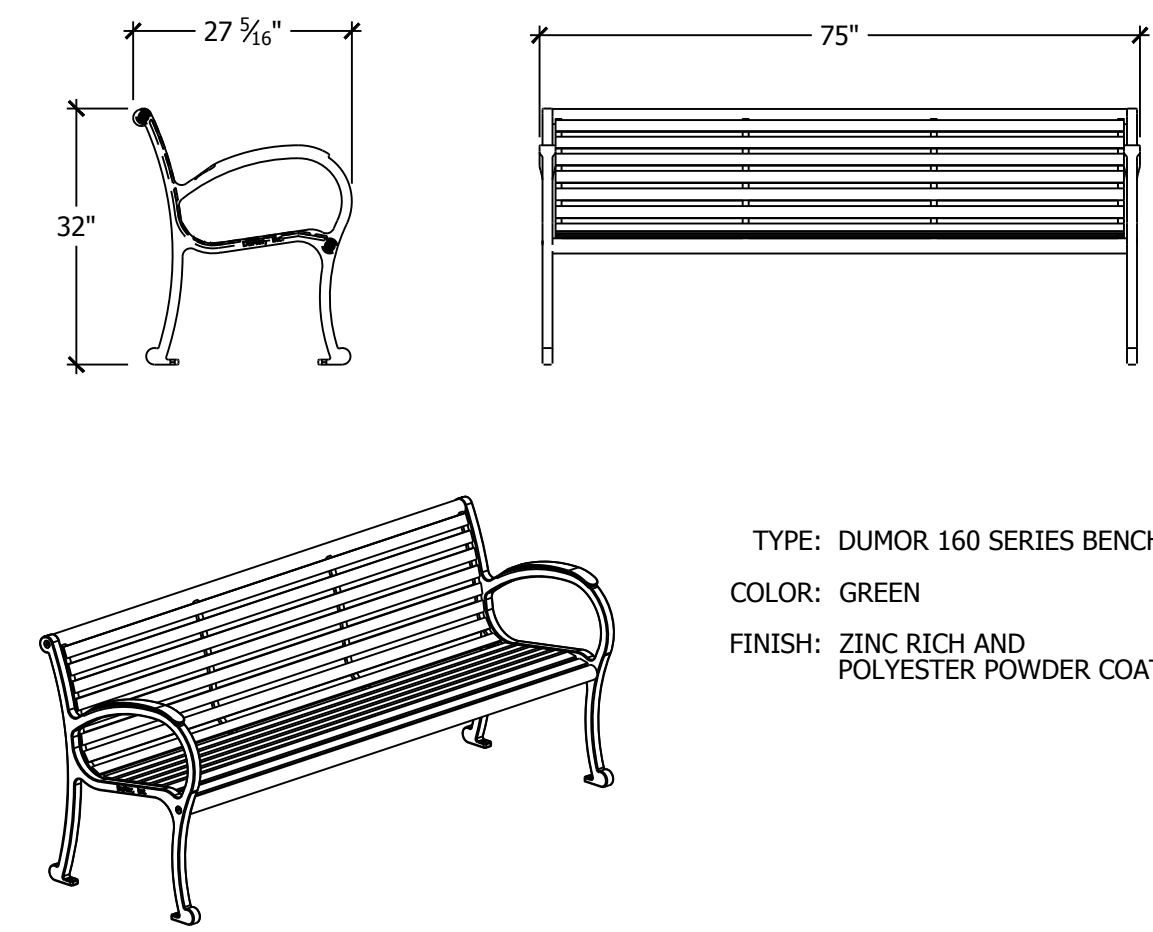


COLORS:
TOP SYMBOL & BORDER = WHITE
BOTTOM TEXT & BORDER = BLACK
TOP BACKGROUND = BLUE
BOTTOM BACKGROUND = WHITE

COLORS:
TEXT & BORDER = BLACK
BACKGROUND = WHITE

BENCH DETAIL

NTS



TYPE: DUMOR 160 SERIES BENCH
COLOR: GREEN
FINISH: ZINC RICH AND POLYESTER POWDER COAT

BIKE RACK DETAIL

NTS



LIGHTING DETAIL

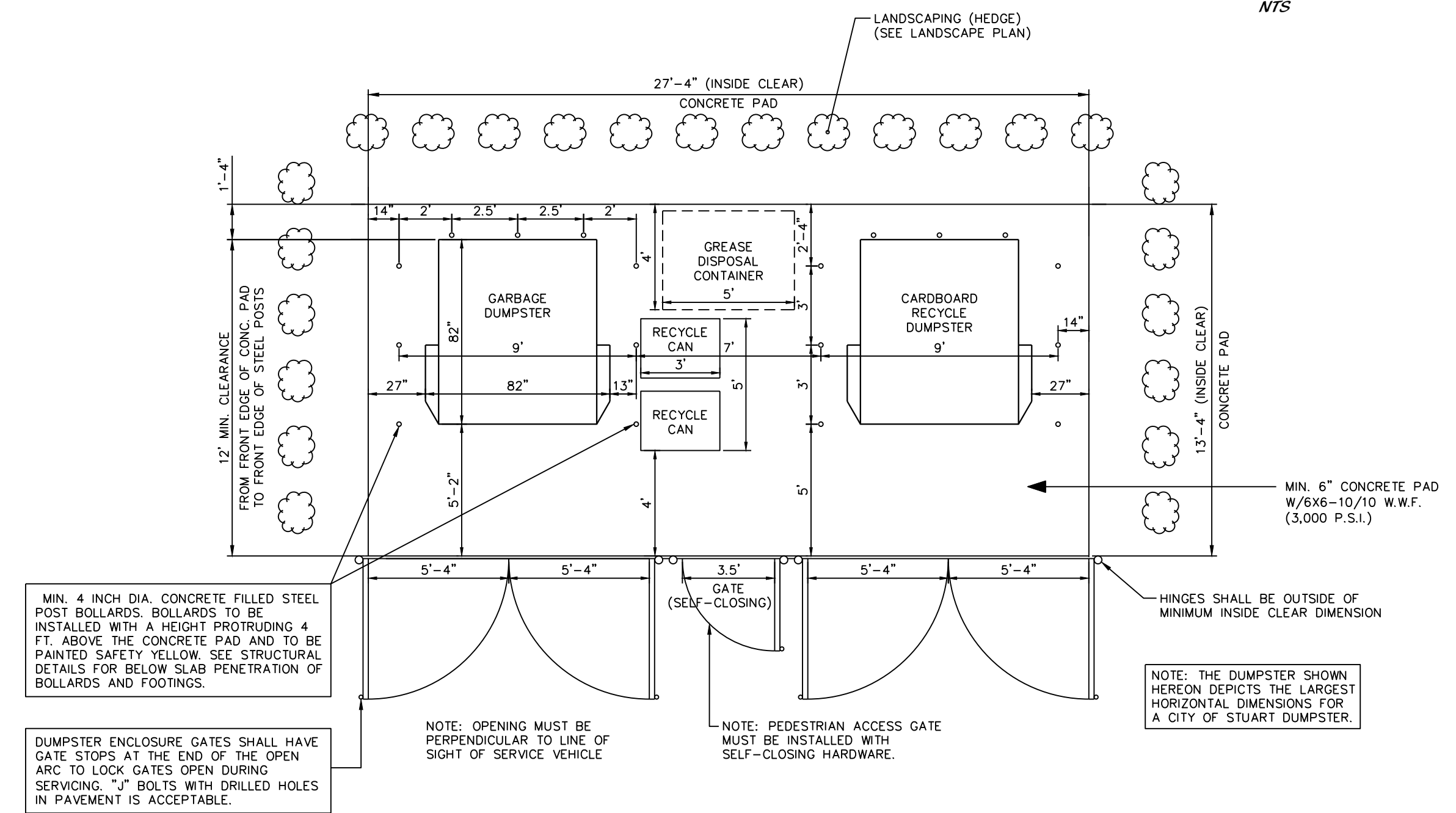
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CREE LIGHTING
OSQ SERIES
DA MOUNT

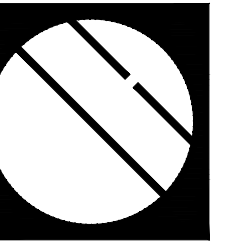
DUMPSTER DETAIL

NTS



CITY OF STUART DUMPSTER ENCLOSURE - SITE DETAIL
DOUBLE DUMPSTER w/ RECYCLING & GREASE DISPOSAL CONTAINER
N.T.S.

ILLUSTRATION #17

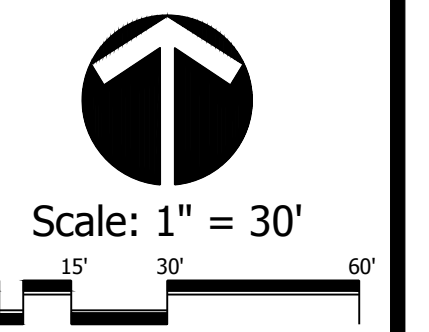


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Avonlea Commercial Corners
Lot 11 Site Plan
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April 21, 2025 7:50:48 a.m.
Drawing: 21-0401 SP.DWG

SHEET 2 OF 2
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Site Details



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 ARCHITECT
 301 W. ATLANTIC AVE.
 SUITE 06
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 PHONE 518.376.6381
 www.drany.com

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REVISIONS		
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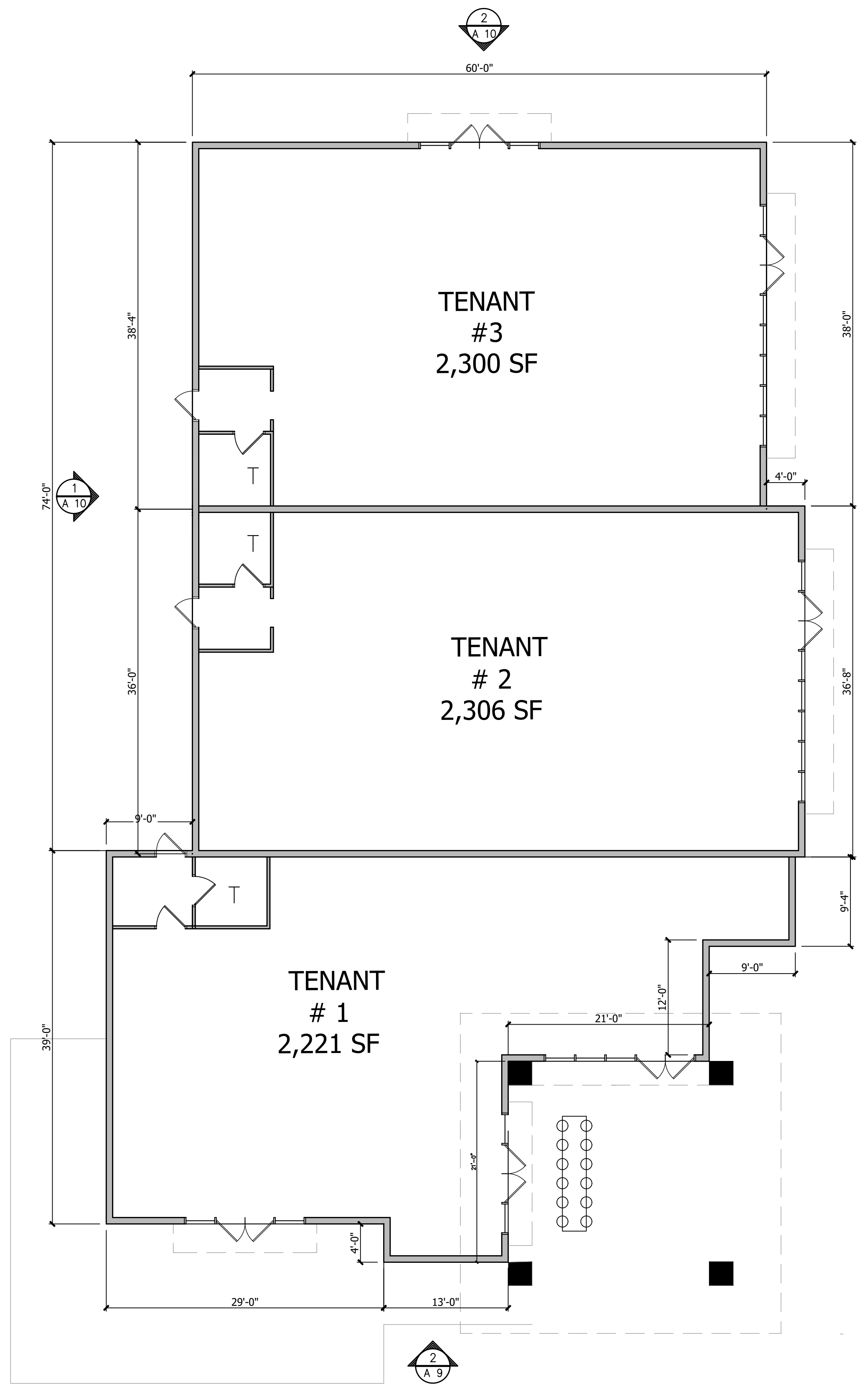
PROGRESS SET
AUGUST 8, 2022

OWNERS
DON HEARING & NIKOLAUS SCHROTH

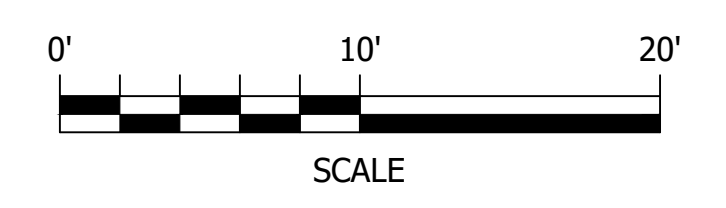
AVONLEA COMMERCIAL CORNERS
 LOTS 14 & 15
 DIXIE HIGHWAY & GREEN RIVER PARKWAY
 CITY OF STUART, FLORIDA

BUILDING PLANS
EAST SITE

A2

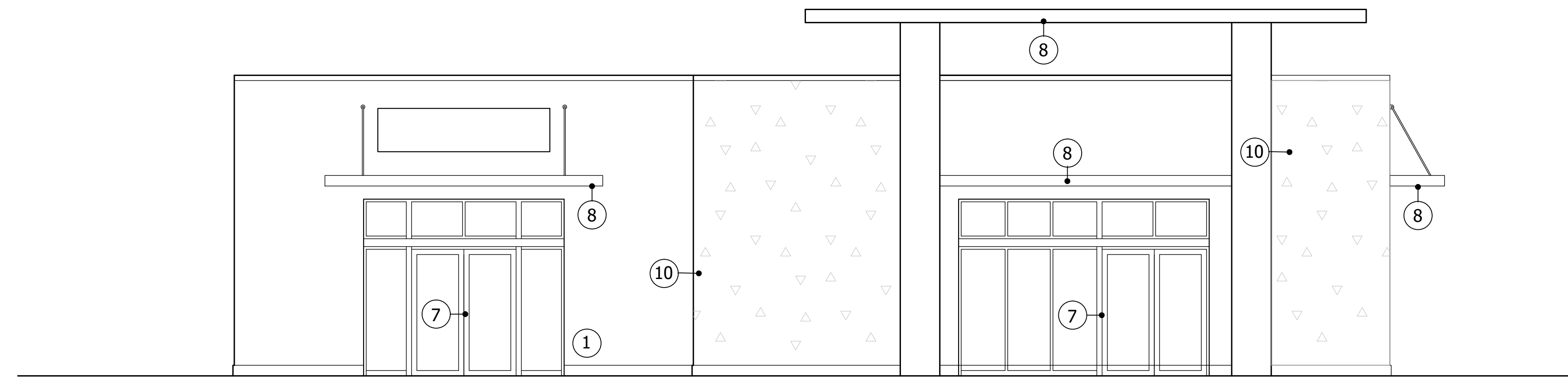


1 EAST SITE - BUILDING PLAN
 6,827 GROSS SF SCALE: 1/8" = 1'-0"

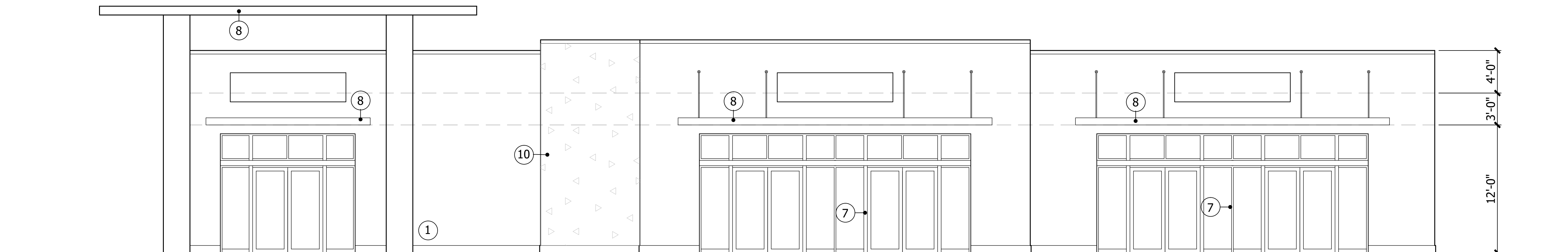


EXTERIOR BUILDING MATERIALS : COLORS

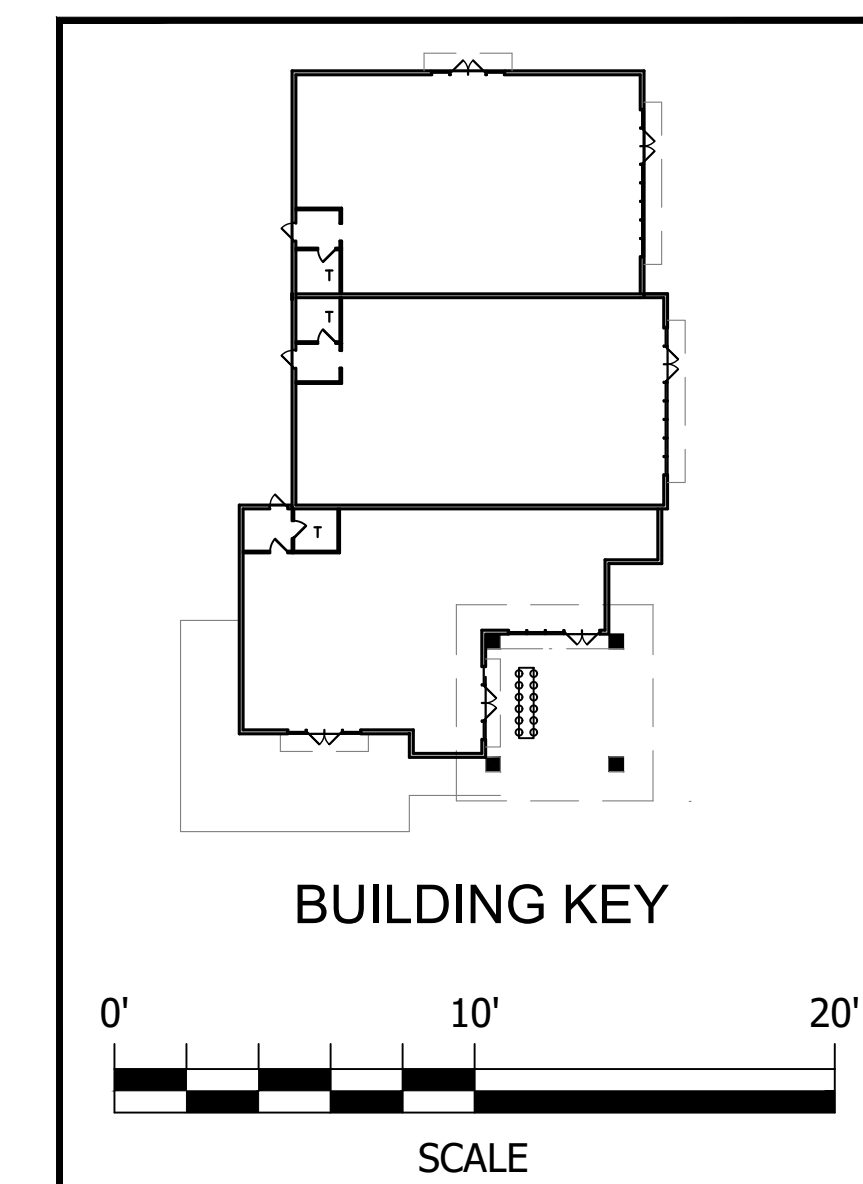
- ① STUCCO : CREAM
- ② STUCCO : SAND
- ③ METAL ROOF : BRONZE
- ④ ALUMINUM RAILINGS : BRONZE
- ⑤ CANVAS AWNING : WHITE
- ⑥ WOOD COLUMNS, BEAMS & BRACKETS : BROWN
- ⑦ ALUMINUM STOREFRONT WINDOWS & DOORS : BRONZE
- ⑧ METAL AWNING & CANOPY : BRONZE
- ⑨ TILE ON FOUNTAIN WALL
- ⑩ MURALS



② SIDE ELEVATION (DIXIE HWY)
A 9 SCALE : 3/16" = 1'-0"



① FRONT ELEVATION (CARDINAL AVE)
A 9 SCALE : 3/16" = 1'-0"



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AUGUST 8, 2022

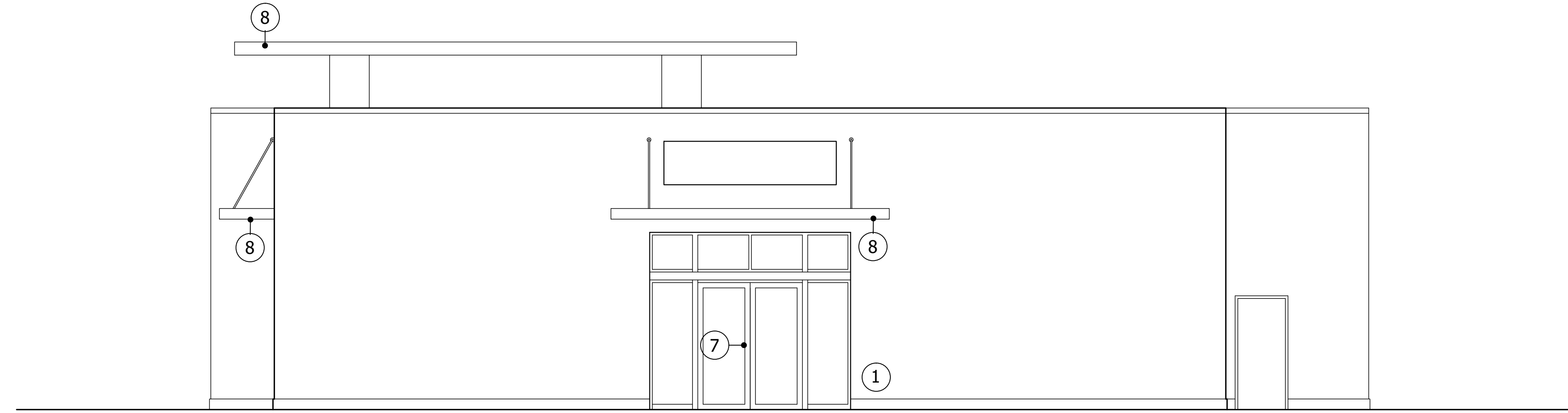
OWNERS
DON HEARING
&
NIKOLAUS SCHROTH

AVONLEA COMMERCIAL CORNERS
LOTS 14 & 15
DIXIE HIGHWAY &
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CITY OF STUART,
FLORIDA

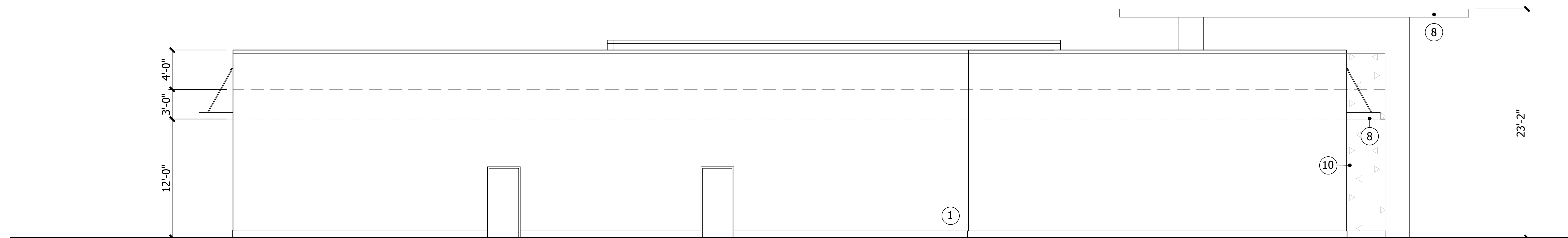
EXTERIOR ELEVATIONS
EAST SITE
A9

EXTERIOR BUILDING MATERIALS : COLORS

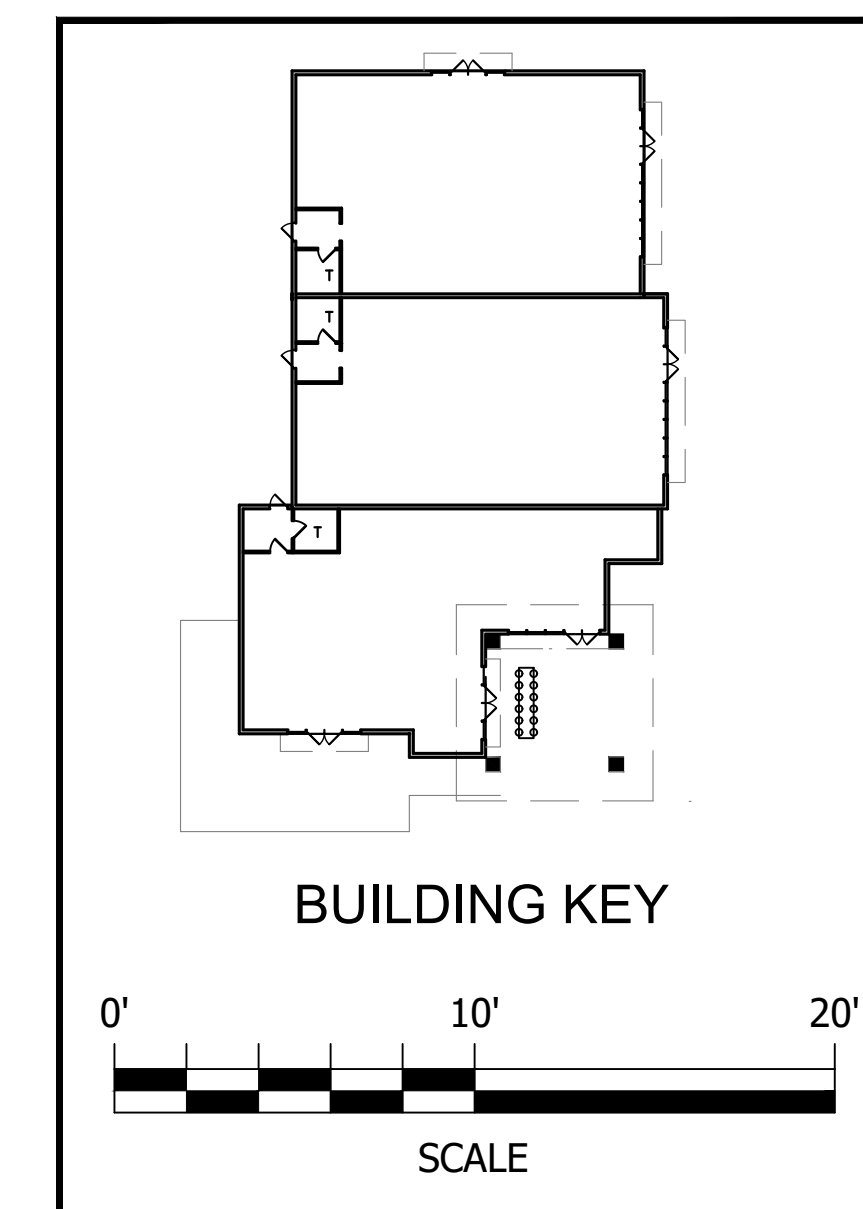
- ① STUCCO : CREAM
- ② STUCCO : SAND
- ③ METAL ROOF : BRONZE
- ④ ALUMINUM RAILINGS : BRONZE
- ⑤ CANVAS AWNING : WHITE
- ⑥ WOOD COLUMNS, BEAMS & BRACKETS : BROWN
- ⑦ ALUMINUM STOREFRONT WINDOWS & DOORS : BRONZE
- ⑧ METAL AWNING & CANOPY : BRONZE
- ⑨ TILE ON FOUNTAIN WALL
- ⑩ MURALS



②
A 10 SIDE ELEVATION (PARKING LOT)
SCALE : 3/16" = 1'-0"



①
A 10 REAR ELEVATION
SCALE : 3/16" = 1'-0"



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DRN.BY: DR	CHKD.BY: DR	

PROGRESS SET
AUGUST 8, 2022

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&
NIKOLAUS SCHROTH

AVONLEA COMMERCIAL CORNERS
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DIXIE HIGHWAY &
GREEN RIVER PARKWAY
CITY OF STUART,
FLORIDA

EXTERIOR ELEVATIONS EAST SITE
A10



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MARK	DESCRIPTION	DATE
DRN.BY:DR	CHKD.BY:DR	

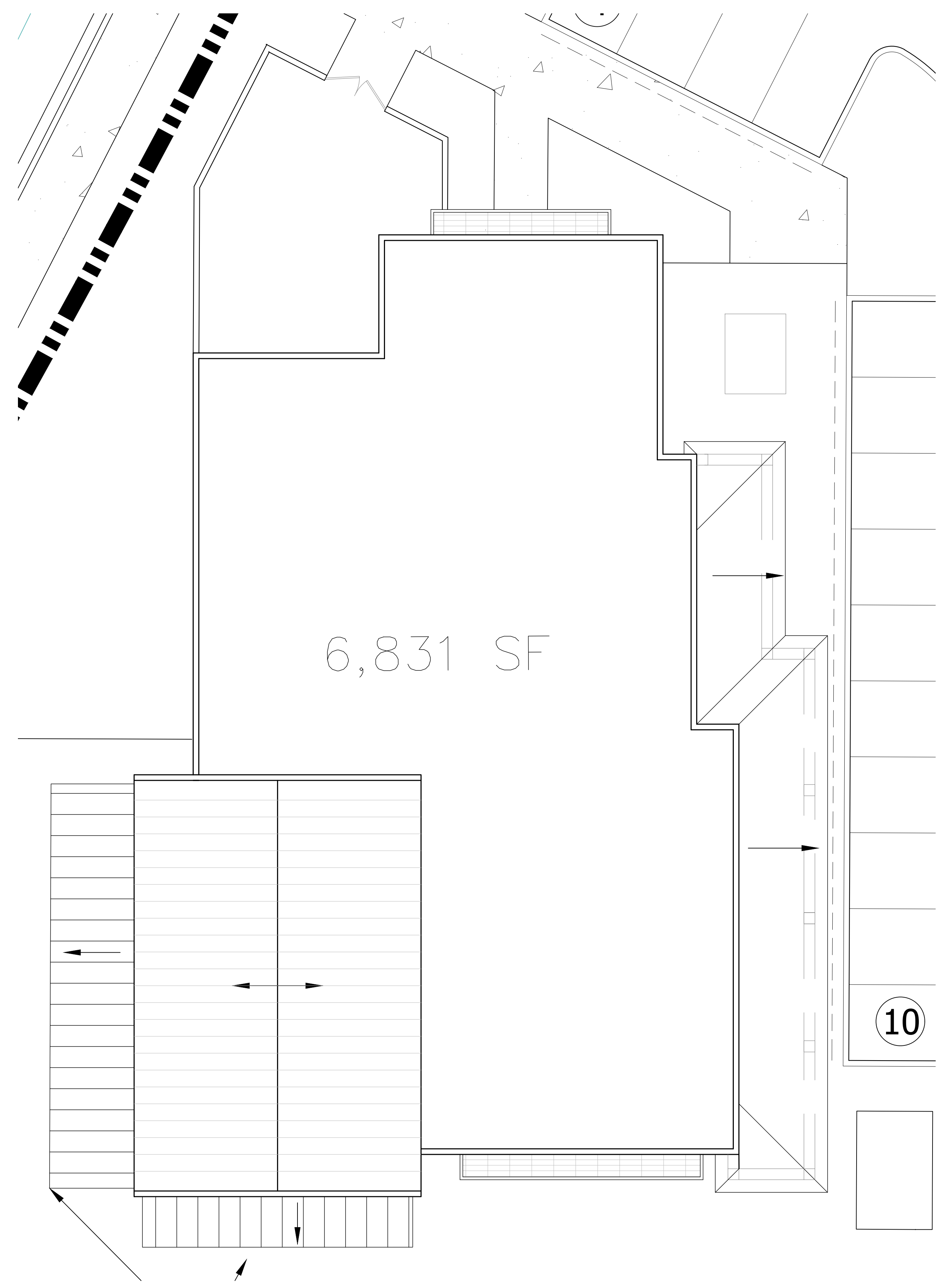
PROGRESS SET
AUGUST 8, 2022

OWNERS
DON HEARING & NIKOLAUS SCHROTH

AVONLEA COMMERCIAL CORNERS
LOTS 14 & 15
DIXIE HIGHWAY & GREEN RIVER PARKWAY
CITY OF STUART, FLORIDA

ROOF PLAN EAST SITE

A13



1 EAST SITE - ROOF PLAN
 SCALE: 1/8"=1'-0"







REVISIONS:	DATE	BY	REASON

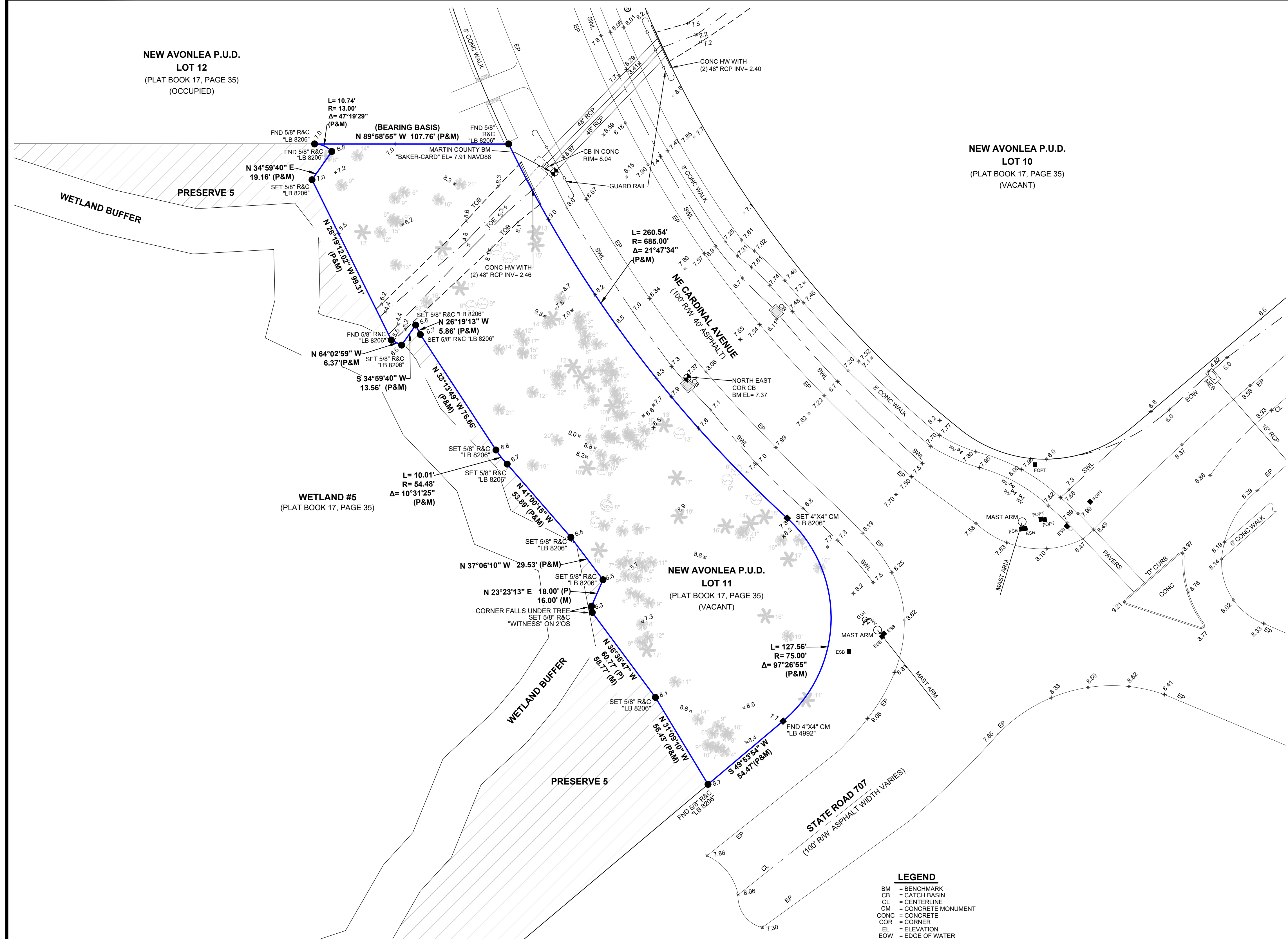
DATE	BY	REASON

**BOUNDARY & TOPOGRAPHICAL SURVEY
LOT 11, NEW AVONLEA P.U.D.
MARTIN COUNTY, FLORIDA**

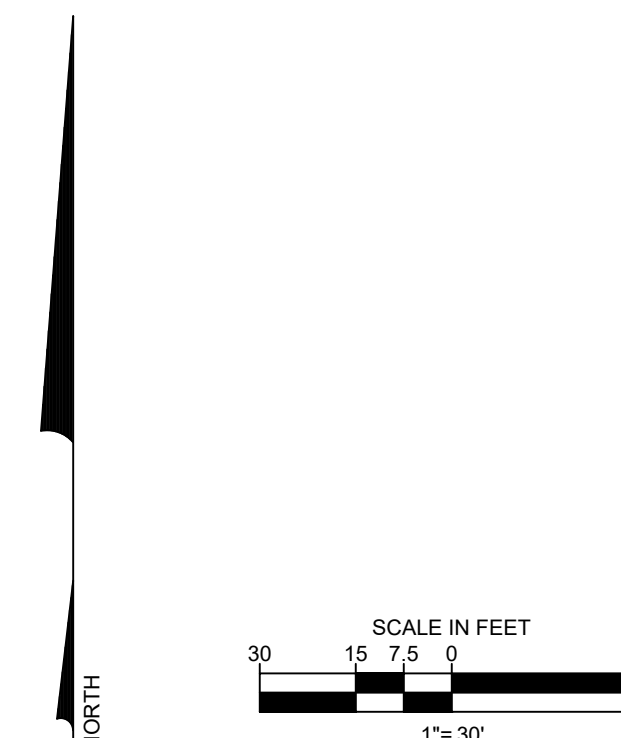
DRAWN BY:	GJS
APPROVED BY:	F. GUETTLER
SCALE:	1" = 30'
DATE:	09-26-25
FIELD BOOK/PAGES:	SURVEY MAP
JOB NUMBER:	20-2198
SHEET:	1 OF 1

LEGAL DESCRIPTION:
LOT 11, NEW AVONLEA P.U.D. ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 17, PAGE 35, PUBLIC RECORDS OF MARTIN COUNTY, FLORIDA.
ADDRESS: NE CARDINAL AVENUE & STATE ROAD 707
STUART, FLORIDA 34994

NOTES:
SUBJECT TO ANY APPLICABLE EASEMENTS, RIGHTS-OF-WAY, OR OTHER RESTRICTIONS OF RECORD.
A TITLE SEARCH OF THE PUBLIC RECORDS HAS NOT BEEN MADE BY THIS OFFICE.
BEARINGS SHOWN HEREON ARE BASED UPON A PLATTED MERIDIAN, ALONG THE NORTHERLY PROPERTY LINE OF LOT 11, HAVING A PLATTED BEARING OF N 89°58'55" W. ALL OTHER BEARINGS ARE RELATIVE THERETO.
LEGAL DESCRIPTION FROM FLORIDA WARRANTY DEED RECORDED IN OFFICIAL RECORD BOOK 3146, PAGE 210, PUBLIC RECORDS OF MARTIN COUNTY, FLORIDA.
ELEVATIONS ARE IN FEET AND RELATED TO THE MARTIN COUNTY BENCHMARK "BAKER-CARD" HAVING A PUBLISHED ELEVATION= 7.91 NAVD88 (NORTH AMERICAN VERTICAL DATUM.). ALL OTHER ELEVATIONS ARE RELATIVE THERETO.
ELEVATIONS SHOWN HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988. ELEVATIONS DEPICTED ON THIS SURVEY WERE OBTAINED USING REAL TIME KINEMATIC (RTK) GPS METHODS WITH AN EXPECTED ACCURACY OF PLUS OR MINUS 0.10 FEET.
PROPERTY LIES IN F.I.R.M. ZONE "X", AS PER MAP NUMBER 12085C0151H, DATED 02-19-20. FLOOD ZONES ARE APPROXIMATE AS SCALED FROM FLOOD INSURANCE RATE MAPS.
ADDITIONS OR DELETIONS TO SURVEY MAPS OR REPORTS BY OTHER THAN THE SIGNING PARTY, IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY.
UTILITIES SHOWN HEREON ARE VISIBLE ABOVE GROUND FEATURES. ADDITIONAL SUBSURFACE UTILITIES AND OR FEATURES MAY EXIST.
THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT SHOWN ON THIS SURVEY, THAT MAY BE FOUND IN THE PUBLIC RECORDS OF MARTIN COUNTY, FLORIDA.
THIS SURVEY IS NOT VALID WITHOUT THE ORIGINAL SIGNATURE AND SEAL OR ELECTRONIC SIGNATURE OF A FLORIDA LICENSED SURVEYOR AND MAPPER.
PUBLIC WATER & SEWER AVAILABLE.
THE EXPECTED USE OF THE SURVEY AND MAP IS COMMERCIAL.
ALL DISTANCES AND ELEVATIONS SHOWN ARE IN ACCORD WITH THE UNITED STATES STANDARD USING FEET.
ALL DIRECTIONAL MEASUREMENTS SHOWN ARE IN THE FORMAT OF DEGREES, MINUTES AND SECONDS.
LOT CONTAINS 39,050 SQUARE FEET OR 0.90 ACRES.
TREES DEPICTED ON THIS SURVEY WERE FIELD LOCATED BY DREW GATEWOOD, ADVANCED RESTORATION ECOLOGY.
DATE OF FIELD DATA ACQUISITION: BOUNDARY & TOPOGRAPHICAL SURVEY FIELD DATE: 01/12/22.
LAST DATE OF FIELD DATA ACQUISITION: UPDATE BOUNDARY & TOPOGRAPHICAL SURVEY FIELD DATE: 09/25/25.
FILE NUMBER: 20-2198. DATA COLLECTOR TSC-7 SERIAL NUMBER: DAD 233400484.

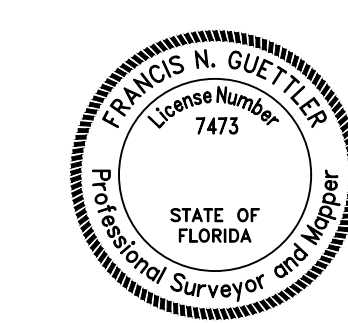


- LEGEND**
- BM = BENCHMARK
 - CB = CATCH BASIN
 - CL = CENTERLINE
 - CM = CONCRETE MONUMENT
 - CONC = CONCRETE
 - COR = CORNER
 - EL = ELEVATION
 - EDW = EDGE OF WATER
 - EP = EDGE OF PAVEMENT
 - ESB = ELECTRIC SERVICE BOX
 - FND = FOUND
 - FOPT = FIBER OPTIC
 - HW = HEAD WALL
 - HYD = FIRE HYDRANT
 - INV = INVERT
 - L = LENGTH
 - LB = LICENSE BUSINESS
 - (M) = MEASURED
 - MES = MITERED END SECTION
 - NAVD88 = NORTH AMERICAN VERTICAL DATUM 88
 - NO. = NUMBER
 - OS = OFFSET
 - (P) = PLAT
 - PSM = PROFESSIONAL SURVEYOR & MAPPER
 - P.U.D. = PLANNED UNIT DEVELOPMENT
 - (M) = MEASURED DISTANCE
 - R = RADIUS
 - R&C = IRON ROD & CAP
 - RW = RIGHT-OF-WAY
 - RCP = REINFORCED CONCRETE PIPE
 - (P) = PLAT DISTANCE
 - SWL = SWALE
 - TOB = TOP OF BANK
 - TOE = TOE OF SLOPE
 - WV = WATER VALVE
 - Δ = DELTA OF CURVE
 - *8.97 = SPOT ELEVATION
 - = PROPERTY LINE
 - = SITE BENCHMARK
 - = PINE TREE
 - = PALM TREE
 - = OAK TREE



SURVEYOR CERTIFICATION:
I HEREBY CERTIFY THAT THE SURVEY OF THE PROPERTY SHOWN AND DESCRIBED HEREON WAS COMPLETED UNDER MY DIRECTION AND SAID SURVEY IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. I FURTHER CERTIFY THAT THIS SURVEY MEETS THE MINIMUM TECHNICAL STANDARDS FOR SURVEYORS SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL SURVEYORS AND MAPPERS IN CHAPTER 5J-17, FLORIDA ADMINISTRATIVE CODE, PURSUANT TO SECTION 472.027 FLORIDA STATE STATUTES.

FRANCIS N. GUETTLER
PROFESSIONAL SURVEYOR AND MAPPER
STATE OF FLORIDA (PSM)#7473





To: Jodi Nentwick-Kugler
Director of Development
City of Stuart Development Department

From: Kyler Durham, P.L.A.
Kimley-Horn and Associates, Inc.

Date: October 23, 2025

Re: **Avalonea Commercial East CPUD – Stuart, Florida
Site Plan Review**

Dear Ms. Nentwick-Kugler

Kimley-Horn has reviewed the submittal of the Tree Disposition and Landscape Plan and associated documents, provided to Kimley-Horn on October 9, 2025, prepared by Cotleur & Hearing for the Avonlea Master Plan in Stuart, Florida. Please find below our review comments pertaining to the following items listed within the application:

Tree Disposition Plans:

- 1. Slash Pines 28 and 91 which are greater than 10 DBH did not get totaled into the mitigation calculations. Update and revised mitigation requirement table and fee as necessary.**
- 2. Trees 49 and 105 have a proximity to curb lines and proposed drainage structures that would be detrimental to the health of the tree. Suggest removing and/or relocating. Please update mitigation calculations as necessary.**

Landscape Plans:

- 1. Update second Landscape note to reflect city of Stuart requirements.**
- 2. Suggest removal of existing tree above, provide necessary replacements to meet onsite requirements in the VUA landscape areas.**
- 3. Irrigation plans to be submitted for review for all proposed landscape.**
- 4. Additional comments may apply once revisions have been made.**

Thank you for the opportunity to assist the City of Stuart in reviewing the noted project. By conducting this limited evaluation, Kimley-Horn is not assuming responsibility for the content and accuracy of the plans, which remain the sole responsibility of the Landscape Architect of Record. Please contact Kyler Durham at kylur.durham@kimley-horn.com, 772-621-2758 if you have any questions or need additional information.

Sincerely,

Kimley-Horn and Associates, Inc.



Kyler Durham, P.L.A.
Landscape Architect

TO: Jodi Kugler
Development Director | City of Stuart

FROM: George Missimer
Cotleur & Hearing

DATE: November 6th, 2025

SUBJECT: Avonlea Commercial Corners, Lot 11
Master PUD Amendment (Z22110007)

On behalf of the applicant, please accept this response letter as part of our CPUD Major Amendment resubmittal, addressing the comments received from Kimley-Horn in a letter dated October 23rd, 2025, for lot 11 of the Avonlea Commercial Corners project.

Tree Disposition Plan Review

1. Slash Pines 28 and 91 which are greater than 10 DBH did not get totaled into the mitigation calculations. Update and revise mitigation requirement table and fee as necessary.

Response: All trees evaluated by the project's certified arborist and environmental consultant as having a health rating of one (1) are dead trees and have been removed from the Tree Disposition Table and mitigation calculations, including tree #28. Trees with a health rating of two or more, such as #91, have been included in the mitigation calculations on the updated tree mitigation plan.

2. Trees 49 and 105 have a proximity to curb lines and proposed drainage structures that would be detrimental to the health of the tree. Suggest removing and/or relocating. Please update mitigation calculations as necessary.

Response: Trees #49 and #105 have been marked as removals and the impacted oak has been included in the updated mitigation calculations. The mitigation table and plan sets have been revised accordingly. Utility conflicts have been verified and cleared.

Landscape Plan Review

1. Update second Landscape note to reflect city of Stuart requirements.

Response: The landscape note has been revised on the updated plan.

2. Suggest removal of existing tree above, provide necessary replacements to meet onsite requirements in the VUA landscape areas.

Response: The Tree Mitigation and Landscape Plans have been revised to show tree #105 as a relocation.

3. Irrigation plans to be submitted for review for all proposed landscape.

Response: An irrigation plan has been included in the resubmittal.

4. Additional comments may apply once revisions have been made.

Response: Acknowledged.

Respectfully,



**Cotleur &
Hearing**

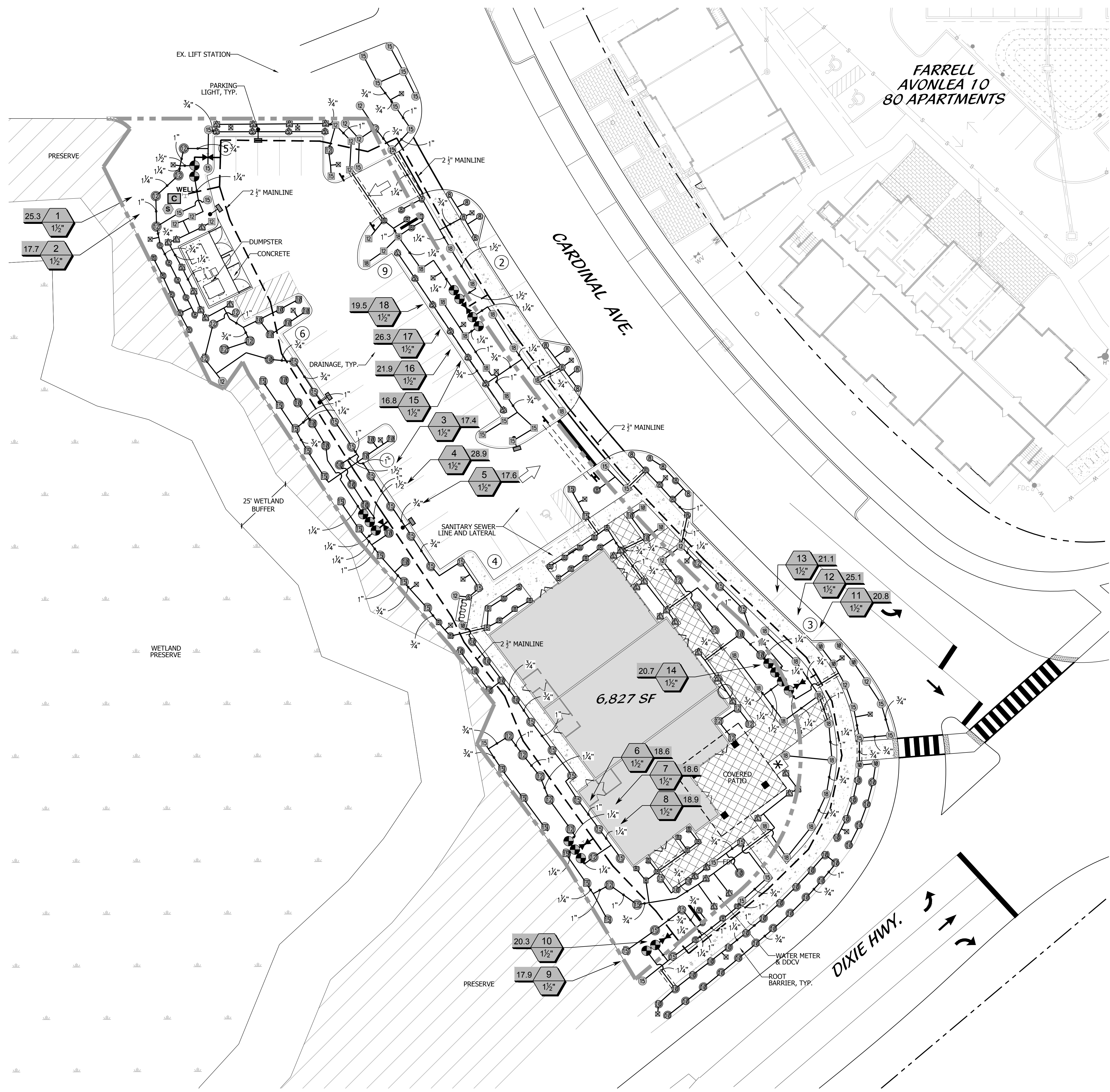
1934 Commerce Lane · Suite 1 · Jupiter, FL · 33458
561.747.6336 · 561.747.1377

George Missimer, AICP

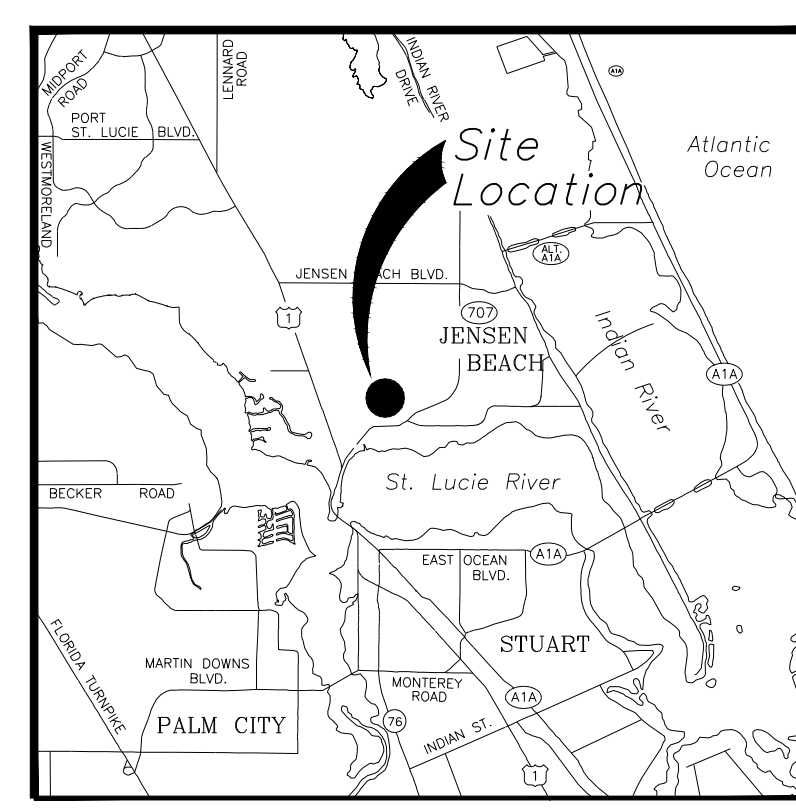
Partner

561-406-1008

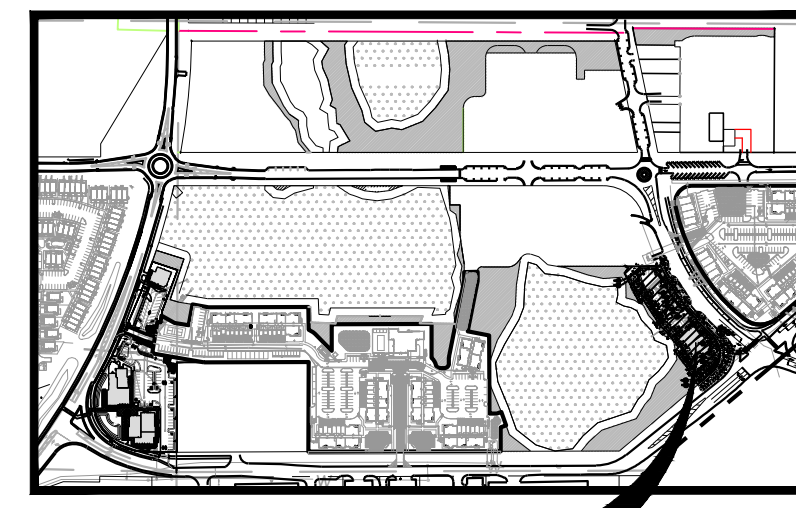
gmissimer@cotleur-hearing.com



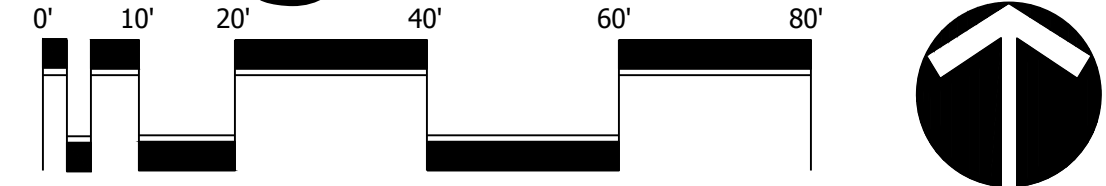
LOCATION MAP



KEY MAP

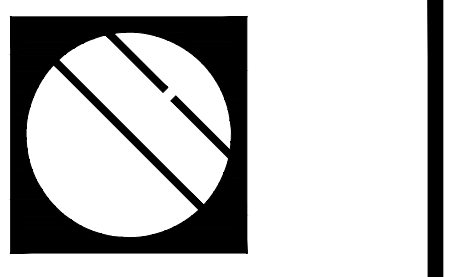


Irrigation Plan



Scale: 1" = 20'-0"

North



Cotleur & Hearing

Landscape Architects
Land Planners
Environmental Consultants
1934 Commerce Lane
Suite 1
Jupiter, Florida 33458
561.747.6336 · Fax 747.1377
www.cotleurhearing.com
Lic# LC-26000535

Avonlea Commercial Corners Lot 11 Irrigation Plan Stuart, Florida

DESIGNED	DEH
DRAWN	RO
APPROVED	DEH
JOB NUMBER	21-0401
DATE	11-19-25
REVISIONS	

November 19, 2025 6:20:25 p.m.
Drawing: 21-0401 LP.DWG

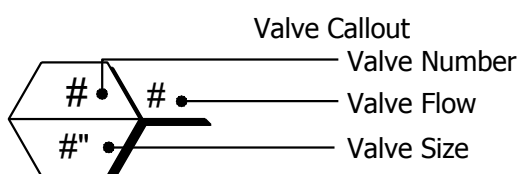
SHEET 1 OF 3

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IRRIGATION SCHEDULE

Table with 5 columns: SYMBOL, MANUFACTURER/MODEL, QTY, ARC, PSI, GPM, RADIUS. Lists various Rain Bird irrigation components like valves, controllers, and pipe.

Table with 3 columns: SYMBOL, MANUFACTURER/MODEL/DESCRIPTION, QTY. Lists landscape products like globe valves, quarter-turn shutoff valves, and wiring.



IRRIGATION NOTES

- 1. THE PLANS AND DRAWINGS ARE DIAGRAMMATIC OF THE WORK TO BE PERFORMED...
15. ALL CONNECTIONS FROM THE 2-WIRE PATH TO THE DECODERS GROUND RODS...
28. AT THE COMPLETION OF THE PROJECT THE CONTRACTOR SHALL PROVIDE TO THE OWNER THE FOLLOWING ITEMS...

SYSTEM DESIGN CRITERIA FOR LATERAL PIPE SIZES

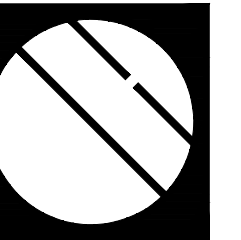
Table mapping pipe size to flow rate and GPM. Columns: Pipe Size, Flow Rate, GPM.

*ALL SLEEVING IS TO BE AT LEAST TWICE THE SIZE OF PIPE TRAVELING THROUGH
*ALL PIPE SHALL BE SIZED IN ACCORDANCE WITH THE ABOVE CRITERIA UNLESS SPECIFICALLY NOTED.

48 HOURS BEFORE DIGGING CALL TOLL FREE 811 SUNSHINE STATE ONE CALL OF FLORIDA, INC.

NOTE: CONTRACTOR TO HAND DIG WITHIN 3' OF ALL MARKED UTILITIES TO VERIFY ACTUAL LOCATION OF UNDERGROUND LINES PRIOR TO PLANTING.

IRRIGATION SCHEDULE AND NOTES



Cotleur & Hearing

Landscape Architects Land Planners Environmental Consultants 1934 Commerce Lane Suite 1 Jupiter, Florida 33458 561.747.6336 Fax 747.1377 www.cotleurhearing.com Lic# LC-26000535

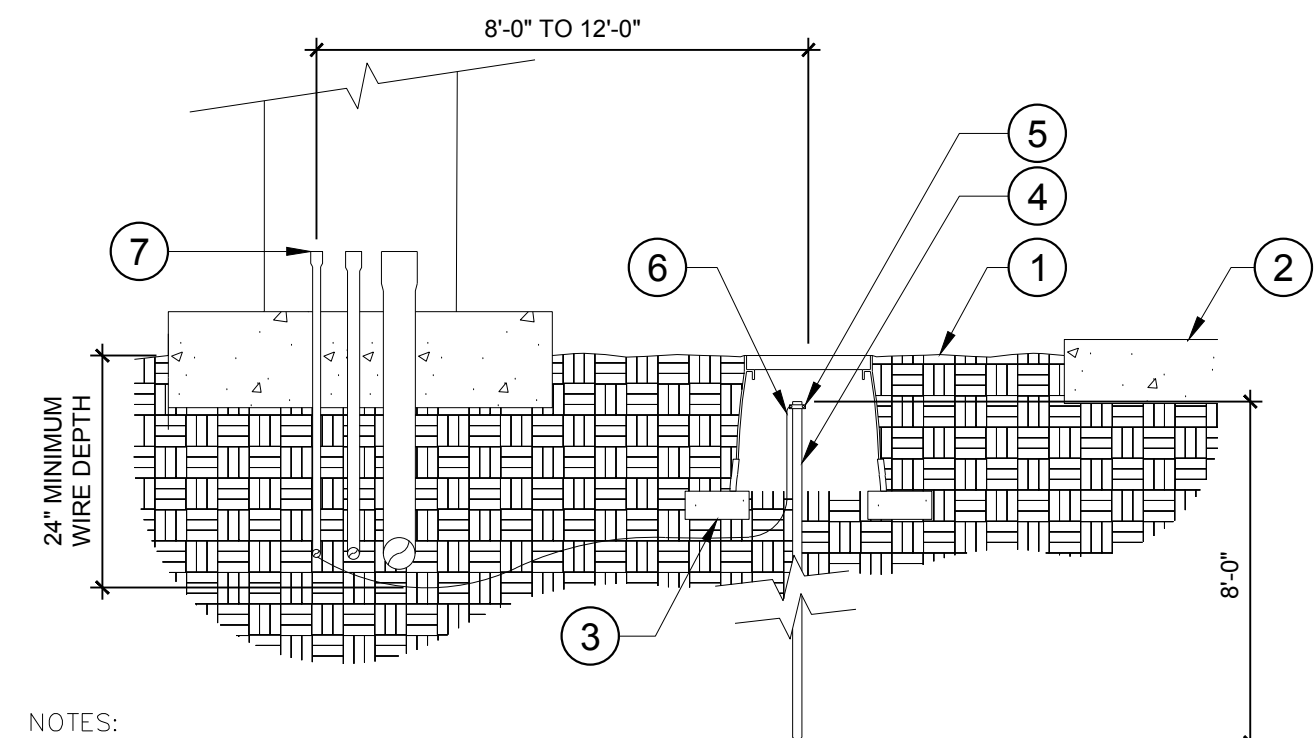
Avonlea Commercial Corners Lot 11 Irrigation Plan Stuart, Florida

Table for project details: DESIGNED DEH, DRAWN JCO, APPROVED DEH, JOB NUMBER 21-0401, DATE 11-19-25, REVISIONS.

November 19, 2025 6:20:25 p.m. Drawing: 21-0401 LP.DWG

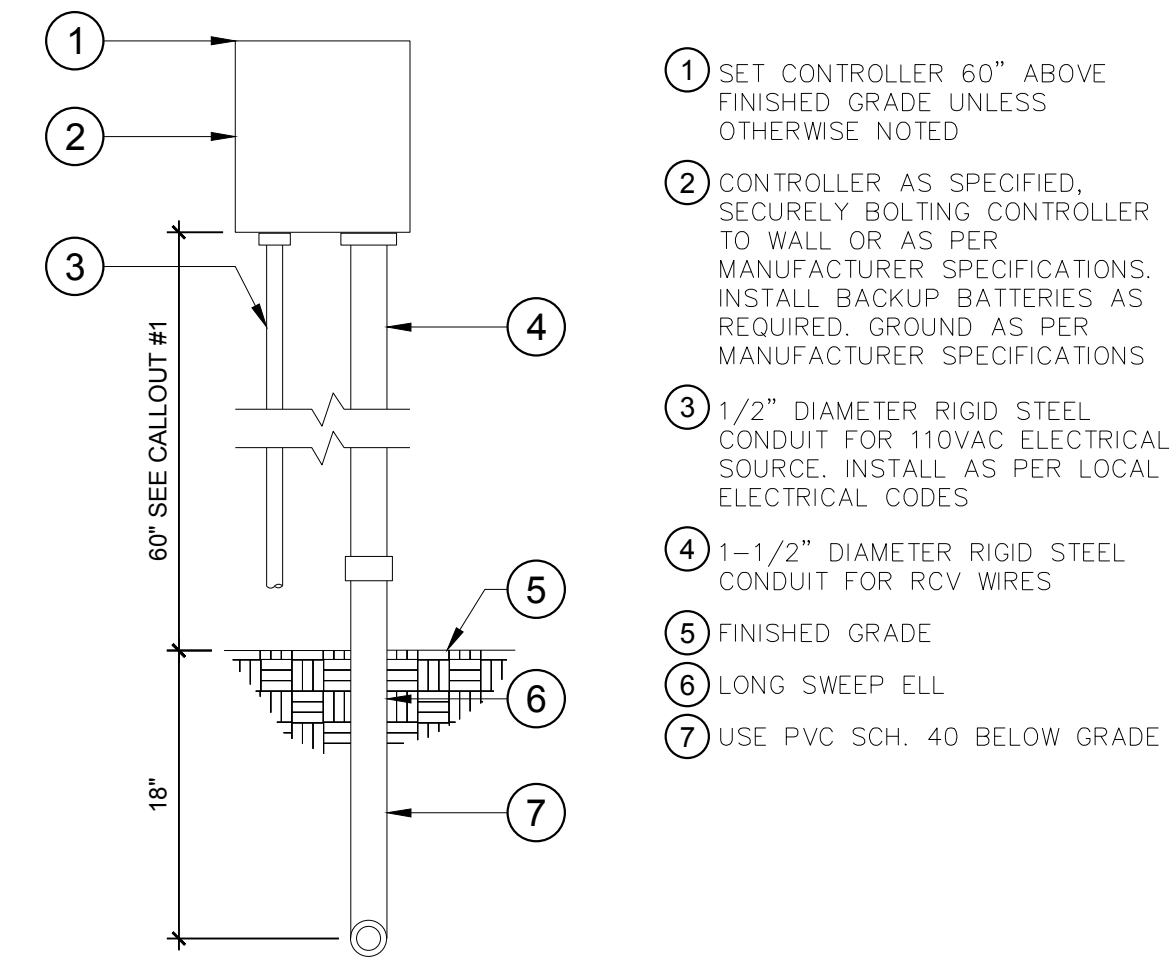
SHEET 2 OF 3

© COTLEUR & HEARING, INC. These drawings are the property of the architect and are not to be used for extensions or on other projects except by agreement in writing with the architect. It is the responsibility of the contractor to verify all dimensions and conditions before construction and to obtain all necessary permits and public records requirement under Florida law.



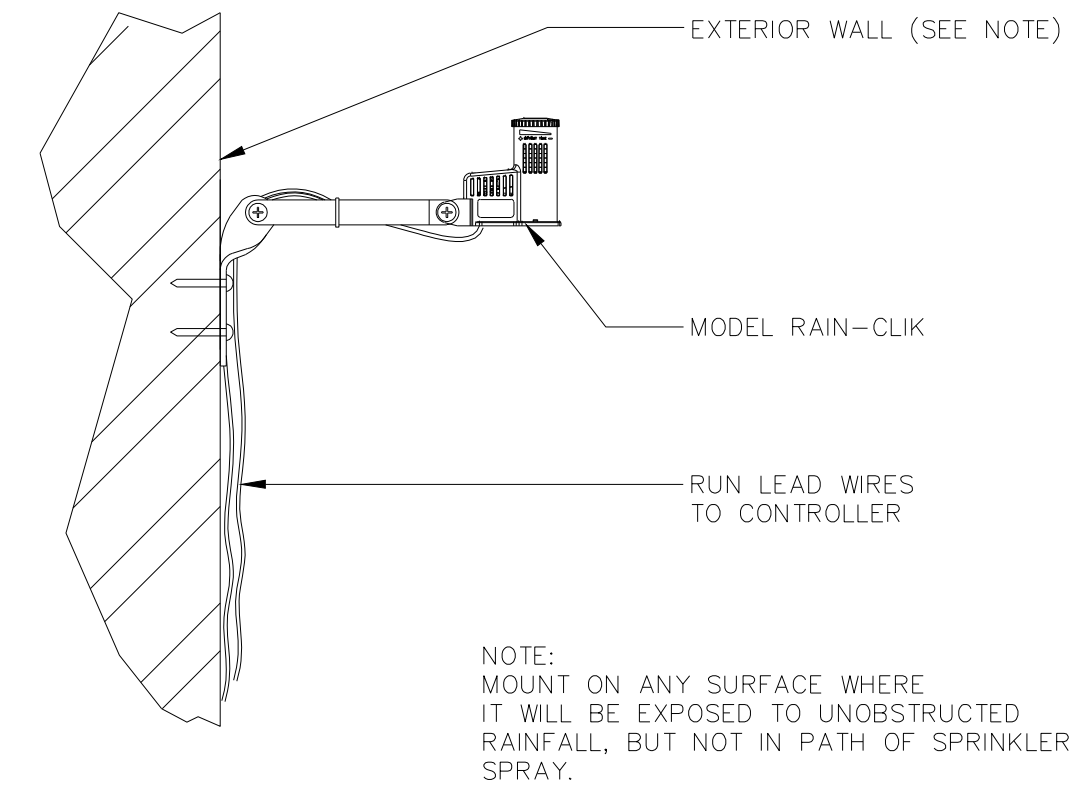
- NOTES:
1. ALL GROUNDING REQUIREMENTS FOR CONTROLLERS SHALL CONFORM TO LOCAL ELECTRIC CODES.
 2. GROUNDING ROD SHALL NOT BE LOCATED IN THE SAME TRENCH AS IRRIGATION MAINLINES OR LATERAL LINES.
 3. VALVE BOX SHALL BE WRAPPED WITH A MINIMUM 3 MIL THICK PLASTIC AND SECURED TO THE VALVE BOX USING DUCT TAPE OR ELECTRICAL TAPE.
 4. INSTALL GROUNDING ROD PER THE CONTROLLER MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.
- 1 FINISH GRADE
 - 2 PAVEMENT
 - 3 THREE (3) 4" x 8" BRICKS
 - 4 5/8" x 8'-0" COPPER GROUNDING ROD
 - 5 GROUNDING ROD CLAMP
 - 6 #6 AWG BARE COPPER WIRE
 - 7 1/2" PVC ELECTRICAL CONDUIT AND SWEEP FOR EARTH GROUND

1 GROUNDING ROD
1" = 1'-0" FX-IR-FX-AUXEQ-01



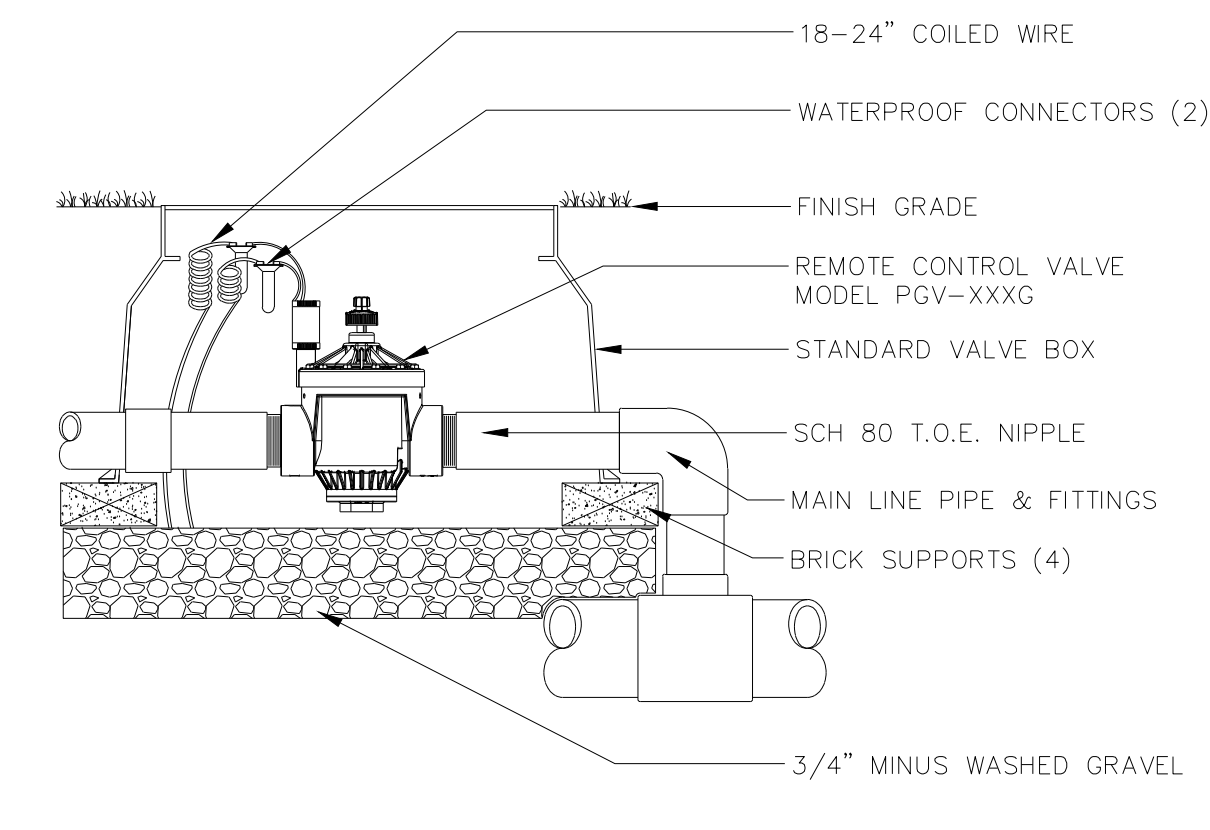
- 1 SET CONTROLLER 60" ABOVE FINISHED GRADE UNLESS OTHERWISE NOTED
- 2 CONTROLLER AS SPECIFIED, SECURELY BOLTING CONTROLLER TO WALL OR AS PER MANUFACTURER SPECIFICATIONS. INSTALL BACKUP BATTERIES AS REQUIRED. GROUND AS PER MANUFACTURER SPECIFICATIONS
- 3 1/2" DIAMETER RIGID STEEL CONDUIT FOR 110VAC ELECTRICAL SOURCE. INSTALL AS PER LOCAL ELECTRICAL CODES
- 4 1-1/2" DIAMETER RIGID STEEL CONDUIT FOR RCV WIRES
- 5 FINISHED GRADE
- 6 LONG SWEEP ELL
- 7 USE PVC SCH. 40 BELOW GRADE

2 WALL MOUNT CONTROLLER
1" = 1'-0" FX-IR-FX-CONT-06



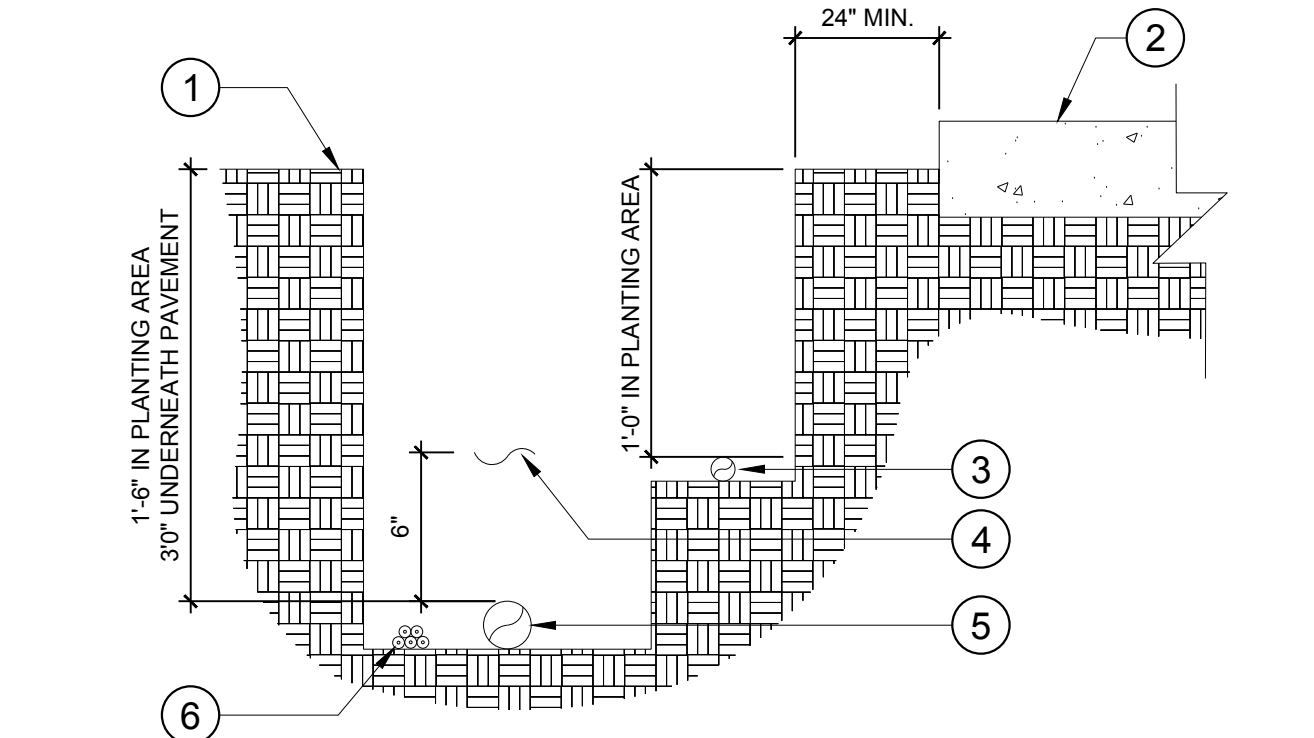
- NOTE: MOUNT ON ANY SURFACE WHERE IT WILL BE EXPOSED TO UNOBSTRUCTED RAINFALL, BUT NOT IN PATH OF SPRINKLER SPRAY.

3 RAIN-CLICK
3" = 1'-0" FX-IR-HUNT-SENS-16



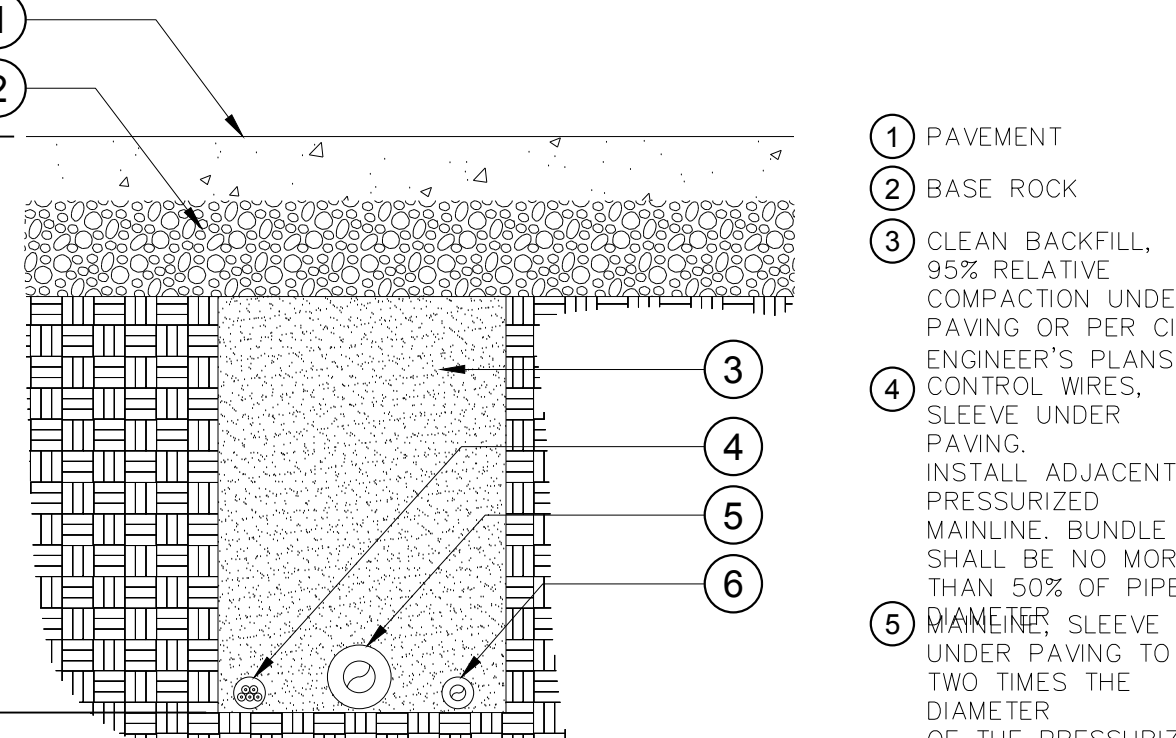
- 18-24" COILED WIRE
- WATERPROOF CONNECTORS (2)
- FINISH GRADE
- REMOTE CONTROL VALVE MODEL PGV-XXXG
- STANDARD VALVE BOX
- SCH 80 T.O.E. NIPPLE
- MAIN LINE PIPE & FITTINGS
- BRICK SUPPORTS (4)
- 3/4" MINUS WASHED GRAVEL

4 PGV GLOBE VALVE
1" = 1'-0" FX-IR-HUNT-VALV-11



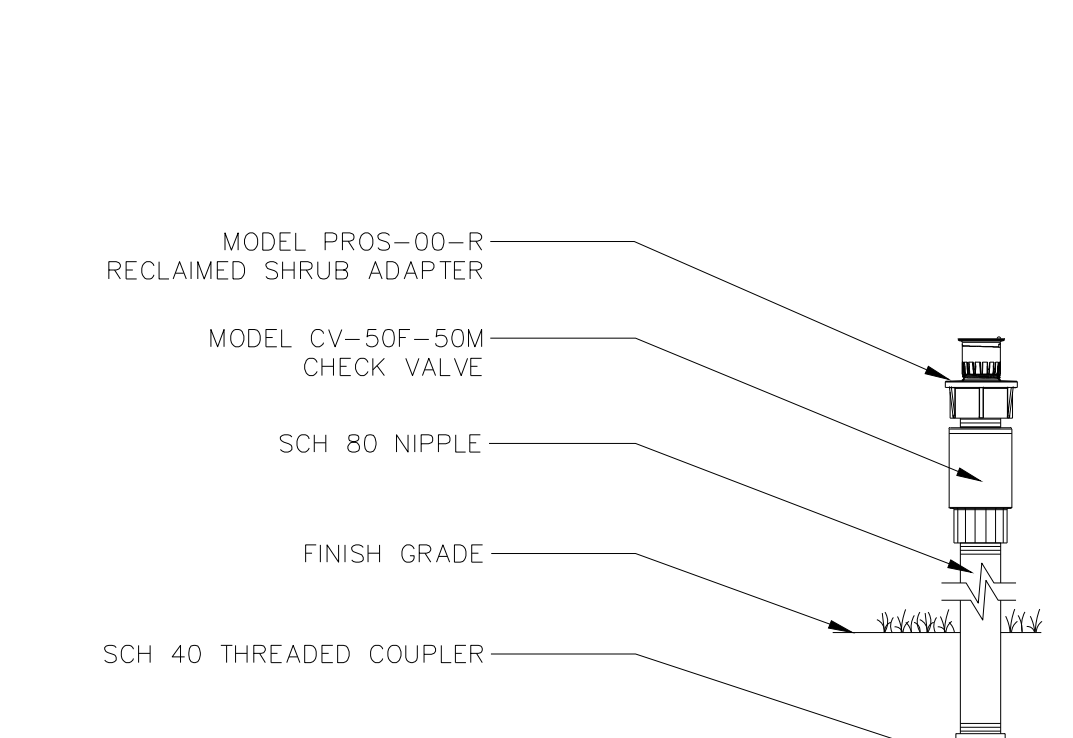
- NOTES:
1. SEE IRRIGATION LEGEND FOR MAINLINE AND LATERAL LINE PIPE SIZE AND TYPE.
 2. DIRECT BURIAL CONTROL WIRES SHALL BE INSTALLED IN SCH. 40 PVC ELECTRICAL CONDUIT IF REQUIRED.
 2. WIRE IRRIGATION WIRE SHALL BE INSTALLED IN SCH. 40 PVC ELECTRICAL CONDUIT.
 4. DETECTABLE LOCATOR TAPE SHALL BE LOCATED SIX INCHES (6") ABOVE THE ENTIRE MAINLINE RUN.
- 1 FINISHED GRADE
 - 2 PAVEMENT
 - 3 NON-PRESSURIZED LINE (LATERAL LINE)
 - 4 DETECTABLE LOCATOR TAPE
 - 5 PRESSURIZED LINE (MAINLINE)
 - 6 DIRECT BURIAL LOW VOLTAGE CONTROL WIRES

5 IRRIGATION TRENCHING
1 1/2" = 1'-0" FX-IR-FX-AUXEQ-08



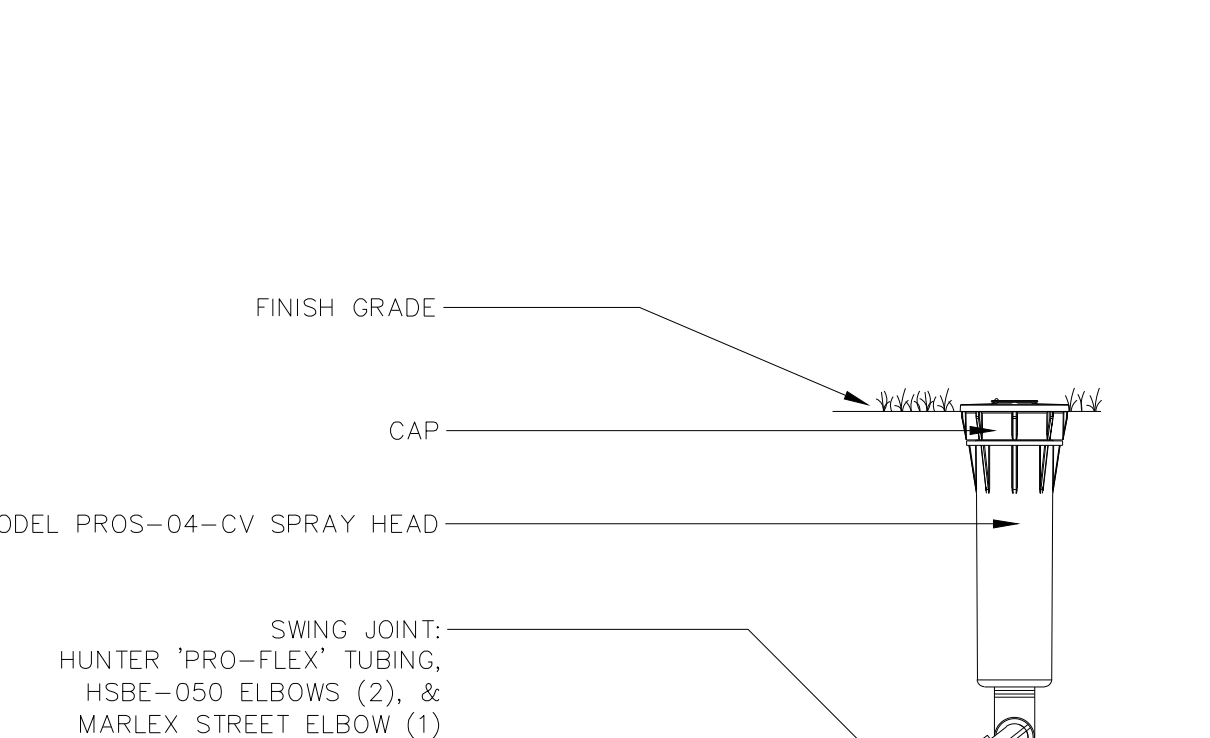
- NOTES:
1. SEE IRRIGATION LEGEND FOR MAINLINE SIZE AND TYPE.
 2. ALL SLEEVES SHALL BE SCH. 40 PVC PIPE.
 3. ALL SLEEVES SHALL EXTEND 12" BEYOND THE EDGE OF PAVEMENT.
 4. END OF SLEEVES SHALL BE LOCATED WITH A WOODEN STAKE OR PVC PIPE. LOCATORS SHALL RUN CONTINUOUSLY FROM THE END OF THE SLEEVE TO FINISHED GRADE.
- 1 PAVEMENT
 - 2 BASE ROCK
 - 3 CLEAN BACKFILL, 95% RELATIVE COMPACTION UNDER PAVING OR PER CIVIL ENGINEER'S PLANS
 - 4 CONTROL WIRES, SLEEVE UNDER PAVING. INSTALL ADJACENT TO PRESSURIZED MAINLINE. BUNDLE SHALL BE NO MORE THAN 50% OF PIPE DIAMETER
 - 5 SLEEVE UNDER PAVING TO BE TWO TIMES THE DIAMETER OF THE PRESSURIZED MAINLINE PIPE
 - 6 NON-PRESSURIZED LINE. SLEEVE UNDER PAVING TO BE TWO TIMES THE DIAMETER OF THE LATERAL LINE

6 PIPE BENEATH PAVEMENT
1" = 1'-0" FX-IR-FX-AUXEQ-08



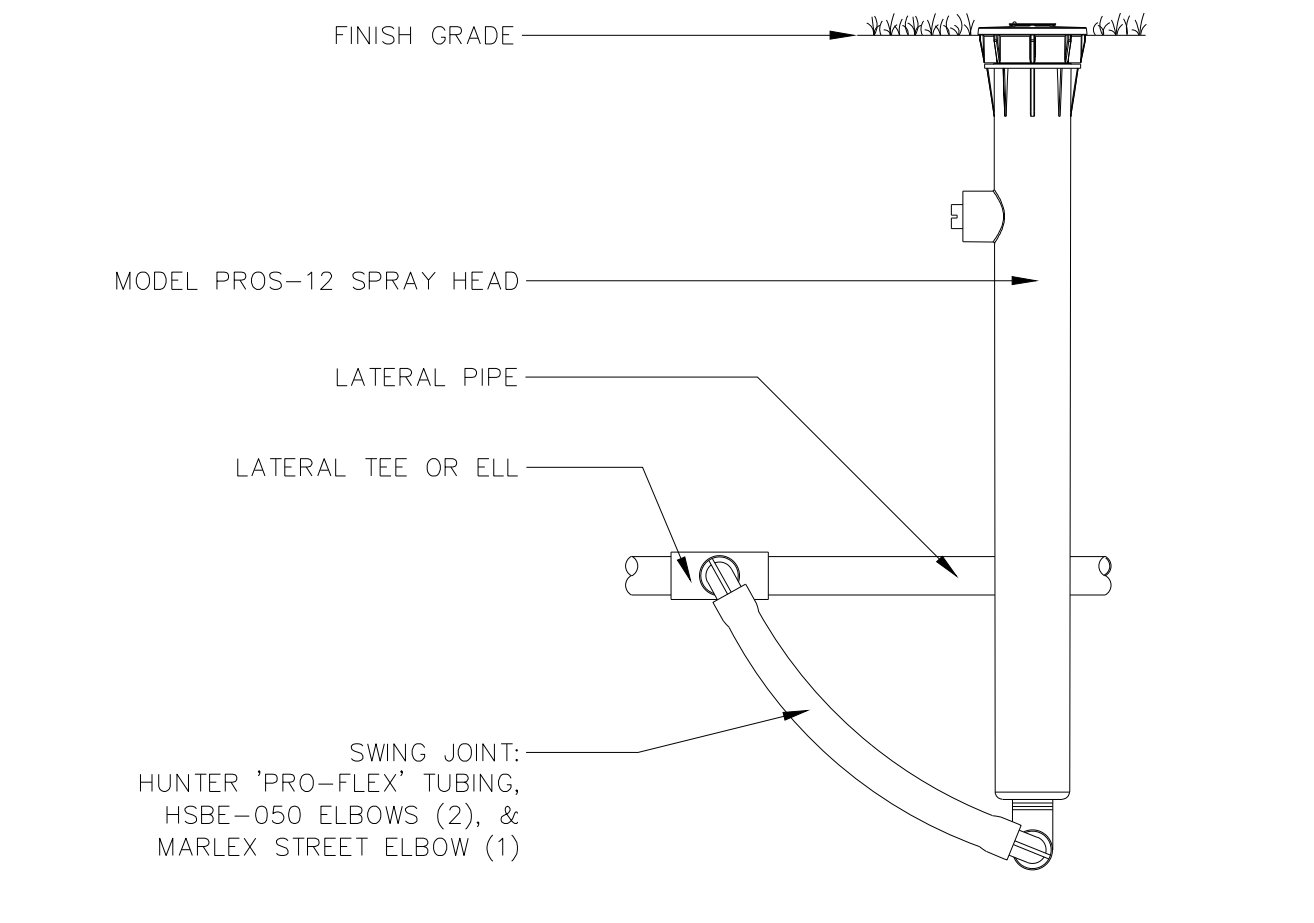
- MODEL PROS-00-R RECLAIMED SHRUB ADAPTER
- MODEL CV-50F-50M CHECK VALVE
- SCH 80 NIPPLE
- FINISH GRADE
- SCH 40 THREADED COUPLER
- SCH 80 NIPPLE
- LATERAL TEE OR ELL
- LATERAL PIPE
- 1/2" PVC STREET ELLS (3)

7 PROS-00-R SPRAY HEAD W/ PVC STREET ELLS
3" = 1'-0" FX-IR-HUNT-SPRA-05

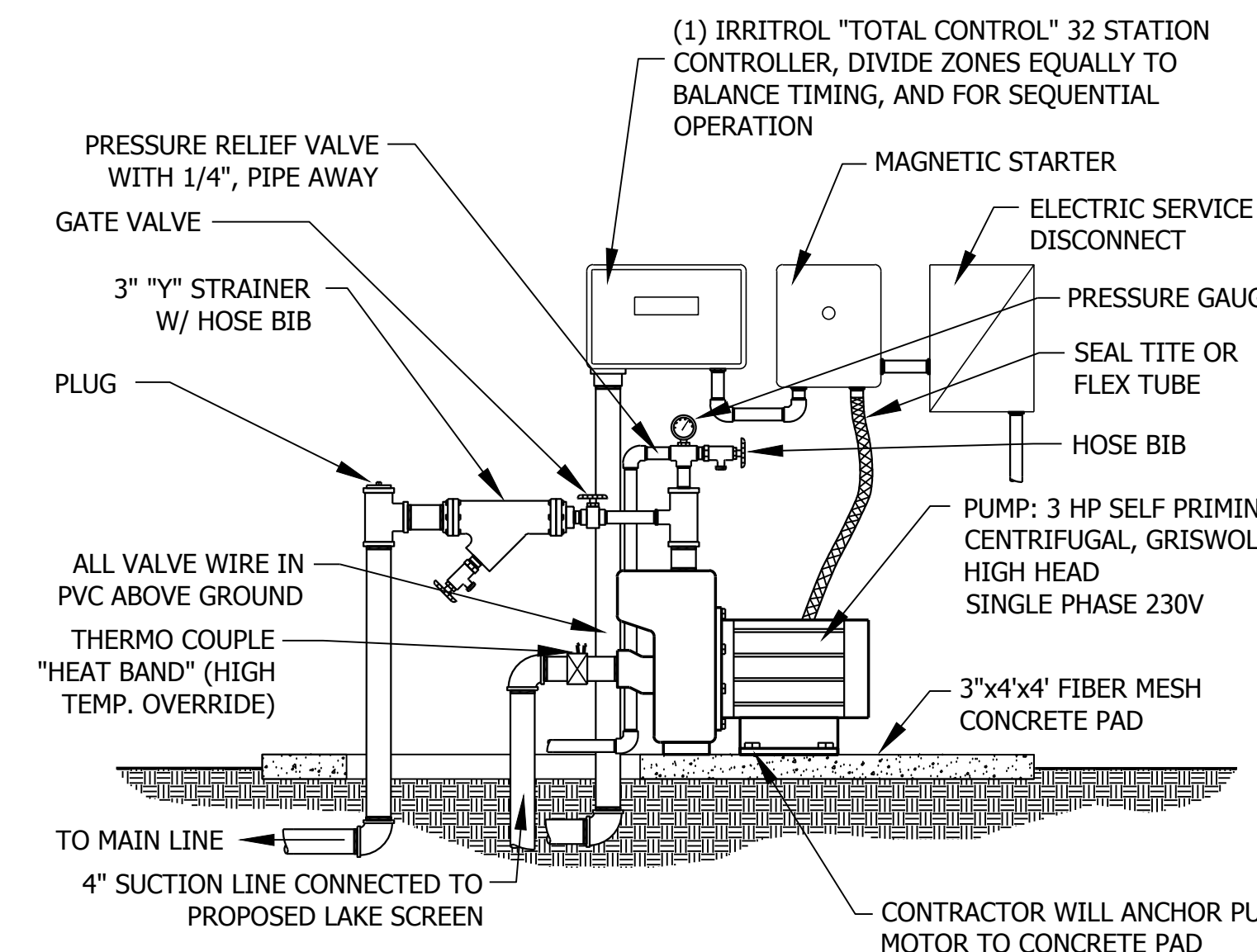


- FINISH GRADE
- CAP
- MODEL PROS-04-CV SPRAY HEAD
- SWING JOINT: HUNTER 'PRO-FLEX' TUBING, HSBE-050 ELBOWS (2), & MARLEX STREET ELBOW (1)
- LATERAL TEE OR ELL
- LATERAL PIPE

8 PROS-04-CV SPRAY HEAD W/ PRO-FLEX TUBING
3" = 1'-0" FX-IR-HUNT-SPRA-28



9 PROS-12 SPRAY HEAD WITH PRO-FLEX TUBING
3" = 1'-0" FX-IR-HUNT-SPRA-48



10 PUMP STATION DETAIL
NTS

SYSTEM DESIGN CRITERIA FOR LATERAL PIPE SIZES

PIPE SIZE	FLOW RANGE (GPM)	CRITERIA
3/4"	0 - 8	GPM
1"	8 - 15	GPM
1-1/4"	15 - 22	GPM
1-1/2"	22 - 32	GPM
2"	32 - 50	GPM
3"	50 - 68	GPM

*ALL SLEEVING IS TO BE AT LEAST TWICE THE SIZE OF PIPE TRAVELING THROUGH

*ALL PIPE SHALL BE SIZED IN ACCORDANCE WITH THE ABOVE CRITERIA UNLESS SPECIFICALLY NOTED.

48 HOURS BEFORE DIGGING
CALL TOLL FREE
811
SUNSHINE STATE ONE CALL
OF FLORIDA, INC.

DESIGNED	DEH
DRAWN	JCO
APPROVED	DEH
JOB NUMBER	21-0401
DATE	11-19-25
REVISIONS	

November 19, 2025 6:20:25 p.m.
Drawing: 21-0401 LP.DWG

TRAFFIC IMPACT ANALYSIS

Avonlea 10-11-13-14-15
Stuart, FL

Prepared for:
Farrell Building Company
Stuart, Florida

Prepared by:


Engineering & Planning, Inc.

1172 SW 30th Street, Suite 500
Palm City, FL 34990
(772) 286-8030

124005
Revised June 2025
Revised May 2024
July 2023
© MacKenzie Engineering and Planning, Inc.
CA 29013

Shaun G. MacKenzie P.E.
PE Number 61751

EXECUTIVE SUMMARY

MacKenzie Engineering & Planning, Inc. was retained to prepare an analysis of the traffic impacts from Avonlea 10-11-13-14-15. The site is located at the northwest corner of Baker Road & Green River Parkway, Stuart, Florida (Parcel -10: 28-37-41-013-000-00100-0, Parcel -11: 28-37-41-013-000-00110-0, Parcel -13: 28-37-41-013-000-00130-0, Parcel -14: 28-37-41-013-000-00140-0, Parcel -15: 28-37-41-013-000-00150-0). The applicant proposes:

- Parcel 10: 80 Low-rise residential units with ground-floor commercial units
- Parcel 11: 6,827 SF of shopping center use
- Parcel 13: 123 Multifamily Housing (Low-rise) units
- Parcels 14 & 15: 16,373 SF of shopping center use

The development is planned for completion by the end of 2030.

The project is expected to generate the following net external trips:

- 1,782 daily, 126 AM peak hour (41 in/85 out), and 179 PM peak hour (106 in/73 out) trips.

The project is expected to generate the following driveway trips:

- 2,444 daily, 156 AM peak hour (59 in/97 out), and 257 PM peak hour (145 in/112 out) trips

The traffic analysis was performed in accordance with the City of Stuart Code, Section 4.05.02 Traffic Impact Analysis. The project meets the thresholds of Section 4.05.02. The results of the analysis show that the project has no significant impacts, the surrounding roadways and intersections are projected to operate acceptably and therefore meet the City's concurrency requirements.

A 205-foot left-turn lane on Dixie Highway into Lot 13 is recommended.

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- Appendix B. Martin County 2022 Roadway Level of Service Inventory Report
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- Appendix E. Signal Timings
- Appendix F. FDOT Design Manual
- Appendix G. Site Plan
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INTRODUCTION

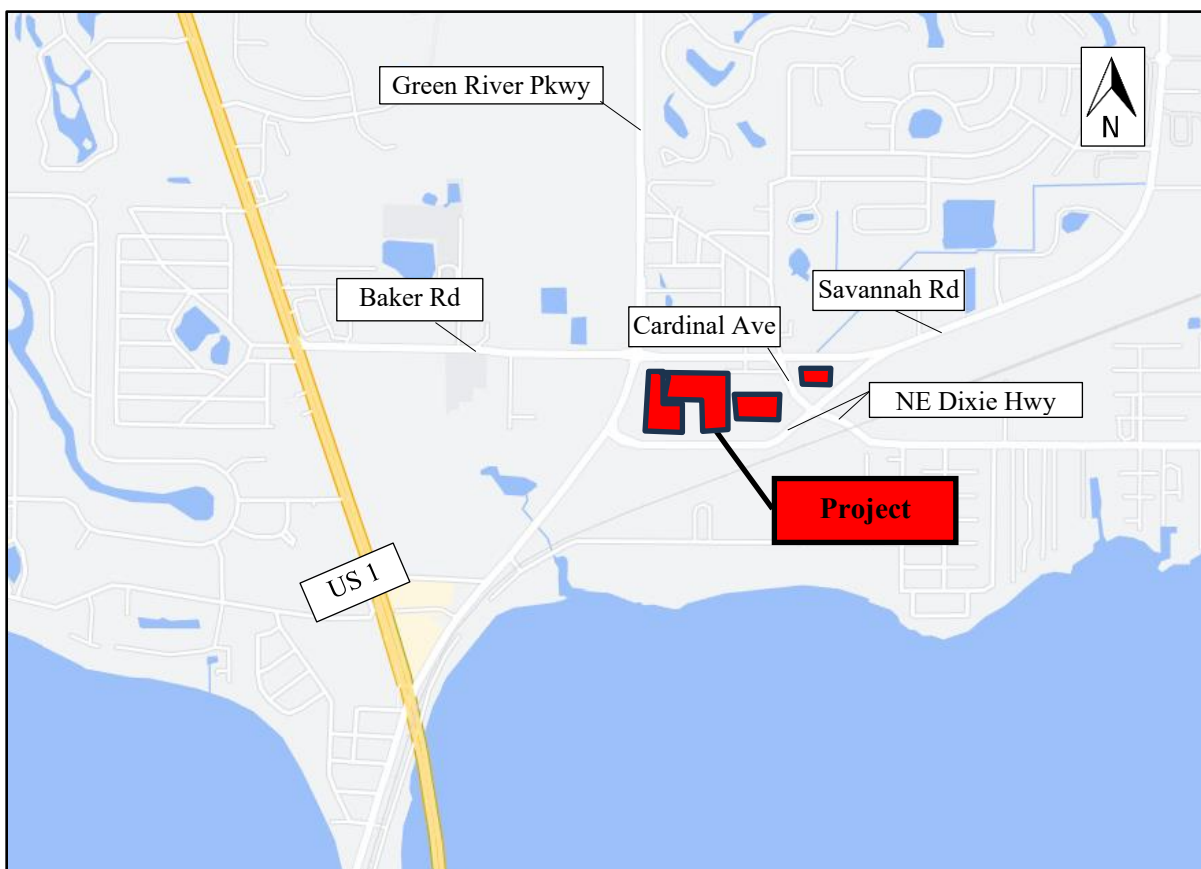
MacKenzie Engineering & Planning, Inc. was retained to prepare an analysis of the traffic impacts from Avonlea 10-11-13-14-15. The site is located at the northwest corner of Baker Road & Green River Parkway, Stuart, Florida (Parcel -10: 28-37-41-013-000-00100-0, Parcel -11: 28-37-41-013-000-00110-0, Parcel -13: 28-37-41-013-000-00130-0, Parcel -14: 28-37-41-013-000-00140-0, Parcel -15: 28-37-41-013-000-00150-0). The applicant proposes:

- Parcel 10: 80 Low-rise residential with ground-floor commercial units
- Parcel 11: 6,827 SF of retail use
- Parcel 13: 123 Multifamily Housing (Low-rise) units
- Parcels 14 & 15: 16,373 SF of retail use

The development is planned for completion by the end of 2030.

A site location map is shown below in Figure 1.

Figure 1. Site Location Map



INVENTORY AND PLANNING DATA

The traffic data used in this analysis were obtained from Martin County, MacKenzie Engineering and Planning, Inc. (MEP), and FDOT. The data included:

- FDOT's Q/LOS Manual
- *Trip Generation, 11th Edition* (ITE report)
- Martin County Impact Fee Study Technical Report, 12/2012, Walter H. Keller, Inc.
- Martin County 2022 Roadway Level of Service Inventory Report

PROJECT TRAFFIC

Traffic Generation

The daily and peak hour traffic generation for the development used the trip generation rates published in the Institute of Transportation Engineers (ITE)'s report, *Traffic Generation (11th Edition)* for:

Land Use 230, Low-rise residential with ground floor commercial

Land Use 220, Multi-Family Housing (Low-rise),

Land Use 822, Strip Retail Plaza (less than 40,000 SF of shopping center use),

Site Land Uses

Proposed Land Use

- Parcel 10: 80 Low-rise residential units with floor ground commercial units
- Parcel 11: 6,827 SF of Strip Retail Plaza use
- Parcel 13: 123 Multifamily Housing (Low-Rise) units
- Parcel 14 & 15: 16,373 SF of Strip Retail Plaza use

The project is expected to generate the following net external trips:

- 1,782 daily, 126 AM peak hour (41 in/85 out), and 179 PM peak hour (106 in/73 out) trips.

The project is expected to generate the following driveway trips:

- 2,444 daily, 156 AM peak hour (59 in/97 out), and 257 PM peak hour (145 in/112 out) trips

Internal Capture

The proposed internal capture is estimated for AM and PM peak hour conditions, respectively. This is a conservation rate given the potential interaction between the diverse uses on the site. Internal capture calculations are detailed in Exhibits 1B and 1C.

Pass-by Capture

The site contains a 48% pass-by capture for the retail plazas in accordance with the Martin County Impact Fee Study Technical Report, 12/2012, Walter H. Keller, Inc.

The combined trip generation of all four projects is shown in Table 1.

Table 1. Trip Generation

Land Use	Intensity		Daily	AM Peak Hour			PM Peak Hour		
			Trips	Total	In	Out	Total	In	Out
Proposed Site Traffic									
<i>Parcel</i>									
13	Multi-family Housing (Low-rise)	123 DU	864	61	15	46	73	46	27
10	Low-Rise Residential w Ground Commercial	80 DU	275	35	8	27	29	21	8
14,15	Strip Retail Plaza (<40K)	16.373 1000 SF	921	40	24	16	110	55	55
11	Strip Retail Plaza (<40K)	6.827 1000 SF	518	22	13	9	59	30	29
Subtotal			2,578	158	60	98	271	152	119
Internal Capture									
	Multi-family Housing (Low-rise)	AM 0.0% PM/ DAILY 5.5%	47	0	0	0	4	3	1
	Low-Rise Residential w Ground Commercial	2.9% 10.3%	28	1	1	0	3	1	2
	Strip Retail Plaza (<40K)	2.5% 5.5%	50	1	0	1	6	3	3
	Strip Retail Plaza (<40K)	0.0% 1.7%	9	0	0	0	1	0	1
Subtotal			134	2	1	1	14	7	7
Pass-By Traffic									
	Strip Retail Plaza (<40K)	48%	418	19	12	7	50	25	25
	Strip Retail Plaza (<40K)	48%	244	11	6	5	28	14	14
Subtotal			662	30	18	12	78	39	39
NET PROPOSED TRIPS			1,782	126	41	85	179	106	73
Total Proposed Driveway Volumes			2,444	156	59	97	257	145	112

Note: Trip generation was calculated using the following data:

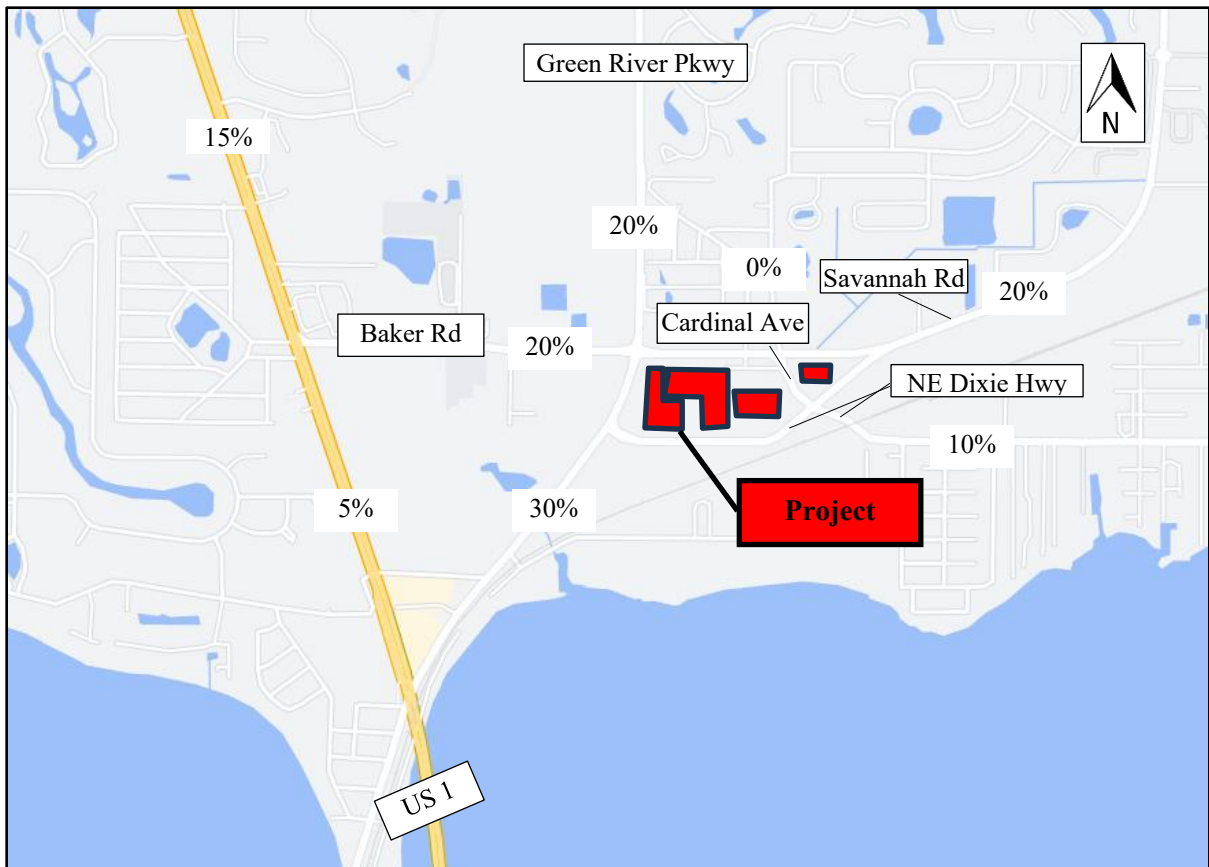
Land Use	ITE		Daily Rate	Pass-by Rate	AM Peak Hour		PM Peak Hour	
	Code	Unit			in/out	Rate	in/out	Equation
Multi-family Housing (Low-rise)	220	DU	$T = 6.41(X) + 75.31$	0%	24/76	$T = 0.31(X) + 22.85$	63/37	$T = 0.43(X) + 20.55$
Low-Rise Residential w Ground Commercial	230	DU	3.44	0%	23/77	0.44	71/29	0.36
Strip Retail Plaza (<40K)	822	1000 SF	$T = 42.2(X) + 229.68$	48%	60/40	$\ln(T) = 0.66 \ln(X) + 1.84$	50/50	$\ln(T) = 0.71 \ln(X) + 2.72$

TRAFFIC ASSIGNMENT

The traffic assignment was developed based upon existing and proposed developments, generators, destinations, and knowledge of the local area. These factors were applied using a traffic engineering rationale similar to the gravity model.

- North – 20%
- South – 30%
- West – 20%
- East – 30%

Figure 2. Traffic Assignment



HISTORICAL GROWTH

In order to provide accurate traffic analysis, the growth rate at each intersection was determined by a volume-weighted averaging of the growth on each leg of the intersection as shown in Table 2. The historic annual growth rate on the surrounding facilities is between 0.5 and 2.7 percent with an average of 1.27 percent. A rounded growth rate of 1.3 percent was used for this analysis.

Table 2. Historic Growth Rate

Roadway	From	To	Avg. Annual Growth Rate
Savanna Rd	NE 24th St	CR-707A (Jensen Beach Blvd.)	0.50%
Baker Rd	SR-5	CR-723	0.60%
Green River Pwy	Dixie Hwy	Baker Rd	2.70%

Historical Growth Rate = 1.27%
Growth Rate Used = 1.3

ROADWAY ANALYSIS

Roadway Capacity

Roadway capacity utilizes the Martin County 2022 Roadway Level of Service Inventory Report.

Study Area

In accordance with 4.05.02 of the City Code, the study area is the adjacent roadways and all roadways where project traffic is estimated to consume five percent of the roadway capacity. As shown in Table 3, the combined impact of the projects does not significantly impact any roadways. Therefore, only the adjacent roadways will be studied.

Table 3. Significant Impact

Road	From	To	Capacity	Project Trips		Significance	Significant Impact
				NB/EB	SB/WB		
Baker Road	Green River Parkway	Cardinal Road	750	26	12	3.5%	NO
Green River Parkway	Dixie Highway	Baker Road	750	22	18	2.9%	NO
Dixie Highway	Green River Parkway	Cardinal Road	880	36	42	4.8%	NO
NE Savannah Road	Baker Road	Cardinal Road	880	16	22	2.5%	NO
Cardinal	Baker Road	NE Savannah Road	880	18	26	3.0%	NO

Volume Development Analysis

The 2022 peak hour traffic volumes from Martin County’s 2022 Roadway Level of Service Inventory Report were increased by the historic growth rate to the projected year 2030 pre-development traffic volumes. Project traffic volumes on each road segment were developed based on the project's trip generation times the project assignment. The trips for each project are shown in Exhibits 2-5 and summarized in Exhibit 6. The post-development 2030 traffic volumes were developed by adding pre-development traffic volume plus project traffic. The post-development 2030 traffic volumes were compared to the service volumes for each respective roadway segment to determine if the road is projected to operate acceptably as shown in Table 4. All roads are projected to operate acceptably as shown in Table 4.

Table 4. Post-development 2030 PM Peak Hour Roadway Analysis

Road	From	To	Capacity	2022 PHPD Volume	Growth Rate	2030 Pre-Dev	Project Trips (Peak)	2030 Post-Dev	Acceptable?
Baker Road	Green River Parkway	Cardinal Road	750	241	1.30%	267	26	293	Yes
Green River Parkway	Dixie Highway	Baker Road	750	384*	1.30%	426	22	448	Yes
Dixie Highway	Green River Parkway	Cardinal Road	880	484	1.30%	537	42	579	Yes
NE Savannah Road	Baker Road	Cardinal Road	880	389*	1.30%	431	22	453	Yes
Cardinal	Baker Road	NE Savannah Road	880	206	1.30%	228	26	254	Yes

*MEP 2023 Traffic Counts Adjusted to Peak Season

INTERSECTION ANALYSIS

The intersections within the study area were evaluated in 2030 total (existing traffic plus background plus project) traffic conditions. The analysis was performed during both AM (morning) and PM hours to assess the traffic conditions and performance of these intersections. The intersections under study are:

- Baker Road and Green River Parkway
- Baker Road and Cardinal Avenue
- NE Dixie Highway and Green River Road
- Cardinal Avenue and NE Dixie Highway

Data from the existing facilities within the study area were collected based on aerial photography and site observations. MacKenzie Engineering and Planning, Inc. collected AM and PM peak hour turning movement counts. The counts were adjusted to peak season conditions using FDOT's peak season adjustment factors.

Peak Hour Factor (PHF)

Field observed peak hour factors were utilized in the intersection analysis and were adjusted as follows:

- PHF greater than 0.95 was reduced to 0.95
- PHF less than 0.92 was increased to 0.92

The adjustment is based on FDOT's Quality/LOS Handbook that recommends using a PHF factor of 0.95 for urbanized areas and 0.92 for transitioning/urban areas and is also recommended in FDOT's Traffic Analysis handbook page 34.

Analysis

Baker Road and Green River Parkway

MEP evaluated the Baker Road and Green River Parkway roundabout. With project traffic, the intersection is projected to operate acceptably with all movements operating under capacity (v/c ratio less than 1.0).

Baker Road and Cardinal Avenue

MEP evaluated the Baker Road and Cardinal Avenue roundabout. With project traffic, the intersection is projected to operate acceptably with all movements operating under capacity (v/c ratio less than 1.0).

NE Dixie Highway and Green River Parkway

MEP evaluated the NE Dixie Highway and Green River Parkway intersection. With project traffic, the intersection is projected to operate acceptably with all movements operating under capacity (v/c ratio less than 1.0).

Cardinal Avenue and NE Dixie Highway

MEP evaluated the Cardinal Avenue and NE Dixie Highway intersection. With project traffic, the intersection is projected to operate acceptably with all movements operating under capacity (v/c ratio less than 1.0).

The volume development sheets are Exhibits 7 through 14 and the intersection analysis results are located following Exhibit 14.

ACCESS

The project proposes access as follows:

Two points of access for Parcel 10:

- Driveway 1 – Right-in/Right-out, Left-out
- Driveway 2 – Full Opening

Two points of access for Parcel 11:

- Driveway 3 – Right-in
- Driveway 4 – Right-out, Left-out

One point of access for Parcel 13:

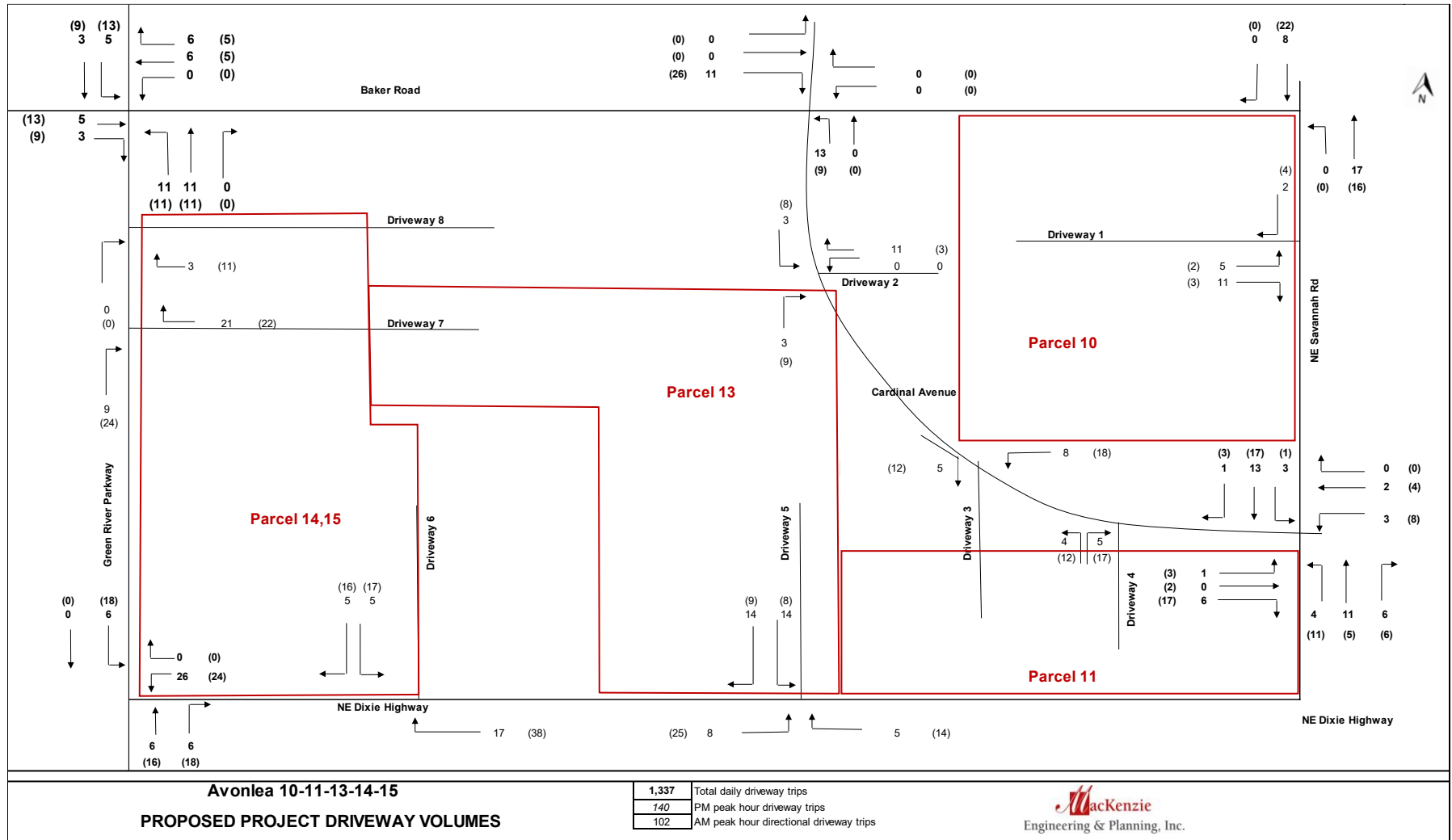
- Driveway 5 – Full Opening

One point of access for Parcels 14 & 15:

- Driveway 6 – Right-in/Right-out, Left-out
- Driveway 7 – Right-in/Right-out
- Driveway 8 – Right-in/Right-out

To calculate the proposed driveway trips a layered approach was used. This approach involves identifying the project trips from each project and adding them together to determine the cumulative trips at each driveway. Figure 3 presents the total proposed driveway trips. The calculations of the trips that are generated from each parcel separately are presented in Exhibits 2 through 5 and the volumes are totaled in Exhibit 6.

Figure 3. Driveway Trips



Driveways

Driveway 1 (Savannah Road)

The site's Driveway 1 is projected to have 4 peak hour right-turning vehicles. This is less than the FDOT's guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 1.

Driveway 2 (Cardinal Avenue)

The site's Driveway 2 is projected to have 9 peak-hour right-turning vehicles. This is less than the FDOT's guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 2. A left-turn lane is not warranted based on project 8 left-turns turning the peak hour. An analysis was performed at Driveway 3 with 18 left-turns that also do not warrant a left-turn lane.

Driveway 3 (Cardinal Avenue)

The site's Driveway 3 is projected to have 12 peak-hour right-turning vehicles. This is less than the FDOT's guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 3. A left-turn lane is not warranted based on project 18 left-turns during the peak hour based on an analysis using NCHRP-457.

Figure 4. Cardinal Avenue Left-Turn Lane Warrant Analysis

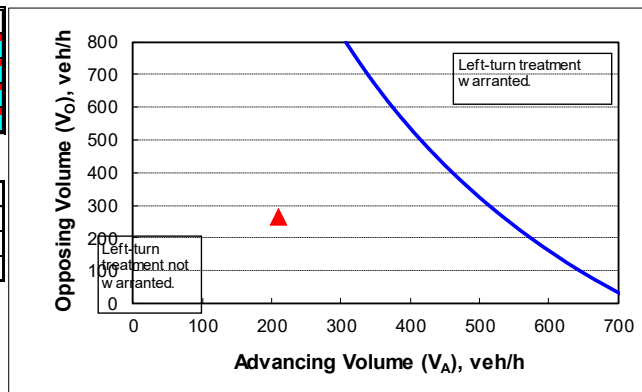
2-lane roadway (English)

INPUT

Variable	Value
85 th percentile speed, mph:	25
Percent of left-turns in advancing volume (V_A), %:	9%
Advancing volume (V_A), veh/h:	210
Opposing volume (V_O), veh/h:	264

OUTPUT

Variable	Value
Limiting advancing volume (V_A), veh/h:	535
Guidance for determining the need for a major-road left-turn bay:	
Left-turn treatment NOT warranted.	



CALIBRATION CONSTANTS

Variable	Value
Average time for making left-turn, s:	3.0
Critical headway, s:	5.0
Average time for left-turn vehicle to clear the advancing lane, s:	1.9

Driveway 4 (Cardinal Avenue)

Driveway 4 is an egress-only access point.

Driveway 5 (NE Dixie Highway)

The site’s Driveway 5 is projected to have 14 peak-hour right-turning vehicles. This is less than the FDOT’s guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 5. A left-turn lane is warranted based on project 25 left-turns during the peak hour based on an analysis using NCHRP-457. The posted speed is 35. FDOT's Greenbook (Pages 3-107) states "At unsignalized intersections the storage length, exclusive of taper, may be based on the number of turning vehicles likely to arrive in an average two-minute period within the peak hour", which is 0.83 vehicles (25 veh / 60 minutes x 2 = 21 feet). In order to provide a conservative analysis, 50 feet of storage is provided. Therefore, a turn lane length of 205 feet is recommended based on the design speed of 40 mph.

Figure 5. Dixie Highway Left-Turn Lane Warrant Analysis

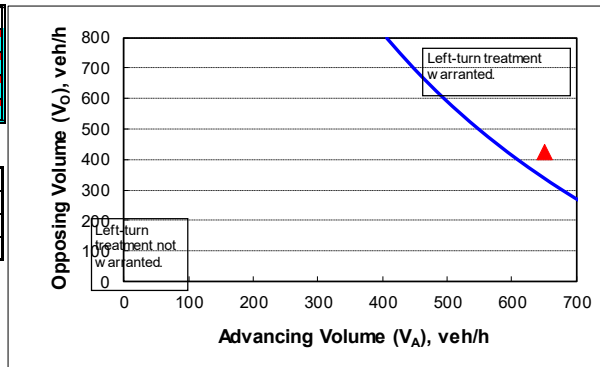
2-lane roadway (English)

INPUT

Variable	Value
85 th percentile speed, mph:	35
Percent of left-turns in advancing volume (V _A), %:	4%
Advancing volume (V _A), veh/h:	651
Opposing volume (V _O), veh/h:	426

OUTPUT

Variable	Value
Limiting advancing volume (V _A), veh/h:	593
Guidance for determining the need for a major-road left-turn bay:	
Left-turn treatment warranted.	



CALIBRATION CONSTANTS

Variable	Value
Average time for making left-turn, s:	3.0
Critical headway, s:	5.0
Average time for left-turn vehicle to clear the advancing lane, s:	1.9

MEP evaluated the Driveway 5 & NE Dixie Highway intersection using HCS 2025. With project traffic, the intersection is projected to operate acceptably with all movements operating under capacity (v/c ratio less than 1.0).

Table 5. Driveway 5 & NE Dixie Highway Storage Summary

Direction	AM 95% Queue Length (Feet)	PM 95% Queue Length (Feet)
EBL	0	25



Engineering & Planning, Inc.

Driveway 6 (NE Dixie Highway)

The site's Driveway 6 is projected to have 38 peak-hour right-turning vehicles. This is less than the FDOT's guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 6.

Driveway 7 (Green River Parkway)

The site's Driveway 7 is projected to have 24 peak-hour right-turning vehicles. This is less than the FDOT's guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 7.

Driveway 8 (Green River Parkway)

The site's Driveway 8 is projected to have 0 peak hour right-turning vehicles. This is less than the FDOT's guideline of 80-125 right-turns for a right-turn lane. Therefore, a right-turn lane is not recommended at Driveway 8.

CONCLUSION

MacKenzie Engineering & Planning, Inc. was retained to prepare an analysis of the traffic impacts from Avonlea 10-11-13-14-15. The site is located at the northwest corner of Baker Road & Green River Parkway, Stuart, Florida (Parcel -10: 28-37-41-013-000-00100-0, Parcel -11: 28-37-41-013-000-00110-0, Parcel -13: 28-37-41-013-000-00130-0, Parcel -14: 28-37-41-013-000-00140-0, Parcel -15: 28-37-41-013-000-00150-0). The applicant proposes:

- Parcel 10: 80 Low-rise residential units with ground-floor commercial units
- Parcel 11: 6,827 SF of shopping center use
- Parcel 13: 123 Multifamily Housing (Low-rise) units
- Parcels 14 & 15: 16,373 SF of shopping center use

The development is planned for completion by the end of 2030.

The project is expected to generate the following net external trips:

- 1,782 daily, 126 AM peak hour (41 in/85 out), and 179 PM peak hour (106 in/73 out) trips.

The project is expected to generate the following driveway trips:

- 2,444 daily, 156 AM peak hour (59 in/97 out), and 257 PM peak hour (145 in/112 out) trips

The traffic analysis was performed in accordance with the City of Stuart Code, Section 4.05.02 Traffic Impact Analysis. The project meets the thresholds of Section 4.05.02. The results of the analysis show that the project has no significant impacts, the surrounding roadways and intersections are projected to operate acceptably and therefore meet the City's concurrency requirements.

A 205-foot left-turn lane on Dixie Highway into Lot 13 is recommended.

EXHIBITS

Exhibit 1. Trip Generation

Exhibits 2-6. Project Driveway and Intersection Trips

Exhibit 7-16. Intersection Volumes Worksheets

Following Exhibit 16 - Intersection Analysis

APPENDICES

1. Institute of Traffic Engineers' (ITE) Report, Trip Generation (11th Edition)
2. Martin County 2022 Roadway Level of Service Inventory Report
3. Traffic Counts
4. Peak Seasonal Factor
5. FDOT Design Manual
6. Site Plan
7. Property ID Card

EXHIBIT 1
Avonlea 10-11-13-14-15
Trip Generation

Land Use	Intensity		Daily	AM Peak Hour			PM Peak Hour		
			Trips	Total	In	Out	Total	In	Out
Proposed Site Traffic									
Multifamily Housing(Mid-Rise)	123	DU	540	43	6	37	48	29	19
Low-Rise Residential w Ground Commercial	80	DU	275	35	8	27	29	21	8
Strip Retail Plaza (<40K)	23.200	1000 SF	1,209	50	30	20	142	71	71
Subtotal			2,024	128	44	84	219	121	98
Internal Capture									
	AM	PM/ DAILY							
Multifamily Housing(Mid-Rise)	0%	10.4%	56	0	0	0	5	4	1
Low-Rise Residential w Ground Commercial	3%	6.9%	19	1	1	0	2	0	2
Strip Retail Plaza (<40K)	2%	4.9%	60	1	0	1	7	3	4
Subtotal	2%	6.4%	135	2	1	1	14	7	7
Pass-By Traffic									
Multifamily Housing(Mid-Rise)	0%		0	0	0	0	0	0	0
Low-Rise Residential w Ground Commercial	0%		0	0	0	0	0	0	0
Strip Retail Plaza (<40K)	48%		552	24	14	10	65	33	32
Subtotal			552	24	14	10	65	33	32
NET PROPOSED TRIPS			1,337	102	29	73	140	81	59
Total Proposed Driveway Volumes			1,889	126	43	83	205	114	91
NET CHANGE IN TRIPS (FOR THE PURPOSES OF CONCURRENCY)			1,337	102	29	73	140	81	59
NET CHANGE IN DRIVEWAY VOLUMES			1,889	126	43	83	205	114	91

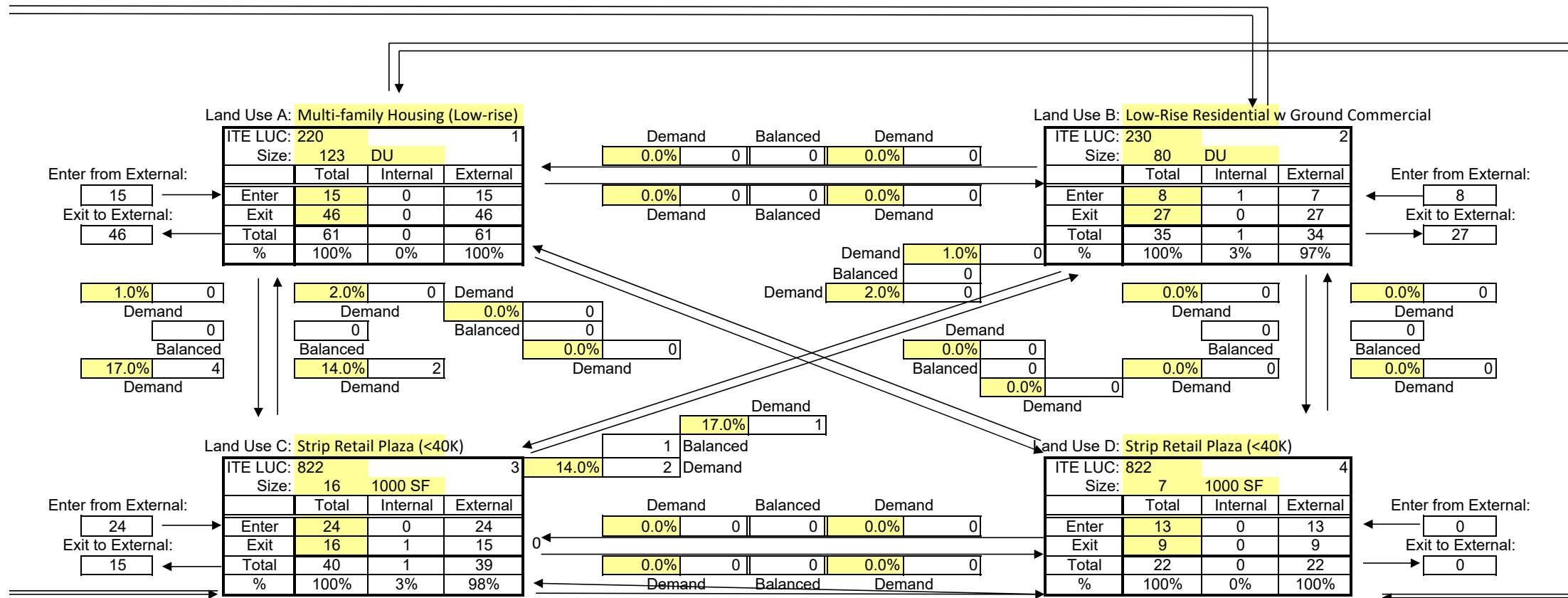
Note: Trip generation was calculated using the following data:

Land Use	ITE Code	Unit	Daily Rate	Pass-by Rate	AM Peak Hour		PM Peak Hour	
					in/out	Rate	in/out	Equation
Multifamily Housing(Mid-Rise)	221	DU	$T = 4.77 (X) + -46.46$	0%	14/86	$T = 0.44 (X) - 11.61$	61/39	$T = 0.39(X) + 0.34$
Low-Rise Residential w Ground Commercial	230	DU	3.44	0%	23/77	0.44	71/29	0.36
Strip Retail Plaza (<40K)	822	1000 SF	$T = 42.2 (X) + 229.68$	48%	60/40	$\ln(T) = 0.66 \ln(X) + 1.84$	50/50	$\ln(T) = 0.71 \ln(X) + 2.72$

EXHIBIT 1B

Analysis Period: PM____, Midday____, AM X
 Analyst: MEP
 Date: 10/25/2023

Project Number: 124005 Task Number: _____
 Project Name: Avonlea 10-11-13-14-15
 Scenario: AM Peak Hour



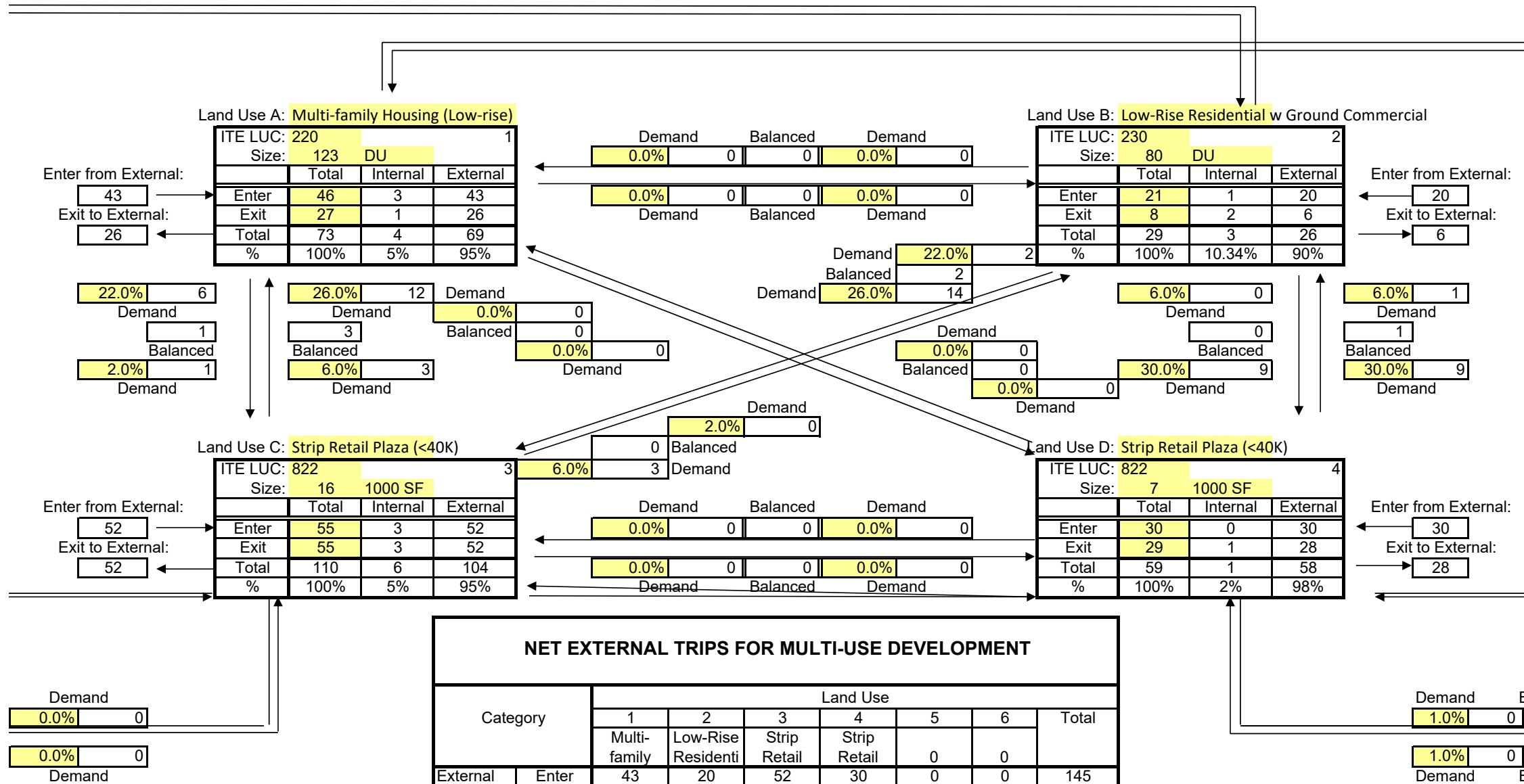
NET EXTERNAL TRIPS FOR MULTI-USE DEVELOPMENT								
Category		Land Use						Total
		1 Multi-family	2 Low-Rise	3 Strip Retail	4 Strip Retail	5 0	6 0	
External Trips	Enter	15	7	24	13	0	0	59
	Exit	46	27	15	9	0	0	97
	Total	61	34	39	22	0	0	156
Internal Trips	Enter	0	1	0	0	0	0	1
	Exit	0	0	1	0	0	0	1
	Total	0	1	1	0	0	0	2
Single Use Trip Gen Estimate		61	35	40	22	0	0	158
		0.00%	2.86%	2.50%	0.00%	0.00%	0.00%	
Internal Capture =		1.27%						

\\[Trip Generation Avonlea 10-11-13-14-15 9-6-23.xlsx]tgen

EXHIBIT 1C

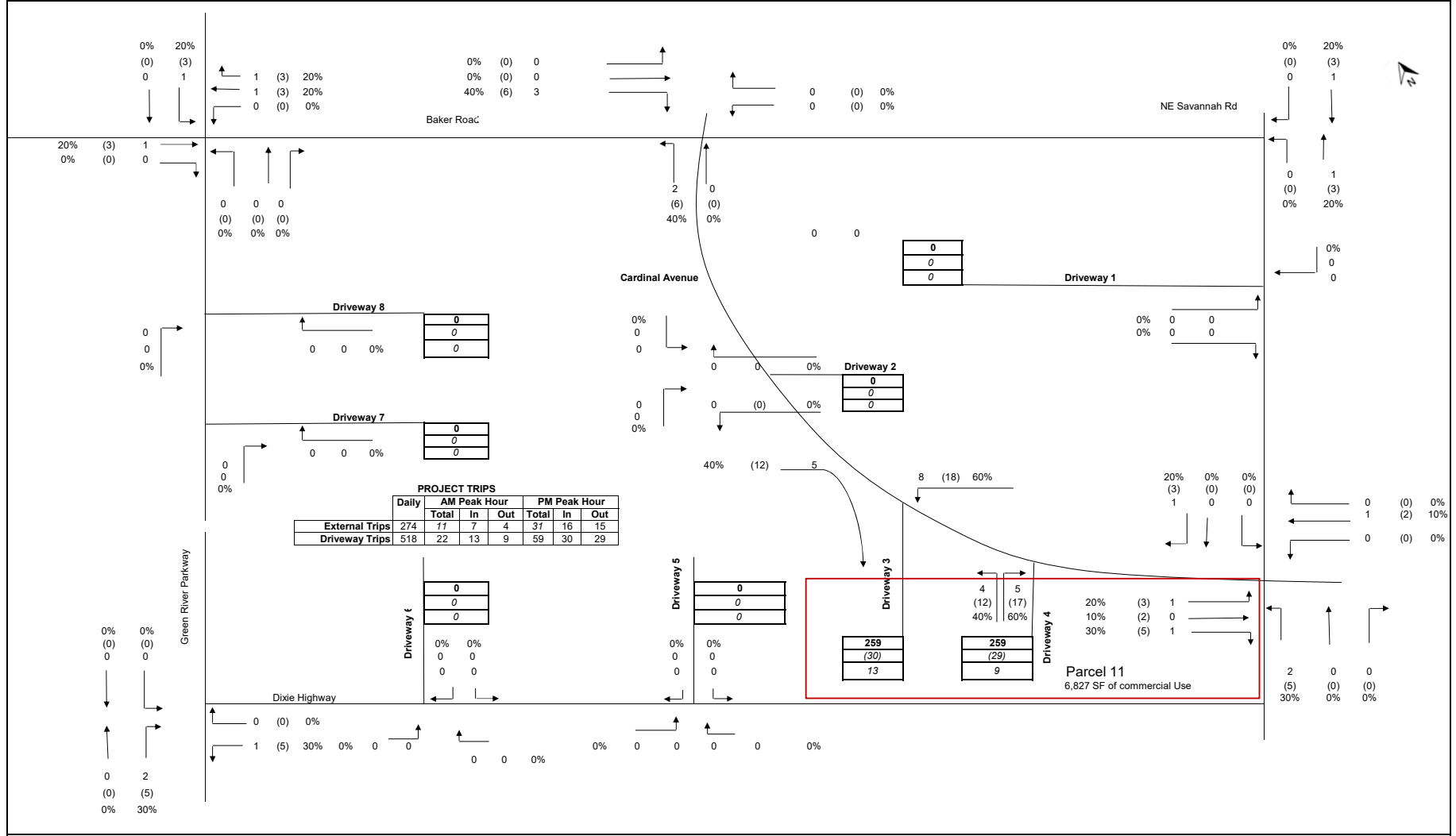
Analysis Period: PM X, AM
 Analyst: MEP
 Date: 10/25/2023

Project Number: 124005 Task Number:
 Project Name: Avonlea 10-11-13-14-15
 Scenario: PM Peak Hour



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EXHIBIT 2

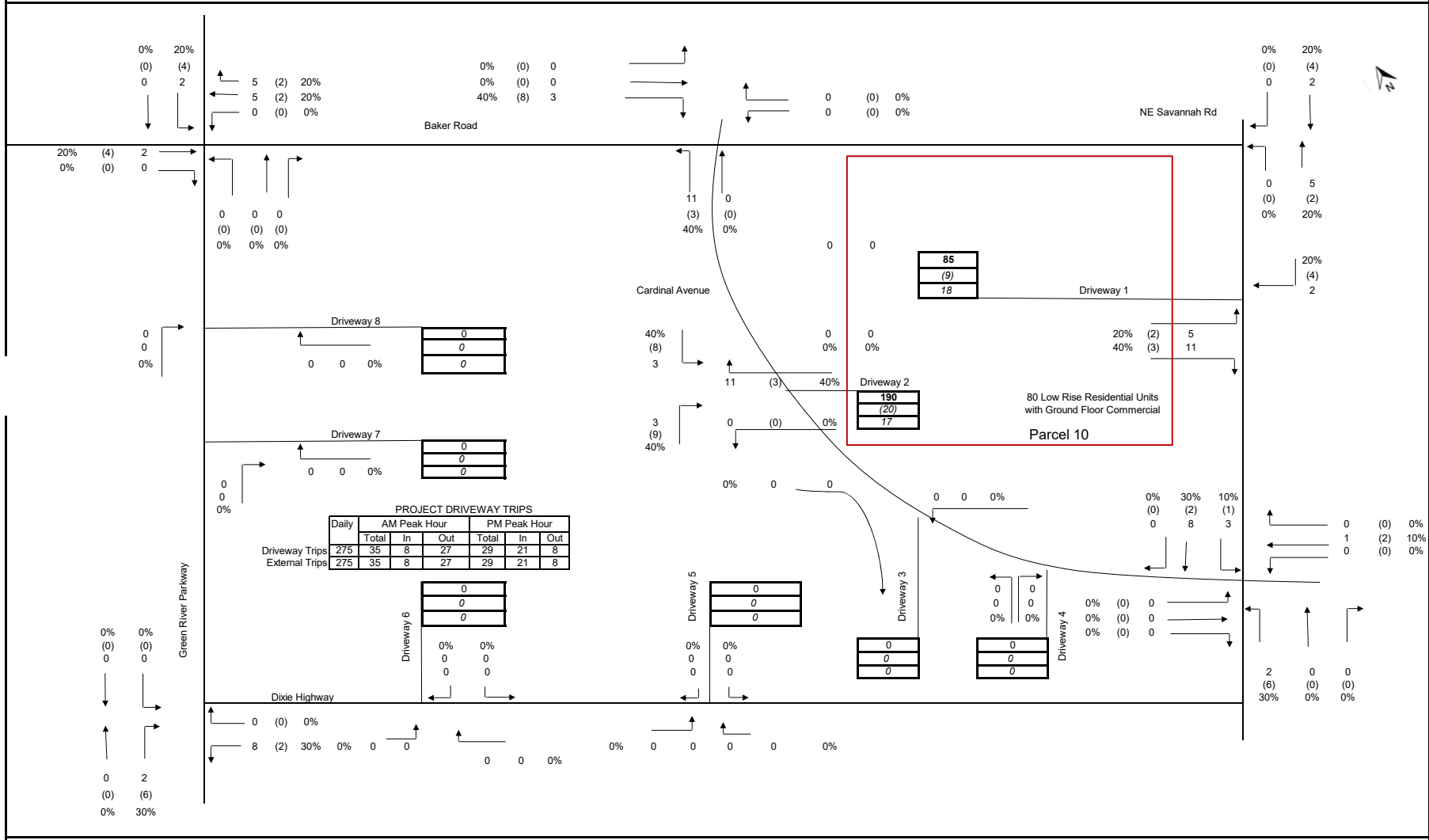


Avonlea 11
PROPOSED PROJECT VOLUMES

274	Total daily driveway trips
(31)	PM peak hour driveway trips
11	AM peak hour directional driveway trips

MacKenzie
Engineering & Planning, Inc.

EXHIBIT 3

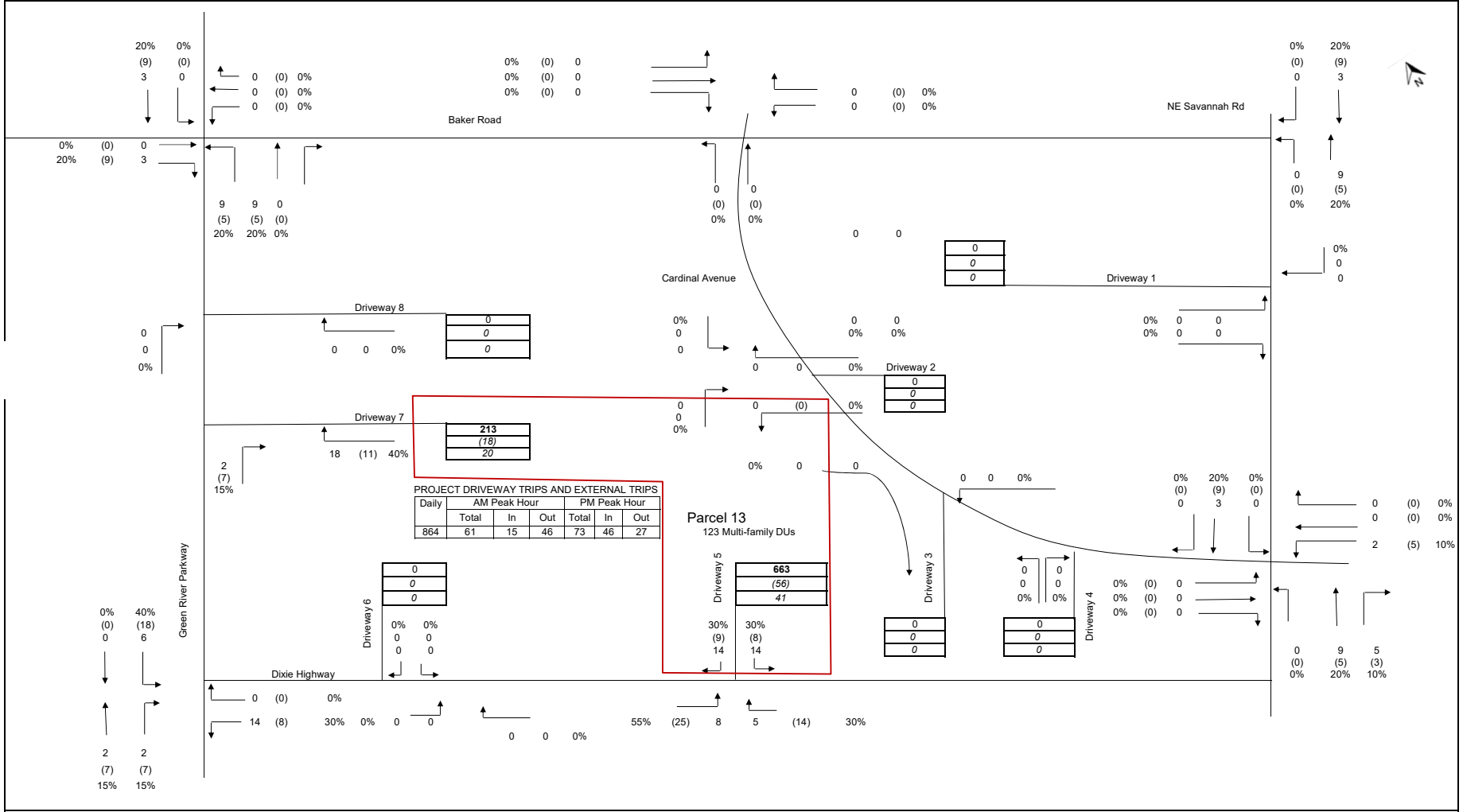


Avonlea 10
PROPOSED PROJECT VOLUMES

275	Total daily driveway trips
(29)	PM peak hour driveway trips
35	AM peak hour directional driveway trips

MacKenzie
Engineering & Planning, Inc.

EXHIBIT 4

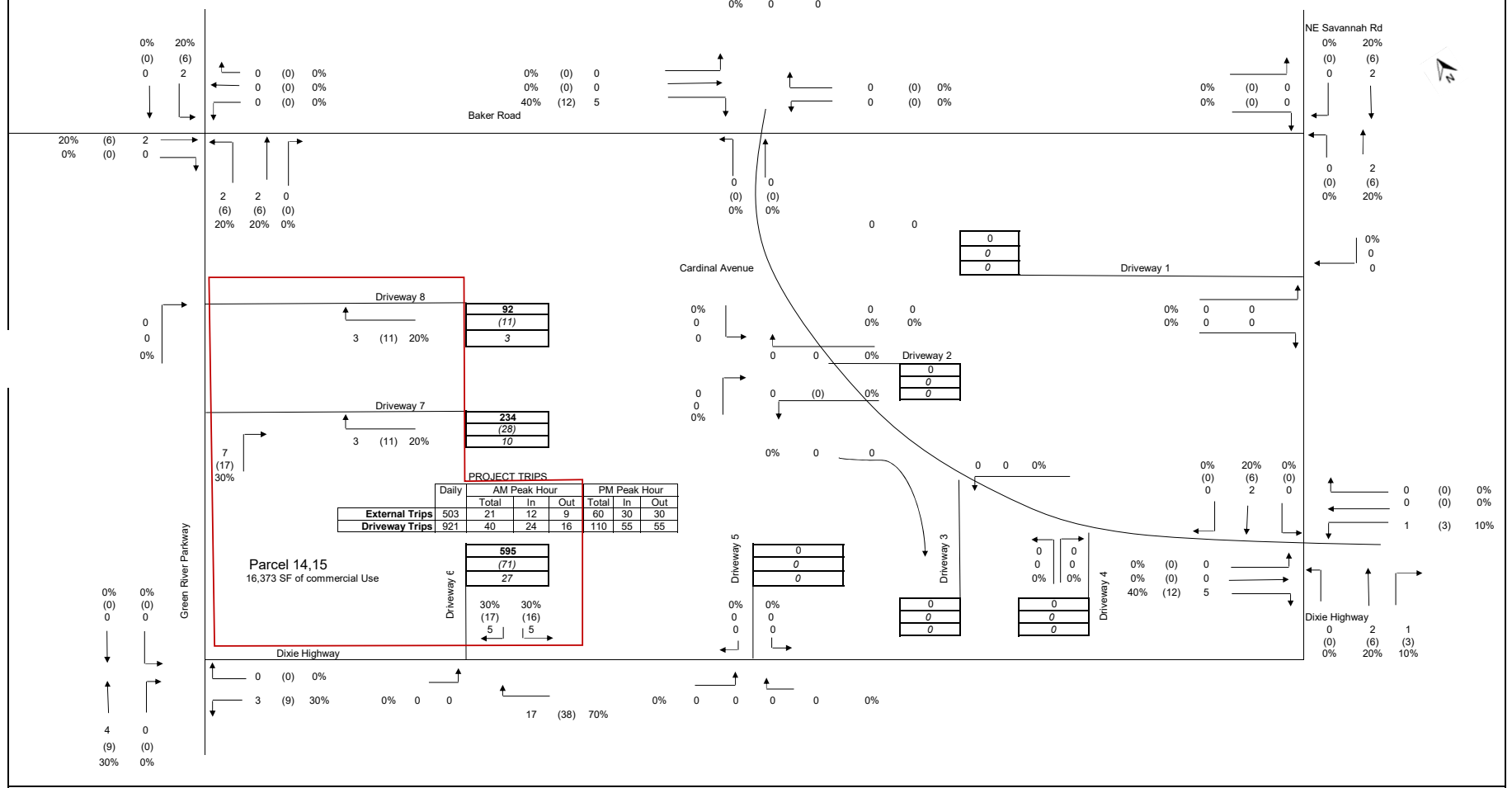


Avonlea 13
PROPOSED PROJECT VOLUMES

864	Total daily driveway trips
(73)	PM peak hour driveway trips
61	AM peak hour directional driveway trips

MacKenzie
Engineering & Planning, Inc.

EXHIBIT 5

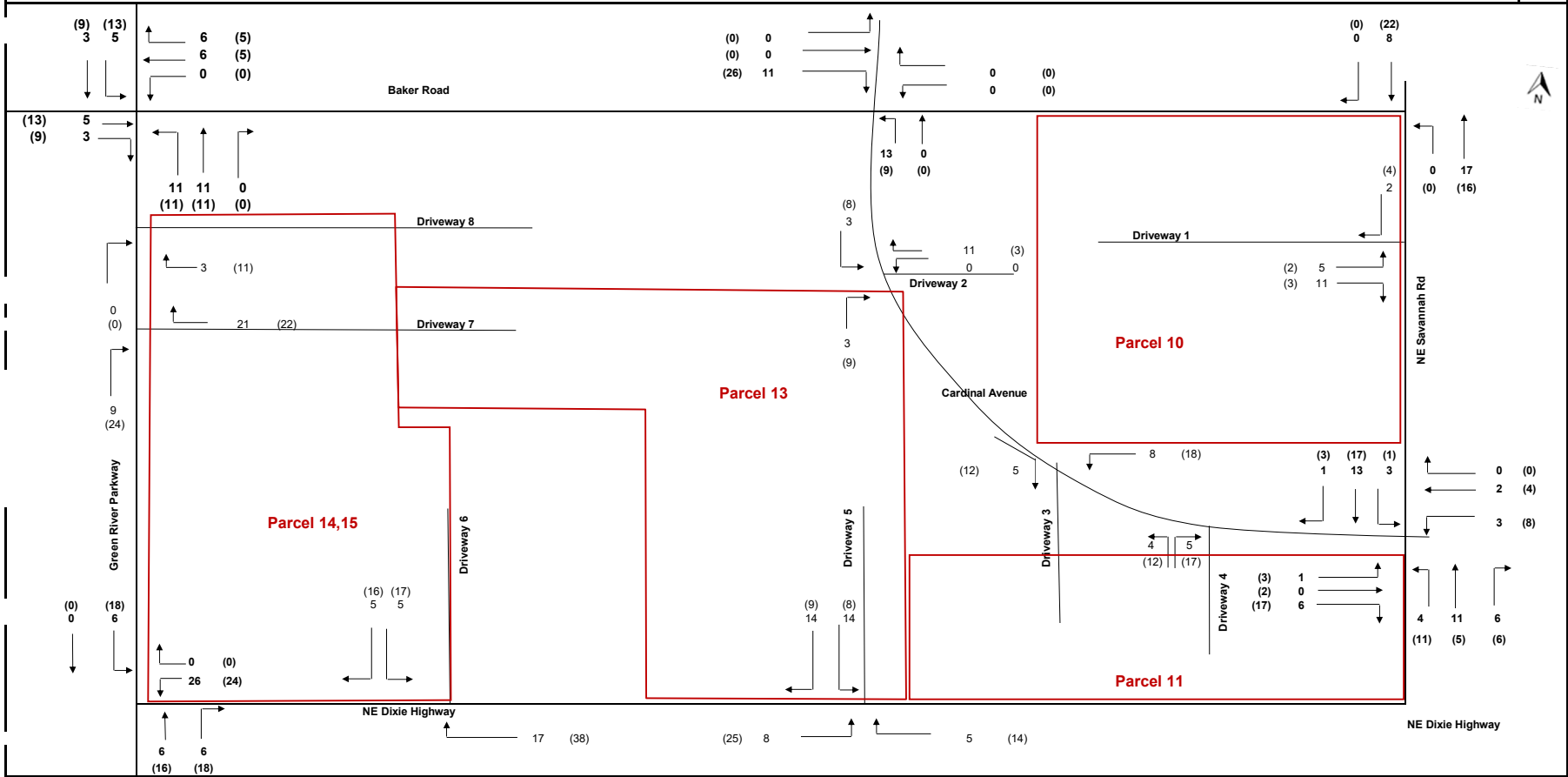


Avonlea 14-15
PROPOSED PROJECT VOLUMES

503	Total daily driveway trips
(60)	PM peak hour driveway trips
21	AM peak hour directional driveway trips

MacKenzie
Engineering & Planning, Inc.

EXHIBIT 6



Avonlea 10-11-13-14-15
PROPOSED PROJECT DRIVEWAY VOLUMES

1,337	Total daily driveway trips
740	PM peak hour driveway trips
102	AM peak hour directional driveway trips

MacKenzie
 Engineering & Planning, Inc.

Avonlea 10-11-13-14-15

AM PEAK HOUR TURNING MOVEMENTS

EXHIBIT 7

Baker Road & Green River Parkway

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
7:00 AM	7:15 AM	3	25	7	7	21	6	6	11	1	8	94	16
7:15 AM	7:30 AM	6	26	13	7	19	7	5	27	4	6	122	17
7:30 AM	7:45 AM	9	27	12	10	22	12	2	26	6	12	126	25
7:45 AM	8:00 AM	8	32	17	8	39	11	6	28	4	10	132	29
8:00 AM	8:15 AM	1	16	11	9	38	8	9	43	3	17	122	24
8:15 AM	8:30 AM	7	34	6	11	23	10	12	52	7	7	106	26
8:30 AM	8:45 AM	7	25	4	9	34	8	9	36	7	14	97	16
8:45 AM	9:00 AM	10	40	7	17	45	10	10	34	7	9	97	30
9:00 AM	9:15 AM												
9:15 AM	9:30 AM												
9:30 AM	9:45 AM												
9:45 AM	10:00 AM												
		51	225	77	78	241	72	59	257	39	83	896	183

Peak Hour Traffic Volume

7:30 AM	8:30 AM	25	109	46	38	122	41	29	149	20	46	486	104
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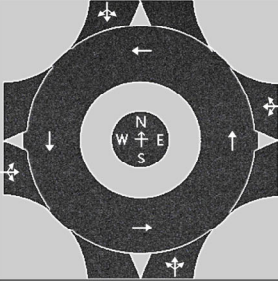
Count Taken:	6/28/2023	PHF	0.94
Buildout year:	2030		
Growth Rate:	1.3%		
PSCF	1.22		

	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/28/2023	25	109	46	38	122	41	29	149	20	46	486	104
Peak Season Factor	6	24	10	8	27	9	6	33	4	10	107	23
Adjusted Volumes	31	133	56	46	149	50	35	182	24	56	593	127
Growth 1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
2030 Volumes	34	147	62	51	165	55	39	202	27	62	658	141
Pre Dev	34	147	62	51	165	55	39	202	27	62	658	141
Project Traffic	0	5	1	0	6	6	9	9	0	5	1	0
Post Dev	34	152	63	51	171	61	48	211	27	67	659	141

Project Traffic Assignment	In	Out	Out	Out	In	In
	0%	4%	0%	7%	4%	10%
					0%	0%
					7%	10%
					0%	0%

Existing	31	133	56	46	149	50	35	182	24	56	593	127
Pre-Development	34	147	62	51	165	55	39	202	27	62	658	141
Post-Development	34	152	63	51	171	61	48	211	27	67	659	141

HCS Roundabouts Report

General Information				Site Information				
Analyst	MEP				Intersection		Green River Pkwy & Baker Rd	
Agency or Co.	MEP				E/W Street Name		Baker Road	
Date Performed	4/24/2024				N/S Street Name		Green River Pkwy	
Analysis Year	2024				Analysis Time Period, hrs		0.25	
Time Analyzed	Existing AM Peak Hour				Peak Hour Factor		0.94	
Project Description	Avonlea 10-11-13-14-15				Jurisdiction			

Volume Adjustments and Site Characteristics																
Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LTR				LTR				LTR				LTR			
Volume (V), veh/h	0	31	133	56	0	46	149	50	0	35	182	24	0	56	593	127
Percent Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Flow Rate (V _{PCE}), pc/h	0	34	146	61	0	50	163	55	0	38	199	26	0	61	650	139
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			
Proportion of CAVs, %	0															

Critical and Follow-Up Headway Adjustment													
Approach	EB			WB			NB			SB			
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	
Critical Headway, s		4.9763			4.9763			4.9763			4.9763		
Follow-Up Headway, s		2.6087			2.6087			2.6087			2.6087		

Flow Computations, Capacity and v/c Ratios													
Approach	EB			WB			NB			SB			
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	
Entry Flow (v _e), pc/h		241			268			263			850		
Entry Volume, veh/h		234			260			255			825		
Circulating Flow (v _c), pc/h	761			271			241			251			
Exiting Flow (v _{ex}), pc/h	233			340			288			761			
Capacity (C _{PCE}), pc/h		635			1047			1079			1068		
Capacity (c), veh/h		617			1016			1048			1037		
v/c Ratio (x)		0.38			0.26			0.24			0.80		

Delay and Level of Service													
Approach	EB			WB			NB			SB			
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	
Lane Control Delay (d), s/veh		11.3			6.0			5.8			19.4		
Lane LOS		B			A			A			C		
95% Queue Length, Q ₉₅ (veh)		1.8			1.0			1.0			8.8		
95% Queue Length, Q ₉₅ (ft)		46.1			25.6			25.6			225.3		
Approach Delay, s/veh LOS	11.3		B	6.0		A	5.8		A	19.4		C	
Intersection Delay, s/veh LOS	13.8						B						

HCS Roundabouts Report

General Information				Site Information				
Analyst	MEP				Intersection	Green River Pkwy & Baker Rd		
Agency or Co.	MEP				E/W Street Name	Baker Road		
Date Performed	4/24/2024				N/S Street Name	Green River Pkwy		
Analysis Year	2030				Analysis Time Period, hrs	0.25		
Time Analyzed	Pre-Dev AM Peak Hour				Peak Hour Factor	0.94		
Project Description	Avonlea 10-11-13-14-15				Jurisdiction			

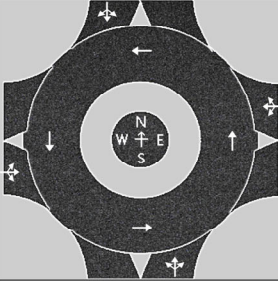
Volume Adjustments and Site Characteristics																
Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LTR				LTR				LTR				LTR			
Volume (V), veh/h	0	34	147	62	0	51	165	55	0	39	202	27	0	62	658	141
Percent Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Flow Rate (v _{PCE}), pc/h	0	37	161	68	0	56	181	60	0	43	221	30	0	68	721	154
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			
Proportion of CAVs, %	0															

Critical and Follow-Up Headway Adjustment												
Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway, s		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway, s		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios												
Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		266			297			294			943	
Entry Volume, veh/h		258			288			285			916	
Circulating Flow (v _c), pc/h	845			301			266			280		
Exiting Flow (v _{ex}), pc/h	259			378			318			845		
Capacity (C _{PCE}), pc/h		583			1015			1052			1037	
Capacity (c), veh/h		566			986			1021			1007	
v/c Ratio (x)		0.46			0.29			0.28			0.91	

Delay and Level of Service												
Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		13.9			6.6			6.3			31.1	
Lane LOS		B			A			A			D	
95% Queue Length, Q ₉₅ (veh)		2.4			1.2			1.1			13.7	
95% Queue Length, Q ₉₅ (ft)		61.4			30.7			28.2			350.7	
Approach Delay, s/veh LOS	13.9		B	6.6		A	6.3		A	31.1		D
Intersection Delay, s/veh LOS	20.4						C					

HCS Roundabouts Report

General Information				Site Information				
Analyst	MEP				Intersection	Green River Pkwy & Baker Rd		
Agency or Co.					E/W Street Name	Baker Road		
Date Performed	4/24/2024				N/S Street Name	Green River Pkwy		
Analysis Year	2030				Analysis Time Period, hrs	0.25		
Time Analyzed	Post-Dev AM Peak Hour				Peak Hour Factor	0.94		
Project Description	Avonlea 10-11-13-14-15				Jurisdiction			

Volume Adjustments and Site Characteristics																
Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LTR				LTR				LTR				LTR			
Volume (V), veh/h	0	34	152	63	0	51	171	61	0	48	211	27	0	67	659	141
Percent Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Flow Rate (V _{PCE}), pc/h	0	37	167	69	0	56	187	67	0	53	231	30	0	73	722	154
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			
Proportion of CAVs, %	0															

Critical and Follow-Up Headway Adjustment												
Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway, s		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway, s		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios												
Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		273			310			314			949	
Entry Volume, veh/h		265			301			305			921	
Circulating Flow (v _c), pc/h	851			321			277			296		
Exiting Flow (v _{ex}), pc/h	270			394			335			847		
Capacity (C _{PCE}), pc/h		579			995			1040			1020	
Capacity (c), veh/h		562			966			1010			991	
v/c Ratio (x)		0.47			0.31			0.30			0.93	

Delay and Level of Service												
Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		14.3			7.0			6.6			34.6	
Lane LOS		B			A			A			D	
95% Queue Length, Q ₉₅ (veh)		2.5			1.3			1.3			14.8	
95% Queue Length, Q ₉₅ (ft)		64.0			33.3			33.3			378.9	
Approach Delay, s/veh LOS	14.3		B	7.0		A	6.6		A	34.6		D
Intersection Delay, s/veh LOS	22.2						C					

Avonlea 10-11-13-14-15

PM PEAK HOUR TURNING MOVEMENTS

EXHIBIT 8

Baker Road & Green River Parkway

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
4:00 PM	4:15 PM	16	33	12	4	34	18	19	76	7	13	57	23
4:15 PM	4:30 PM	15	38	9	6	43	29	12	103	15	11	62	21
4:30 PM	4:45 PM	12	33	11	8	49	30	13	112	11	9	62	15
4:45 PM	5:00 PM	14	43	13	10	37	22	13	95	16	13	56	23
5:00 PM	5:15 PM	17	40	13	5	43	27	14	117	7	8	63	22
5:15 PM	5:30 PM	16	49	6	5	44	27	11	108	10	10	81	21
5:30 PM	5:45 PM	10	39	11	6	32	25	10	89	5	16	58	13
5:45 PM	6:00 PM	11	24	7	5	28	20	9	73	9	11	60	19
6:00 PM	6:15 PM												
6:15 PM	6:30 PM												
6:30 PM	6:45 PM												
6:45 PM	7:00 PM												
		111	299	82	49	310	198	101	773	80	91	499	157

Peak Hour Traffic Volume

4:30 PM	5:30 PM	59	165	43	28	173	106	51	432	44	40	262	81
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Count Taken:	6/28/2023	PHF	0.96	0.95
Buildout year:	2030			
Growth Rate:	1.3%			
PSCF	1.22			

	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/28/2023	59	165	43	28	173	106	51	432	44	40	262	81
Peak Season Factor	13	36	9	6	38	23	11	95	10	9	58	18
Adjusted Volumes	72	201	52	34	211	129	62	527	54	49	320	99
	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
Growth 1.3%	8	22	6	4	23	14	7	57	6	5	35	11
2030 Volumes	80	223	58	38	234	143	69	584	60	54	355	110
Pre Dev	80	223	58	38	234	143	69	584	60	54	355	110
Project Traffic	0	13	6	0	5	5	10	10	0	13	6	0
Post Dev	80	236	64	38	239	148	79	594	60	67	361	110

Project Traffic Assignment	In	Out	Out	Out	In	In
	0%	4%	0%	7%	4%	10%
					0%	0%
					7%	10%
					0%	0%

Existing	72	201	52	34	211	129	62	527	54	49	320	99
Pre-Development	80	223	58	38	234	143	69	584	60	54	355	110
Post-Development	80	236	64	38	239	148	79	594	60	67	361	110

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Green River Hwy & Baker
Agency or Co.			E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Green River Hwy
Analysis Year	2024		Analysis Time Period (hrs)	0.25
Time Analyzed	Existing PM peak Hour		Peak Hour Factor	0.95
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LTR				LTR				LTR				LTR			
Volume (V), veh/h	0	72	201	52	0	34	211	129	0	62	527	54	0	49	320	99
Percent Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Flow Rate (v _{PCE}), pc/h	0	78	218	56	0	37	229	140	0	67	571	59	0	53	347	107
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		352			406			697			507	
Entry Volume, veh/h		342			394			677			492	
Circulating Flow (v _c), pc/h	437			716			349			333		
Exiting Flow (v _{ex}), pc/h	330			403			789			440		
Capacity (C _{PCE}), pc/h		884			665			967			983	
Capacity (c), veh/h		858			645			939			954	
v/c Ratio (x)		0.40			0.61			0.72			0.52	

Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		8.9			17.0			16.7			10.3	
Lane LOS		A			C			C			B	
95% Queue, veh		1.9			4.2			6.5			3.0	
Approach Delay, s/veh	8.9			17.0			16.7			10.3		
Approach LOS	A			C			C			B		
Intersection Delay, s/veh LOS	13.7						B					

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Green River Hwy & Baker
Agency or Co.			E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Green River Hwy
Analysis Year	2030		Analysis Time Period (hrs)	0.25
Time Analyzed	Pre-Dev PM peak Hour		Peak Hour Factor	0.95
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LTR				LTR				LTR				LTR			
Volume (V), veh/h	0	80	223	58	0	38	234	143	0	69	584	60	0	54	355	110
Percent Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Flow Rate (v _{PCE}), pc/h	0	87	242	63	0	41	254	155	0	75	633	65	0	59	385	119
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		392			450			773			563	
Entry Volume, veh/h		381			437			750			547	
Circulating Flow (v _c), pc/h	485			795			388			370		
Exiting Flow (v _{ex}), pc/h	366			448			875			489		
Capacity (C _{PCE}), pc/h		841			613			929			946	
Capacity (c), veh/h		817			595			902			919	
v/c Ratio (x)		0.47			0.73			0.83			0.60	

Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		10.5			24.5			24.4			12.5	
Lane LOS		B			C			C			B	
95% Queue, veh		2.5			6.3			9.8			4.1	
Approach Delay, s/veh	10.5			24.5			24.4			12.5		
Approach LOS	B			C			C			B		
Intersection Delay, s/veh LOS	18.9						C					

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Green River Hwy & Baker
Agency or Co.			E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Green River Hwy
Analysis Year	2030		Analysis Time Period (hrs)	0.25
Time Analyzed	Post-Dev PM peak Hour		Peak Hour Factor	0.95
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LTR				LTR				LTR				LTR			
Volume (V), veh/h	0	80	236	64	0	38	239	148	0	79	594	60	0	67	361	110
Percent Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Flow Rate (v _{PCE}), pc/h	0	87	256	69	0	41	259	160	0	86	644	65	0	73	391	119
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		412			460			795			583	
Entry Volume, veh/h		400			447			772			566	
Circulating Flow (v _c), pc/h	505			817			416			386		
Exiting Flow (v _{ex}), pc/h	394			464			891			501		
Capacity (C _{PCE}), pc/h		824			600			903			931	
Capacity (c), veh/h		800			582			877			904	
v/c Ratio (x)		0.50			0.77			0.88			0.63	

Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		11.4			27.5			30.1			13.5	
Lane LOS		B			D			D			B	
95% Queue, veh		2.8			7.0			11.7			4.5	
Approach Delay, s/veh	11.4			27.5			30.1			13.5		
Approach LOS	B			D			D			B		
Intersection Delay, s/veh LOS	21.9						C					

Avonlea 10-11-13-14-15

AM PEAK HOUR TURNING MOVEMENTS

EXHIBIT 9

Dixie Highway & Green River Parkway

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
7:00 AM	7:15 AM	0	0	0	84	0	2	0	24	35	8	89	0
7:15 AM	7:30 AM	0	0	0	103	0	2	0	30	37	2	96	0
7:30 AM	7:45 AM	0	0	0	103	0	4	0	34	47	3	129	0
7:45 AM	8:00 AM	0	0	0	111	0	1	0	46	57	10	118	0
8:00 AM	8:15 AM	0	0	0	110	0	2	0	35	74	5	113	0
8:15 AM	8:30 AM	0	0	0	119	0	1	0	40	67	9	104	0
8:30 AM	8:45 AM	0	0	0	95	0	2	0	35	49	3	118	0
8:45 AM	9:00 AM	0	0	0	68	0	5	0	39	57	8	92	0
9:00 AM	9:15 AM												
9:15 AM	9:30 AM												
9:30 AM	9:45 AM												
9:45 AM	10:00 AM												
		0	0	0	793	0	19	0	283	423	48	859	0

Peak Hour Traffic Volume

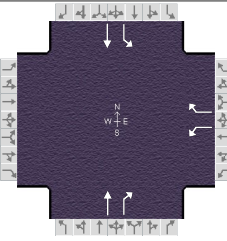
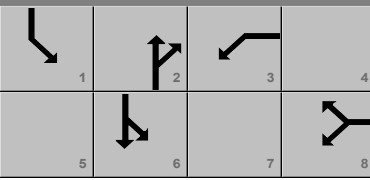
7:30 AM	8:30 AM	0	0	0	443	0	8	0	155	245	27	464	0
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Count Taken: 6/29/2023 PHF 0.98
 Buildout year: 2030
 Growth Rate: 1.3%
 PSCF 1.22

	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/29/2023	0	0	0	443	0	8	0	155	245	27	464	0
Peak Season Factor	0	0	0	97	0	2	0	34	54	6	102	0
Adjusted Volumes	0	0	0	540	0	10	0	189	299	33	566	0
Growth 1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
2030 Volumes	0	0	0	599	0	11	0	210	332	37	628	0
Pre Dev	0	0	0	599	0	11	0	210	332	37	628	0
Project Traffic	0	0	0	23	0	0	0	5	5	2	0	0
Post Dev	0	0	0	622	0	11	0	215	337	39	628	0

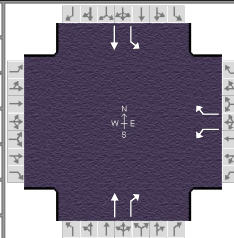
Project Traffic Assignment	In	In	Out	Out	Out	Out	In	In	Out	Out	Out	
Existing	0%	4%	0%	7%	4%	10%	0%	0%	7%	10%	0%	0%
Pre-Development	0	0	0	540	0	10	0	189	299	33	566	0
Post-Development	0	0	0	599	0	11	0	210	332	37	628	0
	0	0	0	622	0	11	0	215	337	39	628	0

HCS7 Signalized Intersection Input Data

General Information					Intersection Information												
Agency	MEP				Duration, h	0.250											
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other										
Jurisdiction					Time Period												
Urban Street	Green River Parkway		Analysis Year	2024		PHF	0.95										
Intersection	NE Dixie Hwy and Gree...		File Name	Green River and Dixie AM.xus													
Project Description	AM Existing																
Demand Information					EB			WB			NB			SB			
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h								540		10		189	299	33	566		
Signal Information																	
Cycle, s	54.9	Reference Phase	2														
Offset, s	0	Reference Point	End		Green	1.7	15.0	19.8	0.0	0.0	0.0						
Uncoordinated	Yes	Simult. Gap E/W	On		Yellow	4.0	4.0	4.0	0.0	0.0	0.0						
Force Mode	Fixed	Simult. Gap N/S	On		Red	2.0	2.2	2.3	0.0	0.0	0.0						
Traffic Information					EB			WB			NB			SB			
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h								540		10		189	299	33	566		
Initial Queue (Q _b), veh/h								0		0		0	0	0	0		
Base Saturation Flow Rate (s ₀), veh/h								1900		1900		1900	1900	1900	1900		
Parking (N _m), man/h									None			None			None		
Heavy Vehicles (P _{HV}), %								2		2		2	2	2	2		
Ped / Bike / RTOR, /h					0	0		0	0		0	0	0	0	0	0	
Buses (N _b), buses/h								0	0	0	0	0	0	0	0	0	
Arrival Type (AT)								3		3		3	3	3	3		
Upstream Filtering (I)								1.00		1.00		1.00	1.00	1.00	1.00		
Lane Width (W), ft								12.0		12.0		12.0	12.0	12.0	12.0		
Turn Bay Length, ft								0		0		0	0	0	0		
Grade (P _g), %						0			0			0			0		
Speed Limit, mi/h								35		35		35	35	35	35		
Phase Information					EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT					
Maximum Green (G _{max}) or Phase Split, s							45.0	45.0		55.0	15.0	55.0					
Yellow Change Interval (Y), s							4.0	4.0		4.0	4.0	4.0					
Red Clearance Interval (R _c), s							2.3	2.3		2.2	2.0	2.2					
Minimum Green (G _{min}), s							7	7		15	4	15					
Start-Up Lost Time (l _t), s							2.0			2.0	2.0	2.0					
Extension of Effective Green (e), s							2.0			2.0	2.0	2.0					
Passage (PT), s							2.0	2.0		2.0	2.0	2.0					
Recall Mode							Off	Off		Min	Off	Min					
Dual Entry							No	Yes		Yes	No	Yes					
Walk (Walk), s						0.0		0.0		0.0		0.0					
Pedestrian Clearance Time (PC), s						0.0		0.0		0.0		0.0					
Multimodal Information					EB			WB			NB			SB			
85th % Speed / Rest in Walk / Corner Radius					0	No	25	0	No	25	0	No	25				
Walkway / Crosswalk Width / Length, ft					9.0	12	0	9.0	12	0	9.0	12	0				
Street Width / Island / Curb						0		0	0	No	0	0	No	0		No	
Width Outside / Bike Lane / Shoulder, ft								12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	
Pedestrian Signal / Occupied Parking					No			No	0.50		No	0.50			0.50		

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP			Analysis Date	9/2/2023		
Jurisdiction				Area Type	Other		
Urban Street	Green River Parkway			PHF	0.95		
Intersection	NE Dixie Hwy and Gree...			Analysis Year	2024		
Project Description	AM Existing			Analysis Period	1 > 7:00		
				File Name	Green River and Dixie AM.xus		



Demand Information	EB			WB			NB			SB			
	L	T	R	L	T	R	L	T	R	L	T	R	
Approach Movement													
Demand (v), veh/h				540		10			189	299	33	566	

Signal Information														
Cycle, s	54.9	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.7	15.0	19.8	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0				
				Red	2.0	2.2	2.3	0.0	0.0	0.0				

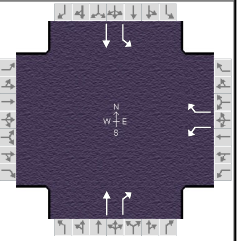
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				8		2	1	6
Case Number				9.0		7.3	1.0	4.0
Phase Duration, s				26.1		21.2	7.7	28.9
Change Period, (Y+R _c), s				6.3		6.2	6.0	6.2
Max Allow Headway (MAH), s				3.1		3.1	3.1	3.1
Queue Clearance Time (g _s), s				18.5		11.9	2.7	17.1
Green Extension Time (g _e), s				1.2		2.4	0.0	2.4
Phase Call Probability				1.00		1.00	0.41	1.00
Max Out Probability				0.00		0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3		18	2	12	1	6		
Adjusted Flow Rate (v), veh/h				568		11	199	315	35	596		
Adjusted Saturation Flow Rate (s), veh/h/ln				1781		1585	1870	1585	1781	1870		
Queue Service Time (g _s), s				16.5		0.2	4.8	9.9	0.7	15.1		
Cycle Queue Clearance Time (g _c), s				16.5		0.2	4.8	9.9	0.7	15.1		
Green Ratio (g/C)				0.36		0.36	0.27	0.27	0.34	0.41		
Capacity (c), veh/h				642		571	511	433	406	771		
Volume-to-Capacity Ratio (X)				0.886		0.018	0.389	0.727	0.086	0.772		
Back of Queue (Q), ft/ln (95 th percentile)				249.7		3.2	82.5	148.3	11.4	229.9		
Back of Queue (Q), veh/ln (95 th percentile)				9.8		0.1	3.2	5.8	0.4	9.1		
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00	0.00	0.00	0.00	0.00		
Uniform Delay (d ₁), s/veh				16.5		11.3	16.3	18.1	12.7	13.9		
Incremental Delay (d ₂), s/veh				1.7		0.0	0.2	0.9	0.0	0.6		
Initial Queue Delay (d ₃), s/veh				0.0		0.0	0.0	0.0	0.0	0.0		
Control Delay (d), s/veh				18.2		11.3	16.4	19.0	12.7	14.6		
Level of Service (LOS)				B		B	B	B	B	B		
Approach Delay, s/veh / LOS	0.0			18.1		B	18.0	B	14.5		B	
Intersection Delay, s/veh / LOS				16.7					B			

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP			Analysis Date	9/2/2023		
Jurisdiction				Area Type	Other		
Urban Street	Green River Parkway			PHF	0.95		
Intersection	NE Dixie Hwy and Gree...			Analysis Year	2024		
Project Description	AM Existing			Analysis Period	1 > 7:00		
	File Name			Green River and Dixie AM.xus			



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				540		10		189	299	33	566	

Signal Information													
Cycle, s	54.9	Reference Phase	2										
Offset, s	0	Reference Point	End	Green	1.7	15.0	19.8	0.0	0.0	0.0			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.0	2.2	2.3	0.0	0.0	0.0			

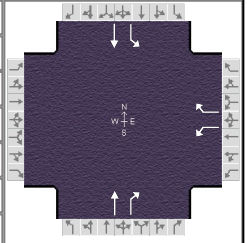
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	1.000	0.984	1.000	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		1.000	1.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.000	0.847		0.000	0.847		1.000	1.000
Left-Turn Pedestrian Adjustment Factor (f_{LPB})				1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})						1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1781	0	1585	0	1870	1585	1781	1870	0
Proportion of Vehicles Arriving on Green (P)	0.00	0.00	0.00	0.36	0.00	0.36	0.00	0.27	0.27	0.03	0.41	0.00
Incremental Delay Factor (k)				0.04		0.04		0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)				6.3		6.2	6.0	6.2
Green Ratio (g/C)				0.36		0.27	0.34	0.41
Permitted Saturation Flow Rate (s_p), veh/h/ln				1781		835	1184	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln						0		
Permitted Effective Green Time (g_p), s				0.0		0.0	17.0	0.0
Permitted Service Time (g_u), s				0.0		0.0	10.3	0.0
Permitted Queue Service Time (g_{ps}), s							0.2	
Time to First Blockage (g_t), s				0.0		15.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

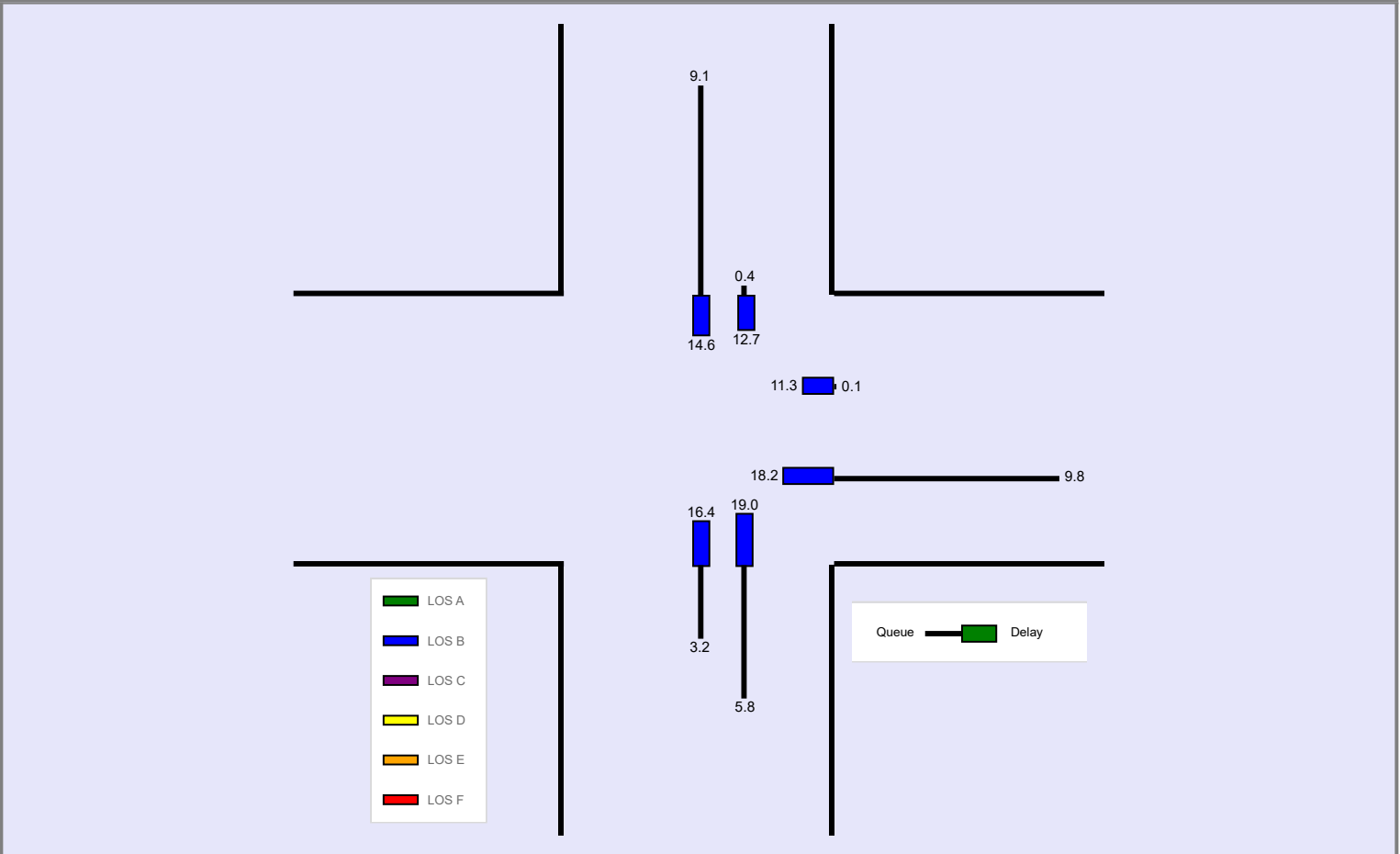
General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other
Jurisdiction				Time Period			
Urban Street	Green River Parkway		Analysis Year	2024		Analysis Period	1 > 7:00
Intersection	NE Dixie Hwy and Gree...		File Name	Green River and Dixie AM.xus			
Project Description	AM Existing						



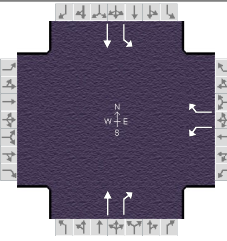
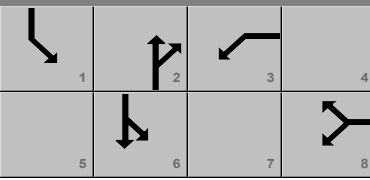
Demand Information	EB			WB			NB			SB			
	L	T	R	L	T	R	L	T	R	L	T	R	
Approach Movement													
Demand (v), veh/h				540		10			189	299	33	566	

Signal Information														
Cycle, s	54.9	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.7	15.0	19.8	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0				
				Red	2.0	2.2	2.3	0.0	0.0	0.0				

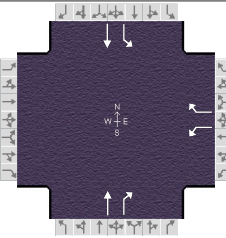
Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)				249.7		3.2	82.5	148.3	11.4	229.9		
Back of Queue (Q), veh/ln (95 th percentile)				9.8		0.1	3.2	5.8	0.4	9.1		
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00	0.00	0.00	0.00	0.00		
Control Delay (d), s/veh				18.2		11.3	16.4	19.0	12.7	14.6		
Level of Service (LOS)				B		B	B	B	B	B		
Approach Delay, s/veh / LOS	0.0			18.1		B	18.0	B	14.5			B
Intersection Delay, s/veh / LOS	16.7						B					



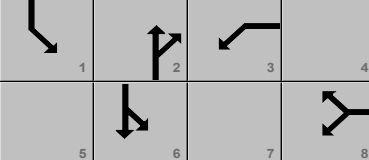
HCS7 Signalized Intersection Input Data

General Information					Intersection Information											
Agency	MEP				Duration, h	0.250										
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other									
Jurisdiction					Time Period											
Urban Street	Green River Parkway		Analysis Year	2030		PHF	0.95									
Intersection	NE Dixie Hwy		File Name	Green River and Dixie AM.xus												
Project Description	AM Pre-Dev															
Demand Information					EB			WB			NB			SB		
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h								599		11		210	332	37	628	
Signal Information																
Cycle, s	62.8	Reference Phase	2		Green	2.0	17.7	24.6	0.0	0.0	0.0					
Offset, s	0	Reference Point	End		Yellow	4.0	4.0	4.0	0.0	0.0	0.0					
Uncoordinated	Yes	Simult. Gap E/W	On		Red	2.0	2.2	2.3	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On													
Traffic Information					EB			WB			NB			SB		
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h								599		11		210	332	37	628	
Initial Queue (Q _b), veh/h								0		0		0	0	0	0	
Base Saturation Flow Rate (s ₀), veh/h								1900		1900		1900	1900	1900	1900	
Parking (N _m), man/h									None			None			None	
Heavy Vehicles (P _{HV}), %								2		2		2	2	2	2	
Ped / Bike / RTOR, /h					0	0		0	0		0	0	0	0	0	
Buses (N _b), buses/h								0	0	0	0	0	0	0	0	0
Arrival Type (AT)								3		3		3	3	3	3	
Upstream Filtering (I)								1.00		1.00		1.00	1.00	1.00	1.00	
Lane Width (W), ft								12.0		12.0		12.0	12.0	12.0	12.0	
Turn Bay Length, ft								0		0		0	0	0	0	
Grade (P _g), %						0			0			0			0	
Speed Limit, mi/h								35		35		35	35	35	35	
Phase Information					EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Maximum Green (G _{max}) or Phase Split, s							45.0	45.0		55.0	15.0	55.0				
Yellow Change Interval (Y), s							4.0	4.0		4.0	4.0	4.0				
Red Clearance Interval (R _c), s							2.3	2.3		2.2	2.0	2.2				
Minimum Green (G _{min}), s							7	7		15	4	15				
Start-Up Lost Time (l _t), s							2.0			2.0	2.0	2.0				
Extension of Effective Green (e), s							2.0			2.0	2.0	2.0				
Passage (PT), s							2.0	2.0		2.0	2.0	2.0				
Recall Mode							Off	Min		Min	Off	Min				
Dual Entry							No	Yes		Yes	No	Yes				
Walk (Walk), s						0.0		0.0		0.0		0.0				
Pedestrian Clearance Time (PC), s						0.0		0.0		0.0		0.0				
Multimodal Information					EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius					0	No	25	0	No	25	0	No	25			
Walkway / Crosswalk Width / Length, ft					9.0	12	0	9.0	12	0	9.0	12	0			
Street Width / Island / Curb						0		0	0	No	0	0	No	0		No
Width Outside / Bike Lane / Shoulder, ft								12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking					No			No	0.50		No	0.50			0.50	

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP			Analysis Date	9/2/2023		
Jurisdiction				Area Type	Other		
Urban Street	Green River Parkway			PHF	0.95		
Intersection	NE Dixie Hwy			Analysis Year	2030		
Project Description	AM Pre-Dev			File Name	Green River and Dixie AM.xus		
							

Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				599		11		210	332	37	628	

Signal Information													
Cycle, s	62.8	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.0	17.7	24.6	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

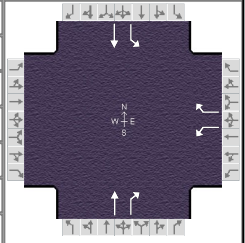
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				8		2	1	6
Case Number				9.0		7.3	1.0	4.0
Phase Duration, s				30.9		23.9	8.0	31.9
Change Period, ($Y+R_c$), s				6.3		6.2	6.0	6.2
Max Allow Headway (MAH), s				3.1		3.1	3.1	3.1
Queue Clearance Time (g_s), s				23.0		14.8	2.9	22.4
Green Extension Time (g_e), s				1.4		2.7	0.0	2.7
Phase Call Probability				1.00		1.00	0.50	1.00
Max Out Probability				0.00		0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3		18	2	12		1	6	
Adjusted Flow Rate (v), veh/h				631		12	221	349		39	661	
Adjusted Saturation Flow Rate (s), veh/h/ln				1781		1585	1870	1585		1781	1870	
Queue Service Time (g_s), s				21.0		0.3	6.1	12.8		0.9	20.4	
Cycle Queue Clearance Time (g_c), s				21.0		0.3	6.1	12.8		0.9	20.4	
Green Ratio (g/C)				0.39		0.39	0.28	0.28		0.35	0.41	
Capacity (c), veh/h				700		623	528	448		391	765	
Volume-to-Capacity Ratio (X)				0.901		0.019	0.418	0.781		0.100	0.864	
Back of Queue (Q), ft/ln (95 th percentile)				311.9		4	110	198.7		15.4	310.7	
Back of Queue (Q), veh/ln (95 th percentile)				12.3		0.2	4.3	7.8		0.6	12.2	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00	0.00	0.00		0.00	0.00	
Uniform Delay (d_1), s/veh				18.0		11.7	18.5	20.9		14.3	17.1	
Incremental Delay (d_2), s/veh				1.8		0.0	0.2	1.1		0.0	1.2	
Initial Queue Delay (d_3), s/veh				0.0		0.0	0.0	0.0		0.0	0.0	
Control Delay (d), s/veh				19.8		11.7	18.7	22.0		14.4	18.2	
Level of Service (LOS)				B		B	B	C		B	B	
Approach Delay, s/veh / LOS	0.0			19.7		B	20.7	C		18.0	B	
Intersection Delay, s/veh / LOS				19.4				B				

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other
Jurisdiction				Time Period			
Urban Street	Green River Parkway		Analysis Year	2030		Analysis Period	1 > 7:00
Intersection	NE Dixie Hwy		File Name	Green River and Dixie AM.xus			
Project Description	AM Pre-Dev						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				599		11		210	332	37	628	

Signal Information													
Cycle, s	62.8	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.0	17.7	24.6	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

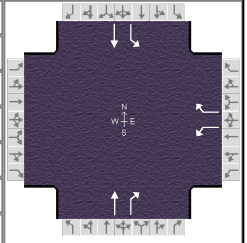
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	1.000	0.984	1.000	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		1.000	1.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.000	0.847		0.000	0.847		1.000	1.000
Left-Turn Pedestrian Adjustment Factor (f_{LPB})				1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})						1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1781	0	1585	0	1870	1585	1781	1870	0
Proportion of Vehicles Arriving on Green (P)	0.00	0.00	0.00	0.39	0.00	0.39	0.00	0.28	0.28	0.03	0.41	0.00
Incremental Delay Factor (k)				0.04		0.04		0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)				6.3		6.2	6.0	6.2
Green Ratio (g/C)				0.39		0.28	0.35	0.41
Permitted Saturation Flow Rate (s_p), veh/h/ln				1781		786	1160	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln						0		
Permitted Effective Green Time (g_p), s				0.0		0.0	19.9	0.0
Permitted Service Time (g_u), s				0.0		0.0	12.0	0.0
Permitted Queue Service Time (g_{ps}), s							0.3	
Time to First Blockage (g_t), s				0.0		17.9	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

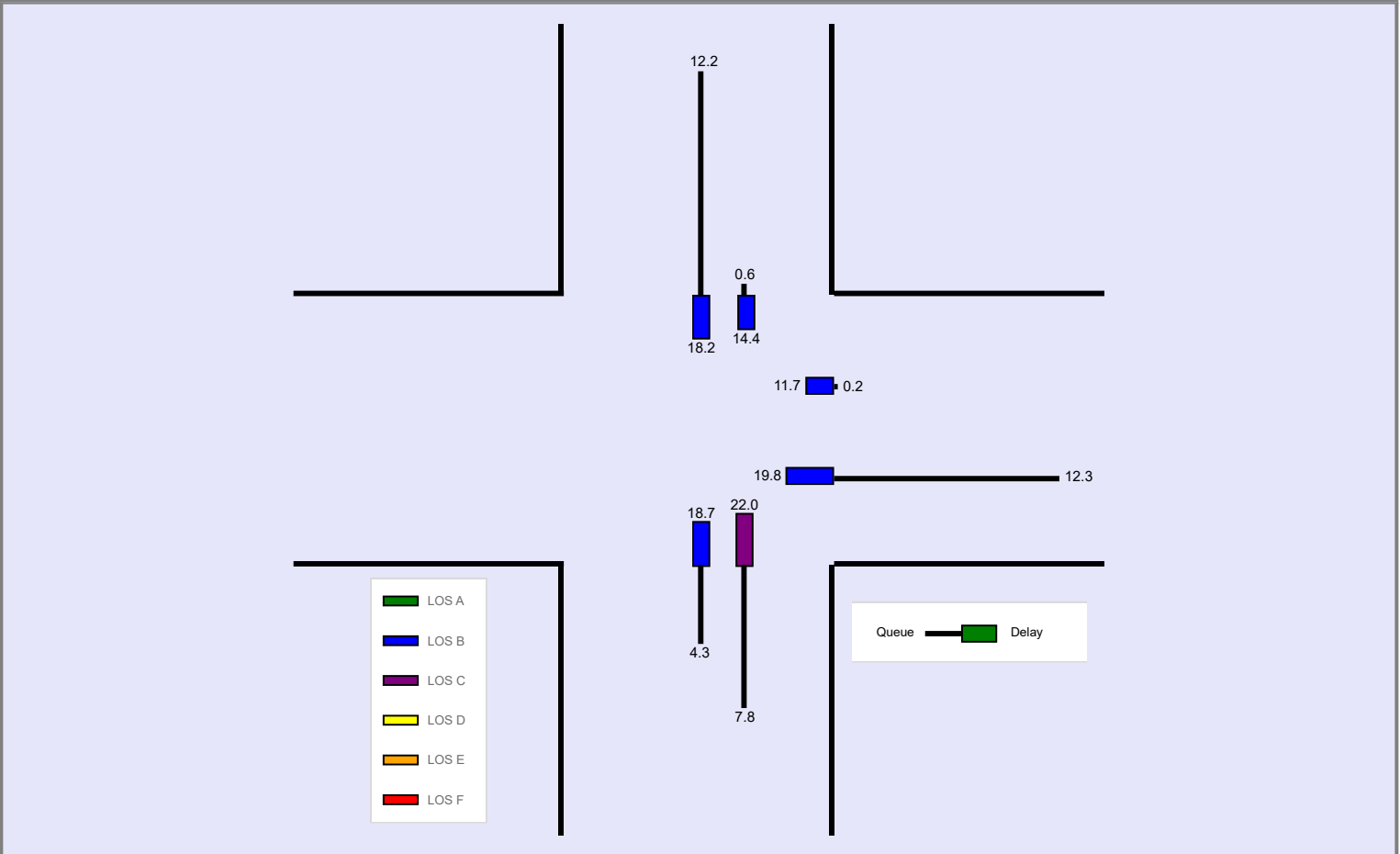
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Agency	MEP			Duration, h	0.250		
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other
Jurisdiction				Time Period			
Urban Street	Green River Parkway		Analysis Year	2030		Analysis Period	1 > 7:00
Intersection	NE Dixie Hwy		File Name	Green River and Dixie AM.xus			
Project Description	AM Pre-Dev						



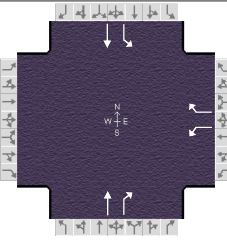
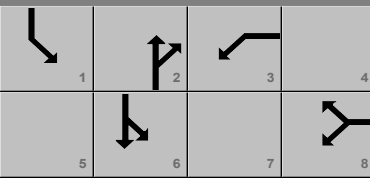
Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				599		11		210	332	37	628	

Signal Information													
Cycle, s	62.8	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.0	17.7	24.6	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)				311.9		4		110	198.7	15.4	310.7	
Back of Queue (Q), veh/ln (95 th percentile)				12.3		0.2		4.3	7.8	0.6	12.2	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00	0.00	
Control Delay (d), s/veh				19.8		11.7		18.7	22.0	14.4	18.2	
Level of Service (LOS)				B		B		B	C	B	B	
Approach Delay, s/veh / LOS	0.0			19.7		B		20.7	C	18.0		B
Intersection Delay, s/veh / LOS	19.4						B					

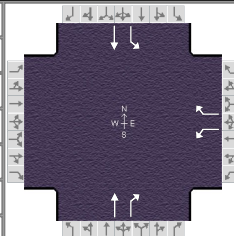


HCS7 Signalized Intersection Input Data

General Information						Intersection Information												
Agency	MEP					Duration, h	0.250											
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other											
Jurisdiction						Time Period												
Urban Street	Green River Parkway		Analysis Year	2030		PHF	0.95											
Intersection	NE Dixie Hwy		File Name	Green River and Dixie AM.xus														
Project Description	AM Post																	
Demand Information						EB			WB			NB			SB			
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h									622		11		215	337	39	628		
Signal Information																		
Cycle, s	65.8	Reference Phase	2															
Offset, s	0	Reference Point	End															
Uncoordinated	Yes	Simult. Gap E/W	On			Green	2.1	18.6	26.6	0.0	0.0	0.0						
Force Mode	Fixed	Simult. Gap N/S	On			Yellow	4.0	4.0	4.0	0.0	0.0	0.0						
						Red	2.0	2.2	2.3	0.0	0.0	0.0						
Traffic Information						EB			WB			NB			SB			
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h									622		11		215	337	39	628		
Initial Queue (Q _b), veh/h									0		0		0	0	0	0		
Base Saturation Flow Rate (s ₀), veh/h									1900		1900		1900	1900	1900	1900		
Parking (N _m), man/h										None			None			None		
Heavy Vehicles (P _{HV}), %									2		2		2	2	2	2		
Ped / Bike / RTOR, /h						0	0		0	0		0	0	0	0	0		
Buses (N _b), buses/h									0	0	0	0	0	0	0	0	0	
Arrival Type (AT)									3		3		3	3	3	3		
Upstream Filtering (I)									1.00		1.00		1.00	1.00	1.00	1.00		
Lane Width (W), ft									12.0		12.0		12.0	12.0	12.0	12.0		
Turn Bay Length, ft									0		0		0	0	0	0		
Grade (P _g), %							0			0			0			0		
Speed Limit, mi/h									35		35		35	35	35	35		
Phase Information						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT					
Maximum Green (G _{max}) or Phase Split, s								45.0	45.0		55.0	15.0	55.0					
Yellow Change Interval (Y), s								4.0	4.0		4.0	4.0	4.0					
Red Clearance Interval (R _c), s								2.3	2.3		2.2	2.0	2.2					
Minimum Green (G _{min}), s								7	7		15	4	15					
Start-Up Lost Time (l _t), s								2.0			2.0	2.0	2.0					
Extension of Effective Green (e), s								2.0			2.0	2.0	2.0					
Passage (PT), s								2.0	2.0		2.0	2.0	2.0					
Recall Mode								Off	Off		Min	Off	Min					
Dual Entry								No	Yes		Yes	No	Yes					
Walk (Walk), s							0.0		0.0		0.0							
Pedestrian Clearance Time (PC), s							0.0		0.0		0.0							
Multimodal Information						EB			WB			NB			SB			
85th % Speed / Rest in Walk / Corner Radius						0	No	25	0	No	25	0	No	25				
Walkway / Crosswalk Width / Length, ft						9.0	12	0	9.0	12	0	9.0	12	0				
Street Width / Island / Curb							0		0	0	No	0	0	No	0		No	
Width Outside / Bike Lane / Shoulder, ft									12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	
Pedestrian Signal / Occupied Parking						No			No	0.50		No	0.50			0.50		

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP			Analysis Date	9/2/2023		
Jurisdiction				Area Type	Other		
Urban Street	Green River Parkway			PHF	0.95		
Intersection	NE Dixie Hwy			Analysis Year	2030		
Project Description	AM Post			Analysis Period	1 > 7:00		
File Name	Green River and Dixie AM.xus						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				622		11		215	337	39	628	

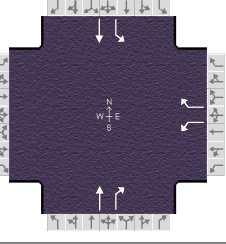
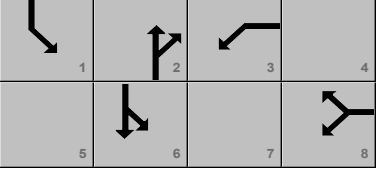
Signal Information													
Cycle, s	65.8	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.1	18.6	26.6	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				8		2	1	6
Case Number				9.0		7.3	1.0	4.0
Phase Duration, s				32.9		24.8	8.1	32.9
Change Period, (Y+R _c), s				6.3		6.2	6.0	6.2
Max Allow Headway (MAH), s				3.1		3.1	3.1	3.1
Queue Clearance Time (g _s), s				24.9		15.7	3.0	23.5
Green Extension Time (g _e), s				1.4		2.7	0.0	2.7
Phase Call Probability				1.00		1.00	0.53	1.00
Max Out Probability				0.00		0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3		18		2	12	1		6
Adjusted Flow Rate (v), veh/h				655		12		226	355	41		661
Adjusted Saturation Flow Rate (s), veh/h/ln				1781		1585		1870	1585	1781		1870
Queue Service Time (g _s), s				22.9		0.3		6.5	13.7	1.0		21.5
Cycle Queue Clearance Time (g _c), s				22.9		0.3		6.5	13.7	1.0		21.5
Green Ratio (g/C)				0.40		0.40		0.28	0.28	0.35		0.41
Capacity (c), veh/h				721		642		530	449	384		760
Volume-to-Capacity Ratio (X)				0.908		0.018		0.427	0.790	0.107		0.870
Back of Queue (Q), ft/ln (95 th percentile)				348.3		4.2		120.4	211.5	17.3		329.8
Back of Queue (Q), veh/ln (95 th percentile)				13.7		0.2		4.7	8.3	0.7		13.0
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00		0.00
Uniform Delay (d ₁), s/veh				18.6		11.8		19.4	21.9	15.0		18.1
Incremental Delay (d ₂), s/veh				3.5		0.0		0.2	1.2	0.0		1.2
Initial Queue Delay (d ₃), s/veh				0.0		0.0		0.0	0.0	0.0		0.0
Control Delay (d), s/veh				22.1		11.8		19.6	23.1	15.1		19.3
Level of Service (LOS)				C		B		B	C	B		B
Approach Delay, s/veh / LOS	0.0			21.9		C	21.7		C	19.1		B
Intersection Delay, s/veh / LOS				20.8					C			

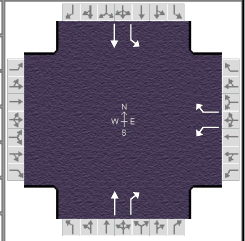
Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information						Intersection Information									
Agency	MEP					Duration, h	0.250								
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other								
Jurisdiction						Time Period									
Urban Street	Green River Parkway		Analysis Year	2030		PHF	0.95								
Intersection	NE Dixie Hwy		File Name	Green River and Dixie AM.xus											
Project Description	AM Post														
Demand Information			EB			WB			NB			SB			
Approach Movement			L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h						622		11		215	337	39	628		
Signal Information															
Cycle, s	65.8	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.1	18.6	26.6	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0					
				Red	2.0	2.2	2.3	0.0	0.0	0.0					
Saturation Flow / Delay				L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f _w)							1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f _{HVg})							0.984	1.000	0.984	1.000	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f _p)				0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f _{bb})				0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f _a)							1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f _{LU})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f _{LT})							0.952	0.000		1.000	1.000		0.952	0.000	
Right-Turn Adjustment Factor (f _{RT})								0.000	0.847		0.000	0.847		1.000	1.000
Left-Turn Pedestrian Adjustment Factor (f _{Lpb})							1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f _{Rpb})									1.000			1.000			1.000
Work Zone Adjustment Factor (f _{wz})							1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f _{DDI})							1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h							1781	0	1585	0	1870	1585	1781	1870	0
Proportion of Vehicles Arriving on Green (P)				0.00	0.00	0.00	0.40	0.00	0.40	0.00	0.28	0.28	0.03	0.41	0.00
Incremental Delay Factor (k)							0.08		0.04		0.04	0.04	0.04	0.04	
Signal Timing / Movement Groups				EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R				
Lost Time (t _L)							6.3		6.2	6.0	6.2				
Green Ratio (g/C)							0.40		0.28	0.35	0.41				
Permitted Saturation Flow Rate (s _p), veh/h/ln							1781		786	1154	0				
Shared Saturation Flow Rate (s _{sh}), veh/h/ln									0						
Permitted Effective Green Time (g _p), s							0.0		0.0	20.8	0.0				
Permitted Service Time (g _u), s							0.0		0.0	12.5	0.0				
Permitted Queue Service Time (g _{ps}), s										0.3					
Time to First Blockage (g _t), s							0.0		18.8	0.0	0.0				
Queue Service Time Before Blockage (g _{ts}), s															
Protected Right Saturation Flow (s _R), veh/h/ln							0		0						
Protected Right Effective Green Time (g _R), s							0.0		0.0						
Multimodal				EB	WB	NB	SB								
Pedestrian F _w / F _v															
Pedestrian F _s / F _{delay}															
Pedestrian M _{corner} / M _{cw}															
Bicycle c _b / d _b															
Bicycle F _w / F _v															

HCS7 Signalized Intersection Results Graphical Summary

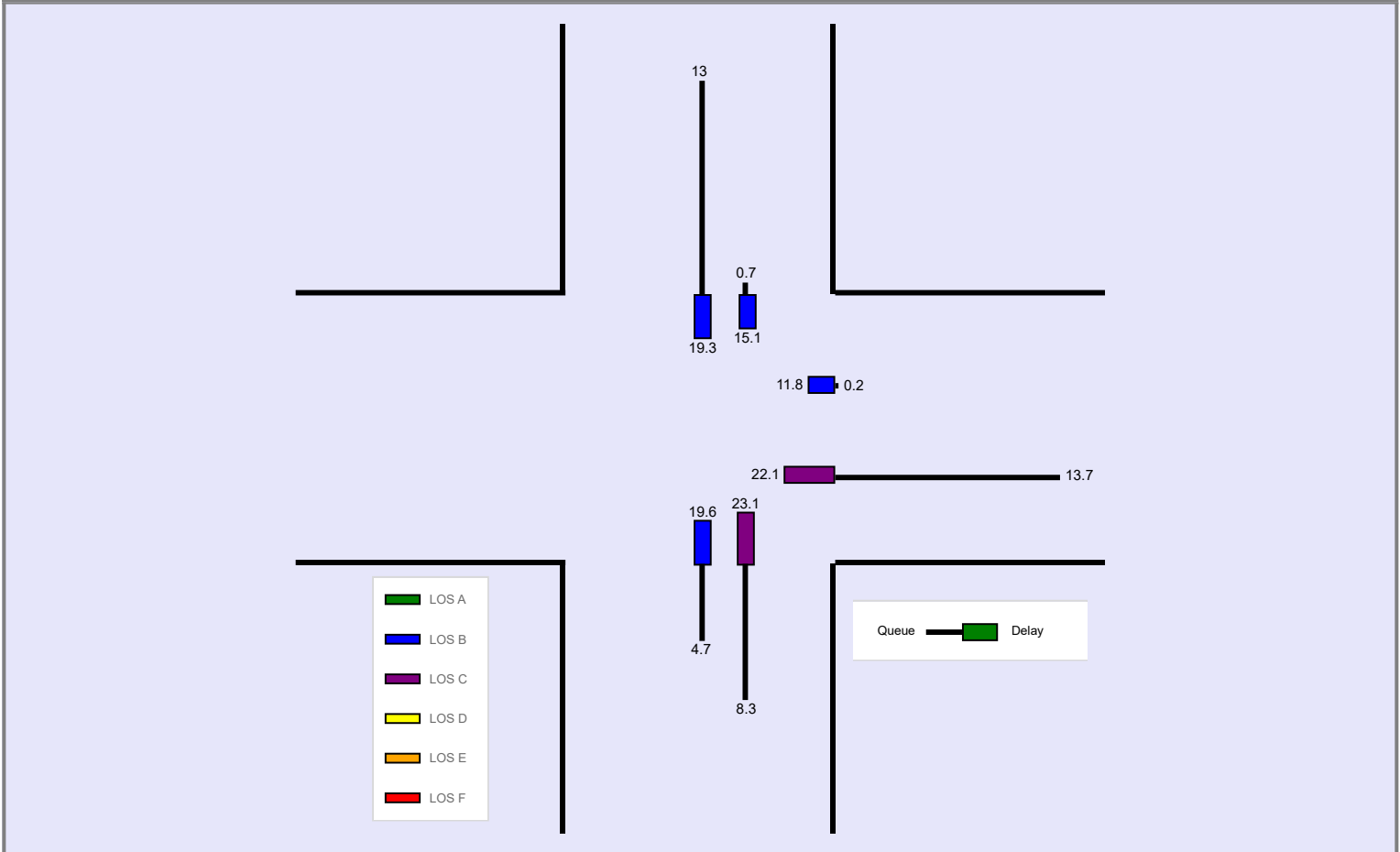
General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other
Jurisdiction				Time Period			
Urban Street	Green River Parkway		Analysis Year	2030		Analysis Period	1 > 7:00
Intersection	NE Dixie Hwy		File Name	Green River and Dixie AM.xus			
Project Description	AM Post						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				622		11		215	337	39	628	

Signal Information														
Cycle, s	65.8	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.1	18.6	26.6	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0				
				Red	2.0	2.2	2.3	0.0	0.0	0.0				

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)				348.3		4.2		120.4	211.5	17.3	329.8	
Back of Queue (Q), veh/ln (95 th percentile)				13.7		0.2		4.7	8.3	0.7	13.0	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00	0.00	
Control Delay (d), s/veh				22.1		11.8		19.6	23.1	15.1	19.3	
Level of Service (LOS)				C		B		B	C	B	B	
Approach Delay, s/veh / LOS	0.0			21.9		C		21.7	C	19.1		B
Intersection Delay, s/veh / LOS	20.8						C					



Avonlea 10-11-13-14-15
 PM PEAK HOUR TURNING MOVEMENTS
 EXHIBIT 10
 Dixie Highway & Green River Parkway

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
4:00 PM	4:15 PM	0	0	0	69	1	10	0	108	91	3	43	0
4:15 PM	4:30 PM	0	0	0	66	0	7	0	113	91	4	49	0
4:30 PM	4:45 PM	0	0	0	57	0	7	0	111	72	2	60	0
4:45 PM	5:00 PM	0	0	0	70	0	14	0	115	96	6	59	0
5:00 PM	5:15 PM	0	0	0	83	0	13	0	131	78	7	72	0
5:15 PM	5:30 PM	0	0	0	61	0	2	0	150	131	5	50	0
5:30 PM	5:45 PM	0	0	0	59	0	2	0	95	84	5	55	0
5:45 PM	6:00 PM	0	0	0	73	0	4	0	74	75	3	54	0
6:00 PM	6:15 PM												
6:15 PM	6:30 PM												
6:30 PM	6:45 PM												
6:45 PM	7:00 PM												
		0	0	0	538	1	59	0	897	718	35	442	0



Peak Hour Traffic Volume

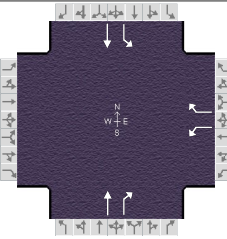
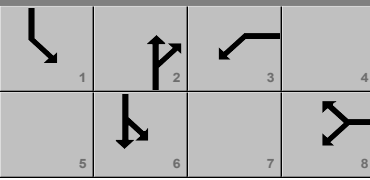
4:30 PM	5:30 PM	0	0	0	271	0	36	0	507	377	20	241	0
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Count Taken: 6/29/2023 PHF 0.91
 Buildout year: 2030
 Growth Rate: 1.3%
 PSCF 1.22

	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/28/2023	0	0	0	271	0	36	0	507	377	20	241	0
Peak Season Factor	0	0	0	60	0	8	0	112	83	4	53	0
Adjusted Volumes	0	0	0	331	0	44	0	619	460	24	294	0
Growth 1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
2030 Volumes	0	0	0	367	0	49	0	686	510	27	326	0
Pre Dev	0	0	0	367	0	49	0	686	510	27	326	0
Project Traffic	0	0	0	22	0	0	0	13	15	12	0	0
Post Dev	0	0	0	389	0	49	0	699	525	39	326	0

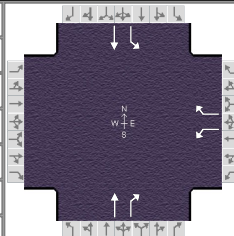
Project Traffic Assignment	In	In	Out	Out	Out	In	In	Out	Out	Out		
Existing	0%	4%	0%	7%	4%	10%	0%	0%	7%	10%	0%	0%
Pre-Development	0	0	0	331	0	44	0	619	460	24	294	0
Post-Development	0	0	0	367	0	49	0	686	510	27	326	0
	0	0	0	389	0	49	0	699	525	39	326	0

HCS7 Signalized Intersection Input Data

General Information						Intersection Information											
Agency						Duration, h		0.250									
Analyst		MEP		Analysis Date		9/2/2023		Area Type		Other							
Jurisdiction						Time Period											
Urban Street		Green River Parkway		Analysis Year		2024		PHF		0.92							
Intersection		NE Dixie Hwy and Gree...		File Name		Green River and Dixie PM.xus											
Project Description		PM Existing															
Demand Information						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h									331		44		619	460	24	294	
Signal Information																	
Cycle, s		58.9		Reference Phase		2											
Offset, s		0		Reference Point		End											
Uncoordinated		Yes		Simult. Gap E/W		On		Green	1.4	24.8	14.2	0.0	0.0	0.0			
Force Mode		Fixed		Simult. Gap N/S		On		Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
								Red	2.0	2.2	2.3	0.0	0.0	0.0			
Traffic Information						EB			WB			NB			SB		
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h									331		44		619	460	24	294	
Initial Queue (Q _b), veh/h									0		0		0	0	0	0	
Base Saturation Flow Rate (s ₀), veh/h									1900		1900		1900	1900	1900	1900	
Parking (N _m), man/h										None			None			None	
Heavy Vehicles (P _{HV}), %									2		2		2	2	2	2	
Ped / Bike / RTOR, /h						0	0		0	0		0	0	0	0	0	
Buses (N _b), buses/h									0	0	0	0	0	0	0	0	0
Arrival Type (AT)									3		3		3	3	3	3	
Upstream Filtering (I)									1.00		1.00		1.00	1.00	1.00	1.00	
Lane Width (W), ft									12.0		12.0		12.0	12.0	12.0	12.0	
Turn Bay Length, ft									0		0		0	0	0	0	
Grade (P _g), %							0			0			0			0	
Speed Limit, mi/h									35		35		35	35	35	35	
Phase Information						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Maximum Green (G _{max}) or Phase Split, s								45.0	45.0		55.0	15.0	55.0				
Yellow Change Interval (Y), s								4.0	4.0		4.0	4.0	4.0				
Red Clearance Interval (R _c), s								2.3	2.3		2.2	2.0	2.2				
Minimum Green (G _{min}), s								7	7		15	4	15				
Start-Up Lost Time (l _t), s								2.0			2.0	2.0	2.0				
Extension of Effective Green (e), s								2.0			2.0	2.0	2.0				
Passage (PT), s								2.0	2.0		2.0	2.0	2.0				
Recall Mode								Off	Off		Min	Off	Min				
Dual Entry								No	Yes		Yes	No	Yes				
Walk (Walk), s							0.0		0.0		0.0		0.0				
Pedestrian Clearance Time (PC), s							0.0		0.0		0.0		0.0				
Multimodal Information						EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius						0	No	25	0	No	25	0	No	25			
Walkway / Crosswalk Width / Length, ft						9.0	12	0	9.0	12	0	9.0	12	0			
Street Width / Island / Curb							0		0	0	No	0	0	No	0		No
Width Outside / Bike Lane / Shoulder, ft									12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking						No			No	0.50		No	0.50				0.50

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency				Duration, h	0.250		
Analyst	MEP	Analysis Date	9/2/2023	Area Type	Other		
Jurisdiction				PHF	0.92		
Urban Street	Green River Parkway	Analysis Year	2024	Analysis Period	1 > 16:00		
Intersection	NE Dixie Hwy and Gree...	File Name	Green River and Dixie PM.xus				
Project Description	PM Existing						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				331		44		619	460	24	294	

Signal Information													
Cycle, s	58.9	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.4	24.8	14.2	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

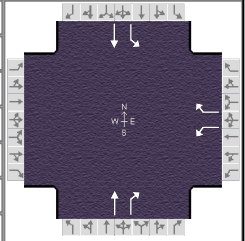
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				8		2	1	6
Case Number				9.0		7.3	1.0	4.0
Phase Duration, s				20.5		31.0	7.4	38.4
Change Period, (Y+R _c), s				6.3		6.2	6.0	6.2
Max Allow Headway (MAH), s				3.1		3.1	3.1	3.1
Queue Clearance Time (g _s), s				13.3		21.2	2.5	7.5
Green Extension Time (g _e), s				0.8		3.5	0.0	3.5
Phase Call Probability				1.00		1.00	0.35	1.00
Max Out Probability				0.00		0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3		18	2	12		1	6	
Adjusted Flow Rate (v), veh/h				360		48	673	500		26	320	
Adjusted Saturation Flow Rate (s), veh/h/ln				1781		1585	1870	1585		1781	1870	
Queue Service Time (g _s), s				11.3		1.4	19.2	15.8		0.5	5.5	
Cycle Queue Clearance Time (g _c), s				11.3		1.4	19.2	15.8		0.5	5.5	
Green Ratio (g/C)				0.24		0.24	0.42	0.42		0.48	0.55	
Capacity (c), veh/h				430		383	788	668		239	1023	
Volume-to-Capacity Ratio (X)				0.836		0.125	0.853	0.748		0.109	0.312	
Back of Queue (Q), ft/ln (95 th percentile)				199.8		21.3	286	212.1		6.7	77.1	
Back of Queue (Q), veh/ln (95 th percentile)				7.9		0.8	11.3	8.4		0.3	3.0	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00	0.00	0.00		0.00	0.00	
Uniform Delay (d ₁), s/veh				21.3		17.5	15.4	14.4		12.1	7.3	
Incremental Delay (d ₂), s/veh				1.7		0.1	1.0	0.6		0.1	0.1	
Initial Queue Delay (d ₃), s/veh				0.0		0.0	0.0	0.0		0.0	0.0	
Control Delay (d), s/veh				23.0		17.6	16.5	15.1		12.1	7.4	
Level of Service (LOS)				C		B	B	B		B	A	
Approach Delay, s/veh / LOS	0.0			22.3		C	15.9	B		7.7	A	
Intersection Delay, s/veh / LOS				15.8				B				

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency				Duration, h	0.250
Analyst	MEP	Analysis Date	9/2/2023	Area Type	Other
Jurisdiction				PHF	0.92
Urban Street	Green River Parkway	Analysis Year	2024	Analysis Period	1 > 16:00
Intersection	NE Dixie Hwy and Gree...	File Name	Green River and Dixie PM.xus		
Project Description	PM Existing				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				331		44		619	460	24	294	

Signal Information													
Cycle, s	58.9	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.4	24.8	14.2	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

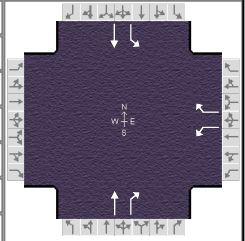
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	1.000	0.984	1.000	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		1.000	1.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.000	0.847		0.000	0.847		1.000	1.000
Left-Turn Pedestrian Adjustment Factor (f_{LPB})				1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{Rpb})						1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1781	0	1585	0	1870	1585	1781	1870	0
Proportion of Vehicles Arriving on Green (P)	0.00	0.00	0.00	0.24	0.00	0.24	0.00	0.42	0.42	0.02	0.55	0.00
Incremental Delay Factor (k)				0.04		0.04		0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)				6.3		6.2	6.0	6.2
Green Ratio (g/C)				0.24		0.42	0.48	0.55
Permitted Saturation Flow Rate (s_p), veh/h/ln				1781		1077	765	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln						0		
Permitted Effective Green Time (g_p), s				0.0		0.0	26.9	0.0
Permitted Service Time (g_u), s				0.0		0.0	5.7	0.0
Permitted Queue Service Time (g_{ps}), s							0.7	
Time to First Blockage (g_t), s				0.0		24.9	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

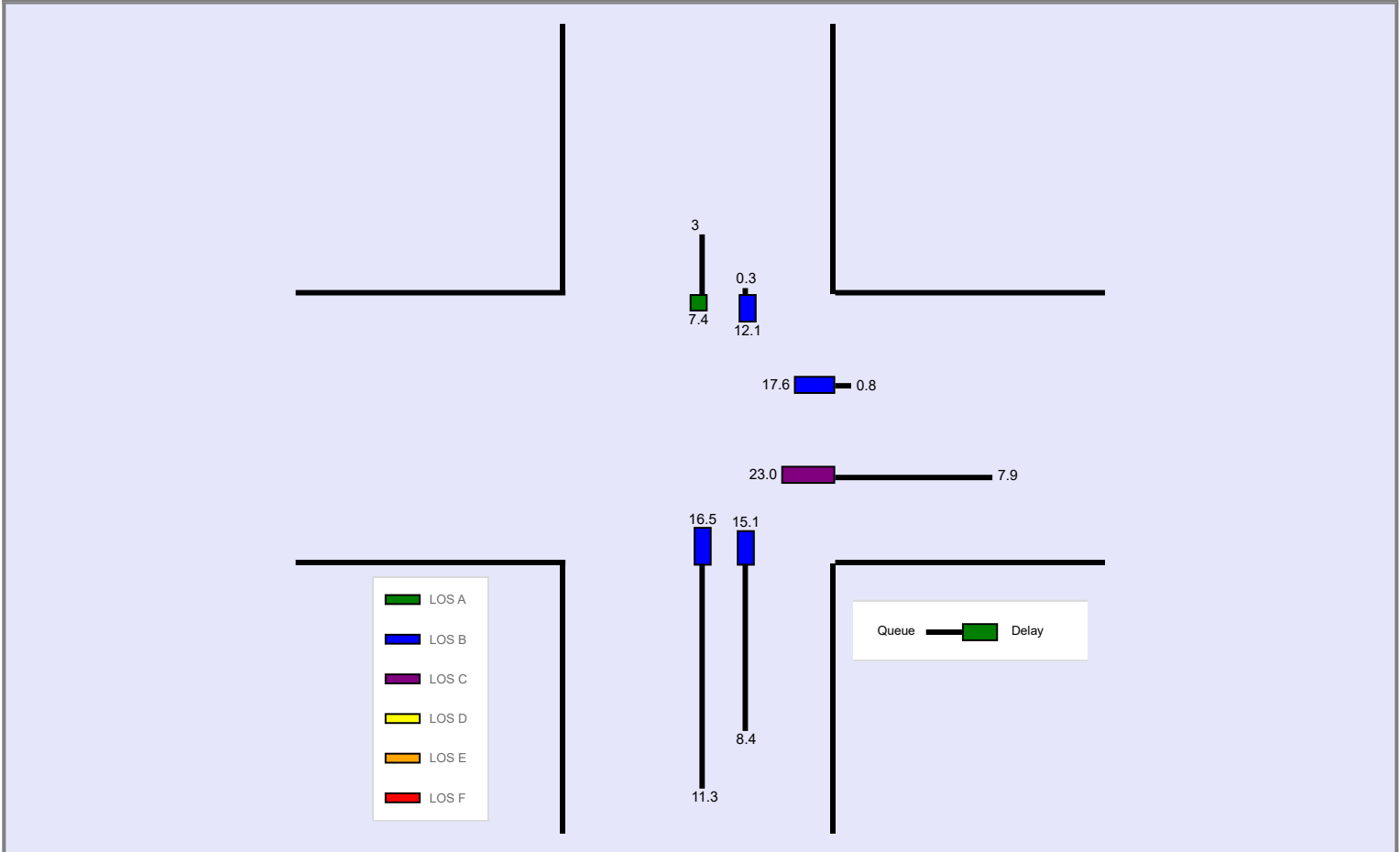
General Information				Intersection Information			
Agency				Duration, h	0.250		
Analyst	MEP	Analysis Date	9/2/2023	Area Type	Other		
Jurisdiction				PHF	0.92		
Urban Street	Green River Parkway	Analysis Year	2024	Analysis Period	1 > 16:00		
Intersection	NE Dixie Hwy and Gree...	File Name	Green River and Dixie PM.xus				
Project Description	PM Existing						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				331		44		619	460	24	294	

Signal Information													
Cycle, s	58.9	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.4	24.8	14.2	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)				199.8		21.3		286	212.1	6.7	77.1	
Back of Queue (Q), veh/ln (95 th percentile)				7.9		0.8		11.3	8.4	0.3	3.0	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00	0.00	
Control Delay (d), s/veh				23.0		17.6		16.5	15.1	12.1	7.4	
Level of Service (LOS)				C		B		B	B	B	A	
Approach Delay, s/veh / LOS	0.0			22.3		C		15.9	B	7.7	A	
Intersection Delay, s/veh / LOS	15.8						B					

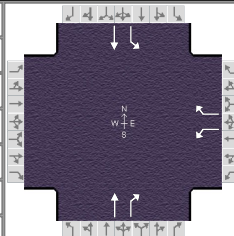


HCS7 Signalized Intersection Input Data

General Information						Intersection Information															
Agency						Duration, h		0.250													
Analyst		MEP		Analysis Date		9/2/2023		Area Type		Other											
Jurisdiction						Time Period		PHF													
Urban Street		Green River Parkway		Analysis Year		2030		Analysis Period		1 > 16:00											
Intersection		NE Dixie Hwy		File Name		Green River and Dixie PM.xus															
Project Description		PM Pre-Dev																			
Demand Information												EB		WB		NB		SB			
Approach Movement												L	T	R	L	T	R	L	T	R	
Demand (v), veh/h															367		49	686	510	27	326
Signal Information																					
Cycle, s		74.9		Reference Phase		2															
Offset, s		0		Reference Point		End															
Uncoordinated		Yes		Simult. Gap E/W		On		Green		1.8						33.6		20.9		0.0	
Force Mode		Fixed		Simult. Gap N/S		On		Yellow		4.0		4.0		4.0		0.0		0.0		0.0	
				Red		2.0		2.2		2.3		0.0		0.0		0.0					
Traffic Information						EB		WB		NB		SB									
Approach Movement						L	T	R	L	T	R	L	T	R							
Demand (v), veh/h									367		49	686	510	27	326						
Initial Queue (Q _b), veh/h									0		0	0	0	0	0						
Base Saturation Flow Rate (s ₀), veh/h									1900		1900	1900	1900	1900	1900						
Parking (N _m), man/h									0	L + R	0	None			None						
Heavy Vehicles (P _{HV}), %									2		2	2	2	2	2						
Ped / Bike / RTOR, /h						0	0		0	0		0	0	0	0	0					
Buses (N _b), buses/h									0	0	0	0	0	0	0	0	0				
Arrival Type (AT)									3		3	3	3	3	3						
Upstream Filtering (I)									1.00		1.00	1.00	1.00	1.00	1.00						
Lane Width (W), ft									12.0		12.0	12.0	12.0	12.0	12.0						
Turn Bay Length, ft									0		0	0	0	0	0						
Grade (P _g), %							0			0		0			0						
Speed Limit, mi/h									35		35	35	35	35	35						
Phase Information						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT								
Maximum Green (G _{max}) or Phase Split, s								45.0	45.0		55.0	15.0	55.0								
Yellow Change Interval (Y), s								4.0	4.0		4.0	4.0	4.0								
Red Clearance Interval (R _c), s								2.3	2.3		2.2	2.0	2.2								
Minimum Green (G _{min}), s								7	7		15	4	15								
Start-Up Lost Time (l _t), s								2.0			2.0	2.0	2.0								
Extension of Effective Green (e), s								2.0			2.0	2.0	2.0								
Passage (PT), s								2.0	2.0		2.0	2.0	2.0								
Recall Mode								Off	Off		Min	Off	Min								
Dual Entry								No	Yes		Yes	No	Yes								
Walk (Walk), s							0.0		0.0		0.0										
Pedestrian Clearance Time (PC), s							0.0		0.0		0.0										
Multimodal Information						EB		WB		NB		SB									
85th % Speed / Rest in Walk / Corner Radius						0	No	25	0	No	25	0	No	25							
Walkway / Crosswalk Width / Length, ft						9.0	12	0	9.0	12	0	9.0	12	0							
Street Width / Island / Curb							0		0	0	No	0	0	No							
Width Outside / Bike Lane / Shoulder, ft									12	5.0	2.0	12	5.0	2.0							
Pedestrian Signal / Occupied Parking						No			No	0.50		No	0.50								

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency				Duration, h	0.250		
Analyst	MEP	Analysis Date	9/2/2023	Area Type	Other		
Jurisdiction				PHF	0.92		
Urban Street	Green River Parkway	Analysis Year	2030	Analysis Period	1 > 16:00		
Intersection	NE Dixie Hwy	File Name	Green River and Dixie PM.xus				
Project Description	PM Pre-Dev						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				367		49		686	510	27	326	

Signal Information													
Cycle, s	74.9	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.8	33.6	20.9	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

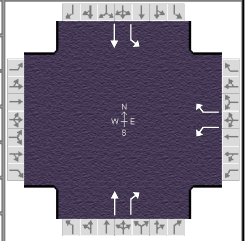
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				8		2	1	6
Case Number				9.0		7.3	1.0	4.0
Phase Duration, s				27.2		39.8	7.8	47.7
Change Period, (Y+R _c), s				6.3		6.2	6.0	6.2
Max Allow Headway (MAH), s				3.1		3.1	3.1	3.1
Queue Clearance Time (g _s), s				19.9		29.5	2.6	9.8
Green Extension Time (g _e), s				0.9		4.0	0.0	4.1
Phase Call Probability				1.00		1.00	0.46	1.00
Max Out Probability				0.00		0.02	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3		18	2	12		1	6	
Adjusted Flow Rate (v), veh/h				399		53	746	554		29	354	
Adjusted Saturation Flow Rate (s), veh/h/ln				1603		1427	1870	1585		1781	1870	
Queue Service Time (g _s), s				17.9		2.1	27.5	22.3		0.6	7.8	
Cycle Queue Clearance Time (g _c), s				17.9		2.1	27.5	22.3		0.6	7.8	
Green Ratio (g/C)				0.28		0.28	0.45	0.45		0.50	0.55	
Capacity (c), veh/h				449		400	840	712		200	1036	
Volume-to-Capacity Ratio (X)				0.889		0.133	0.887	0.778		0.147	0.342	
Back of Queue (Q), ft/ln (95 th percentile)				276.1		30.4	418.5	297.6		10.3	125.2	
Back of Queue (Q), veh/ln (95 th percentile)				10.9		1.2	16.5	11.7		0.4	4.9	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00	0.00	0.00		0.00	0.00	
Uniform Delay (d ₁), s/veh				26.0		20.3	19.0	17.5		15.6	9.2	
Incremental Delay (d ₂), s/veh				2.4		0.1	2.6	0.7		0.1	0.1	
Initial Queue Delay (d ₃), s/veh				0.0		0.0	0.0	0.0		0.0	0.0	
Control Delay (d), s/veh				28.4		20.3	21.6	18.2		15.7	9.3	
Level of Service (LOS)				C		C	C	B		B	A	
Approach Delay, s/veh / LOS	0.0			27.4		C	20.2	C		9.8	A	
Intersection Delay, s/veh / LOS				19.8				B				

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information	
Agency				Duration, h	0.250
Analyst	MEP	Analysis Date	9/2/2023	Area Type	Other
Jurisdiction				PHF	0.92
Urban Street	Green River Parkway	Analysis Year	2030	Analysis Period	1 > 16:00
Intersection	NE Dixie Hwy	File Name	Green River and Dixie PM.xus		
Project Description	PM Pre-Dev				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				367		49		686	510	27	326	

Signal Information													
Cycle, s	74.9	Reference Phase	2										
Offset, s	0	Reference Point	End	Green	1.8	33.6	20.9	0.0	0.0	0.0			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.0	2.2	2.3	0.0	0.0	0.0			

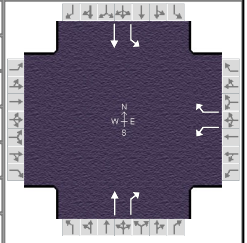
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	1.000	0.984	1.000	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	0.000	0.000	0.000	0.900	1.000	0.900	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		1.000	1.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.000	0.847		0.000	0.847		1.000	1.000
Left-Turn Pedestrian Adjustment Factor (f_{LPB})				1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})						1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1603	0	1427	0	1870	1585	1781	1870	0
Proportion of Vehicles Arriving on Green (P)	0.00	0.00	0.00	0.28	0.00	0.28	0.00	0.45	0.45	0.02	0.55	0.00
Incremental Delay Factor (k)				0.04		0.04		0.08	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)				6.3		6.2	6.0	6.2
Green Ratio (g/C)				0.28		0.45	0.50	0.55
Permitted Saturation Flow Rate (s_p), veh/h/ln				1603		1043	715	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln						0		
Permitted Effective Green Time (g_p), s				0.0		0.0	35.8	0.0
Permitted Service Time (g_u), s				0.0		0.0	6.3	0.0
Permitted Queue Service Time (g_{ps}), s							1.3	
Time to First Blockage (g_t), s				0.0		33.8	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

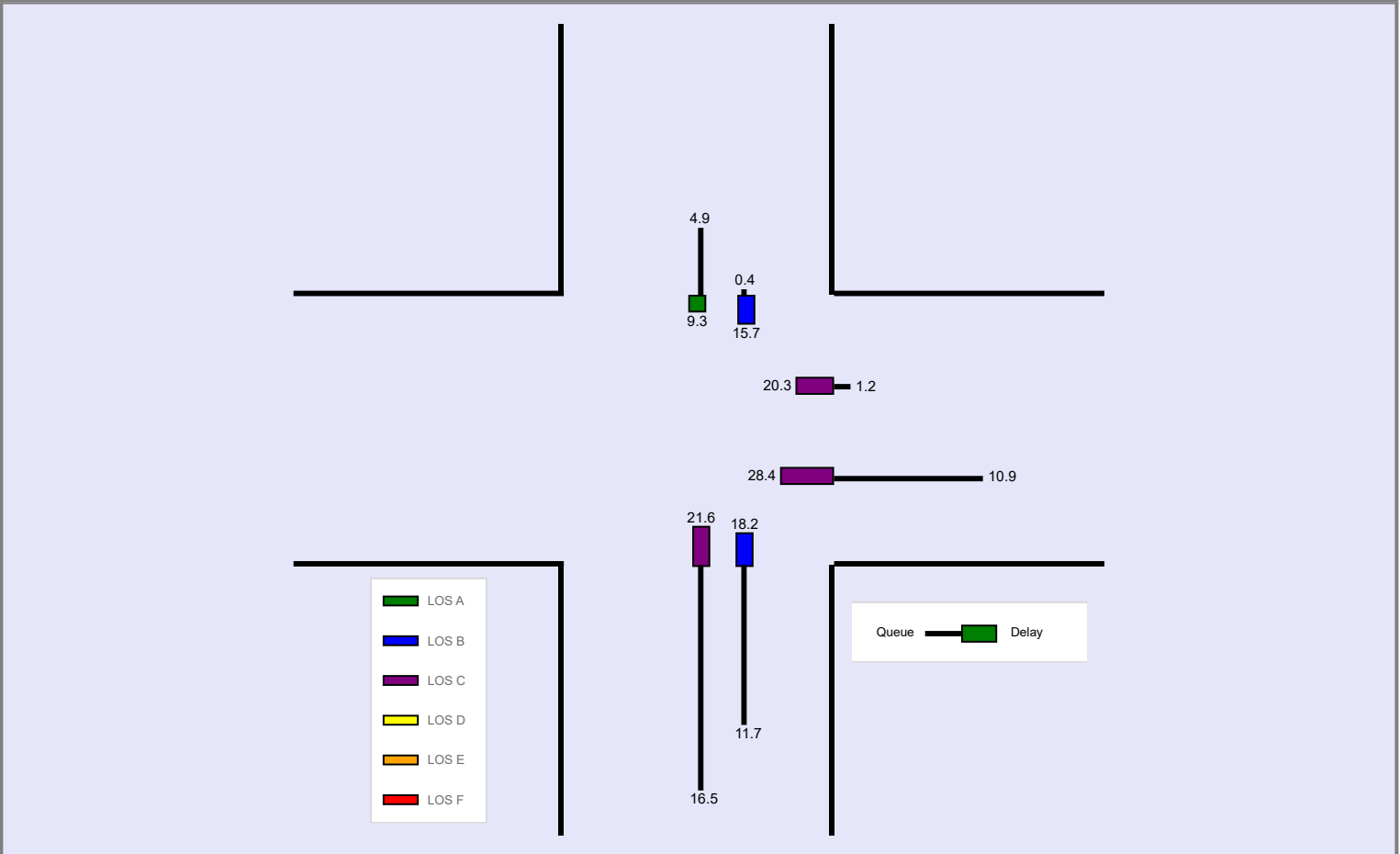
General Information				Intersection Information			
Agency				Duration, h	0.250		
Analyst	MEP	Analysis Date	9/2/2023	Area Type	Other		
Jurisdiction				PHF	0.92		
Urban Street	Green River Parkway	Analysis Year	2030	Analysis Period	1 > 16:00		
Intersection	NE Dixie Hwy	File Name	Green River and Dixie PM.xus				
Project Description	PM Pre-Dev						



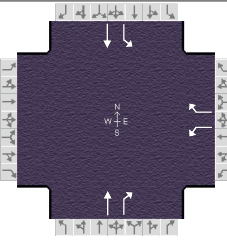
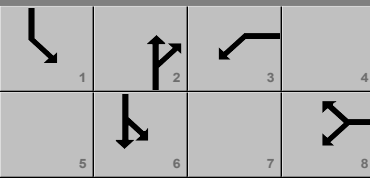
Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				367		49		686	510	27	326	

Signal Information													
Cycle, s	74.9	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	1.8	33.6	20.9	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

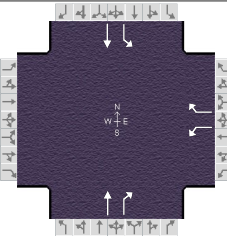
Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)				276.1		30.4		418.5	297.6	10.3	125.2	
Back of Queue (Q), veh/ln (95 th percentile)				10.9		1.2		16.5	11.7	0.4	4.9	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00	0.00	
Control Delay (d), s/veh				28.4		20.3		21.6	18.2	15.7	9.3	
Level of Service (LOS)				C		C		C	B	B	A	
Approach Delay, s/veh / LOS	0.0			27.4		C	20.2		C	9.8		A
Intersection Delay, s/veh / LOS	19.8						B					



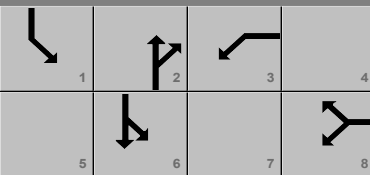
HCS7 Signalized Intersection Input Data

General Information						Intersection Information												
Agency	MEP					Duration, h	0.250											
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other											
Jurisdiction						Time Period												
Urban Street	Green River Parkway		Analysis Year	2030		PHF	0.92											
Intersection	NE Dixie Hwy		File Name	Green River and Dixie PM.xus														
Project Description	PM Post																	
Demand Information						EB			WB			NB			SB			
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h									389		49	699	525	39	326			
Signal Information																		
Cycle, s	75.8	Reference Phase	2															
Offset, s	0	Reference Point	End															
Uncoordinated	Yes	Simult. Gap E/W	On			Green	2.4	34.6	20.4	0.0	0.0	0.0						
Force Mode	Fixed	Simult. Gap N/S	On			Yellow	4.0	4.0	4.0	0.0	0.0	0.0						
						Red	2.0	2.2	2.3	0.0	0.0	0.0						
Traffic Information						EB			WB			NB			SB			
Approach Movement						L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h									389		49	699	525	39	326			
Initial Queue (Q _b), veh/h									0		0	0	0	0	0			
Base Saturation Flow Rate (s ₀), veh/h									1900		1900	1900	1900	1900	1900			
Parking (N _m), man/h										None			None			None		
Heavy Vehicles (P _{HV}), %									2		2	2	2	2	2			
Ped / Bike / RTOR, /h						0	0		0	0		0	0	0	0	0		
Buses (N _b), buses/h									0	0	0	0	0	0	0	0	0	
Arrival Type (AT)									3		3	3	3	3	3			
Upstream Filtering (I)									1.00		1.00	1.00	1.00	1.00	1.00			
Lane Width (W), ft									12.0		12.0	12.0	12.0	12.0	12.0			
Turn Bay Length, ft									0		0	0	0	0	0			
Grade (P _g), %							0			0			0			0		
Speed Limit, mi/h									35		35	35	35	35	35			
Phase Information						EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT					
Maximum Green (G _{max}) or Phase Split, s								45.0	45.0		55.0	15.0	55.0					
Yellow Change Interval (Y), s								4.0	4.0		4.0	4.0	4.0					
Red Clearance Interval (R _c), s								2.3	2.3		2.2	2.0	2.2					
Minimum Green (G _{min}), s								7	7		15	4	15					
Start-Up Lost Time (l _t), s								2.0			2.0	2.0	2.0					
Extension of Effective Green (e), s								2.0			2.0	2.0	2.0					
Passage (PT), s								2.0	2.0		2.0	2.0	2.0					
Recall Mode								Off	Off		Min	Off	Min					
Dual Entry								No	Yes		Yes	No	Yes					
Walk (Walk), s							0.0		0.0		0.0							
Pedestrian Clearance Time (PC), s							0.0		0.0		0.0							
Multimodal Information						EB			WB			NB			SB			
85th % Speed / Rest in Walk / Corner Radius						0	No	25	0	No	25	0	No	25				
Walkway / Crosswalk Width / Length, ft						9.0	12	0	9.0	12	0	9.0	12	0				
Street Width / Island / Curb							0		0	0	No	0	0	No	0		No	
Width Outside / Bike Lane / Shoulder, ft									12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	
Pedestrian Signal / Occupied Parking						No			No	0.50		No	0.50			0.50		

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP			Analysis Date	9/2/2023		
Jurisdiction				Area Type	Other		
Urban Street	Green River Parkway			Time Period	PHF		
Intersection	NE Dixie Hwy			Analysis Year	2030		
Project Description	PM Post			File Name	Green River and Dixie PM.xus		
							

Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				389		49		699	525	39	326	

Signal Information													
Cycle, s	75.8	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.4	34.6	20.4	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

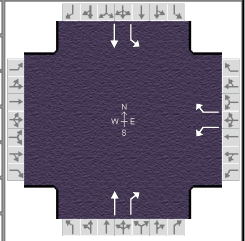
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase				8		2	1	6
Case Number				9.0		7.3	1.0	4.0
Phase Duration, s				26.7		40.8	8.4	49.1
Change Period, ($Y+R_c$), s				6.3		6.2	6.0	6.2
Max Allow Headway (MAH), s				3.1		3.1	3.1	3.1
Queue Clearance Time (g_s), s				19.3		30.3	2.9	9.7
Green Extension Time (g_e), s				0.9		4.1	0.0	4.2
Phase Call Probability				1.00		1.00	0.59	1.00
Max Out Probability				0.00		0.03	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3		18		2	12	1		6
Adjusted Flow Rate (v), veh/h				423		53		760	571	42		354
Adjusted Saturation Flow Rate (s), veh/h/ln				1781		1585		1870	1585	1781		1870
Queue Service Time (g_s), s				17.3		1.9		28.3	23.3	0.9		7.7
Cycle Queue Clearance Time (g_c), s				17.3		1.9		28.3	23.3	0.9		7.7
Green Ratio (g/C)				0.27		0.27		0.46	0.46	0.52		0.57
Capacity (c), veh/h				479		426		854	723	210		1060
Volume-to-Capacity Ratio (X)				0.882		0.125		0.890	0.789	0.202		0.334
Back of Queue (Q), ft/ln (95 th percentile)				292.5		31.3		432.1	308.2	14.6		121.5
Back of Queue (Q), veh/ln (95 th percentile)				11.5		1.2		17.0	12.1	0.6		4.8
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00		0.00
Uniform Delay (d_1), s/veh				26.7		21.0		18.9	17.6	15.6		8.8
Incremental Delay (d_2), s/veh				2.2		0.0		3.1	0.7	0.2		0.1
Initial Queue Delay (d_3), s/veh				0.0		0.0		0.0	0.0	0.0		0.0
Control Delay (d), s/veh				28.8		21.1		22.1	18.3	15.8		8.9
Level of Service (LOS)				C		C		C	B	B		A
Approach Delay, s/veh / LOS	0.0			28.0		C		20.4	C	9.6		A
Intersection Delay, s/veh / LOS				20.1						C		

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other
Jurisdiction				Time Period			
Urban Street	Green River Parkway		Analysis Year	2030		Analysis Period	1 > 16:00
Intersection	NE Dixie Hwy		File Name	Green River and Dixie PM.xus			
Project Description	PM Post						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				389		49		699	525	39	326	

Signal Information													
Cycle, s	75.8	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.4	34.6	20.4	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0			
				Red	2.0	2.2	2.3	0.0	0.0	0.0			

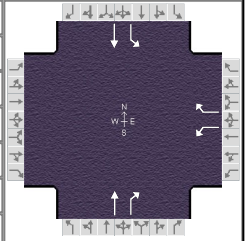
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	1.000	0.984	1.000	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	0.000	0.000	0.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		1.000	1.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.000	0.847		0.000	0.847		1.000	1.000
Left-Turn Pedestrian Adjustment Factor (f_{LPB})				1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{Rpb})						1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1781	0	1585	0	1870	1585	1781	1870	0
Proportion of Vehicles Arriving on Green (P)	0.00	0.00	0.00	0.27	0.00	0.27	0.00	0.46	0.46	0.03	0.57	0.00
Incremental Delay Factor (k)				0.04		0.04		0.10	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)				6.3		6.2	6.0	6.2
Green Ratio (g/C)				0.27		0.46	0.52	0.57
Permitted Saturation Flow Rate (s_p), veh/h/ln				1781		1043	706	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln						0		
Permitted Effective Green Time (g_p), s				0.0		0.0	36.7	0.0
Permitted Service Time (g_u), s				0.0		0.0	6.4	0.0
Permitted Queue Service Time (g_{ps}), s							1.9	
Time to First Blockage (g_t), s				0.0		34.7	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

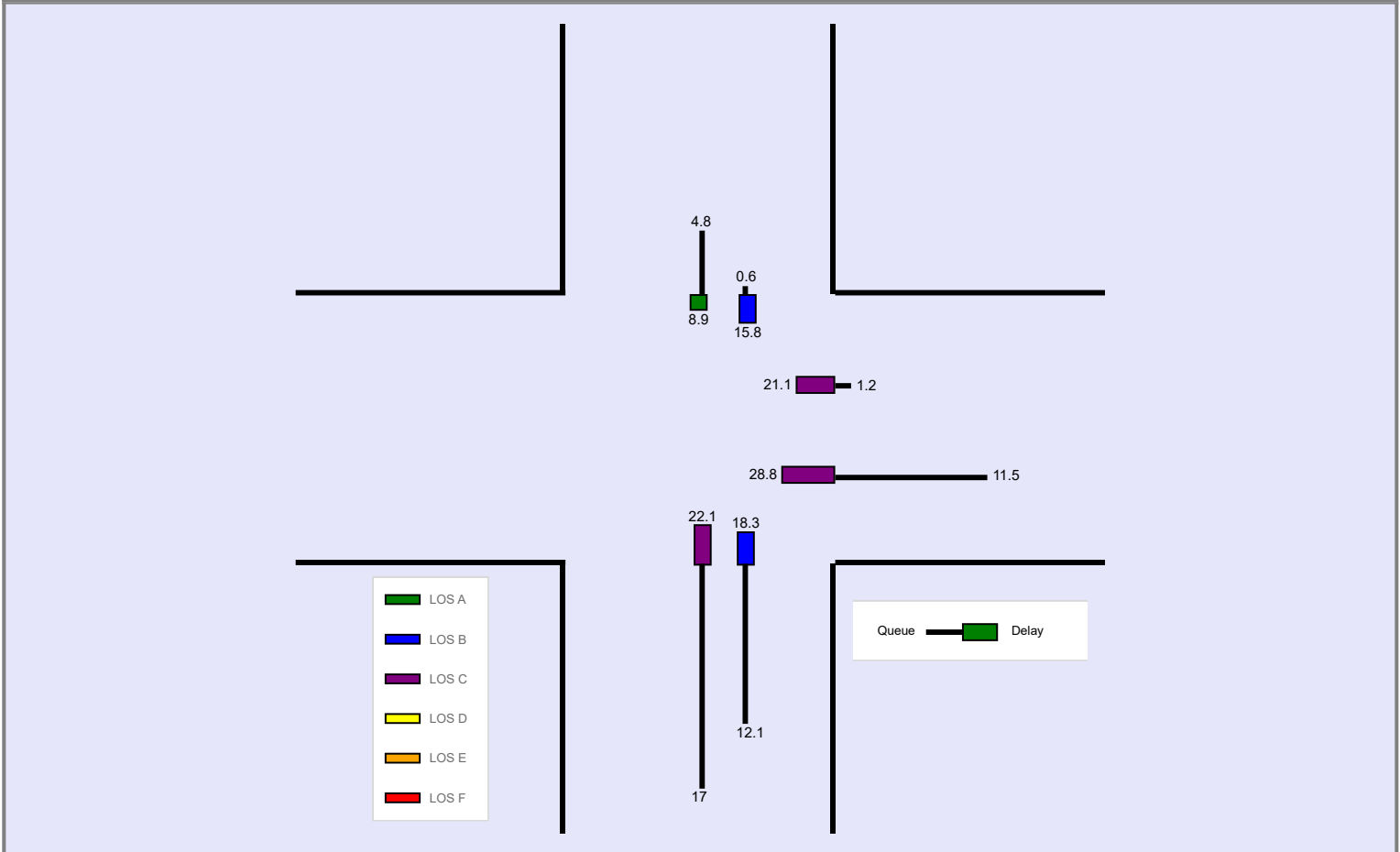
General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP		Analysis Date	9/2/2023		Area Type	Other
Jurisdiction				Time Period			
Urban Street	Green River Parkway		Analysis Year	2030		Analysis Period	1 > 16:00
Intersection	NE Dixie Hwy		File Name	Green River and Dixie PM.xus			
Project Description	PM Post						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h				389		49		699	525	39	326	

Signal Information														
Cycle, s	75.8	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.4	34.6	20.4	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	4.0	0.0	0.0	0.0				
				Red	2.0	2.2	2.3	0.0	0.0	0.0				

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)				292.5		31.3		432.1	308.2	14.6	121.5	
Back of Queue (Q), veh/ln (95 th percentile)				11.5		1.2		17.0	12.1	0.6	4.8	
Queue Storage Ratio (RQ) (95 th percentile)				0.00		0.00		0.00	0.00	0.00	0.00	
Control Delay (d), s/veh				28.8		21.1		22.1	18.3	15.8	8.9	
Level of Service (LOS)				C		C		C	B	B	A	
Approach Delay, s/veh / LOS	0.0			28.0		C	20.4		C	9.6		A
Intersection Delay, s/veh / LOS	20.1						C					



Avonlea 10-11-13-14-15

AM PEAK HOUR TURNING MOVEMENTS

EXHIBIT 11

Dixie Highway & Cardinal Road

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
7:00 AM	7:15 AM	16	25	5	36	20	5	2	30	9	10	59	0
7:15 AM	7:30 AM	13	19	0	25	14	2	0	27	17	13	70	1
7:30 AM	7:45 AM	11	28	3	37	24	7	0	39	29	13	79	10
7:45 AM	8:00 AM	21	32	3	29	26	6	0	43	27	20	69	4
8:00 AM	8:15 AM	13	30	1	29	23	9	0	52	33	17	87	7
8:15 AM	8:30 AM	20	31	3	34	25	21	1	46	27	22	66	7
8:30 AM	8:45 AM	13	25	3	23	32	14	0	42	17	18	66	3
8:45 AM	9:00 AM	19	30	0	30	27	17	0	46	27	21	47	4
9:00 AM	9:15 AM												
9:15 AM	9:30 AM												
9:30 AM	9:45 AM												
9:45 AM	10:00 AM												
		126	220	18	243	191	81	3	325	186	134	543	36

Peak Hour Traffic Volume

7:30 AM	8:30 AM	65	121	10	129	98	43	1	180	116	72	301	28
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Count Taken:	6/29/2023	PHF	0.96
Buildout year:	2030		
Growth Rate:	1.3%		
PSCF	1.22		

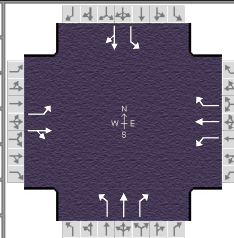
	Cardinal			NE Dixie			NE Dixie			Savanna		
	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/28/2023	65	121	10	129	98	43	1	180	116	72	301	28
Peak Season Factor	14	27	2	28	22	9	0	40	26	16	66	6
Adjusted Volumes	79	148	12	157	120	52	1	220	142	88	367	34
Growth 1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
2030 Volumes	9	16	1	17	13	6	0	24	15	10	40	4
Pre Dev	88	164	13	174	133	58	1	244	157	98	407	38
Project Traffic	1	0	6	2	2	0	4	9	5	3	11	1
Post Dev	89	164	19	176	135	58	5	253	162	101	418	39

Project Traffic Assignment	In	Out	Out	Out	In	In
	0%	4%	0%	7%	4%	10%
	0%	0%	0%	0%	7%	10%

Existing	79	148	12	157	120	52	1	220	142	88	367	34
Pre-Development	88	164	13	174	133	58	1	244	157	98	407	38
Post-Development	89	164	19	176	135	58	5	253	162	101	418	39

HCS7 Signalized Intersection Input Data

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM Existing							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	79	148	12	157	120	52	1	220	142	88	367	34

Signal Information													
Cycle, s	55.9	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.1	1.8	7.4	0.1	5.5	10.4			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

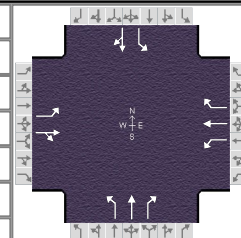
Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	79	148	12	157	120	52	1	220	142	88	367	34
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h	None			None			None			None		
Heavy Vehicles (P _{HV}), %	2	2		2	2	2	2	2	2	2	2	
Ped / Bike / RTOR, /h	0	0	0	0	0	20	0	0	60	0	0	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	
Turn Bay Length, ft	170	0		340	0	0	240	0	220	0	0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	35	35	35	35	35	35	35	35	35	35	35	35

Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	15.0	25.0	20.0	25.0	15.0	45.0	15.0	45.0
Yellow Change Interval (Y), s	3.4	3.4	3.7	4.0	3.7	4.0	4.0	4.4
Red Clearance Interval (R _c), s	3.3	3.4	3.4	2.0	2.4	2.0	2.4	2.0
Minimum Green (G _{min}), s	7	7	7	7	7	10	7	10
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Recall Mode	Off	Min	Ped	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25	0	No	25	0	No	25	0	No	25
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No	0	0	No	0	0	No	0	0	No
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50		No	0.50		No	0.50		No	0.50	

HCS7 Signalized Intersection Results Summary

General Information					Intersection Information				
Agency	MEP				Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other		
Jurisdiction			Time Period	7:00-9:00 AM		PHF	0.95		
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 7:00		
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus					
Project Description	AM Existing								



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	79	148	12	157	120	52	1	220	142	88	367	34

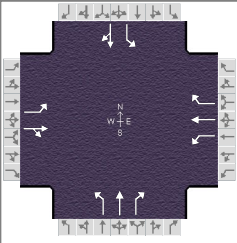
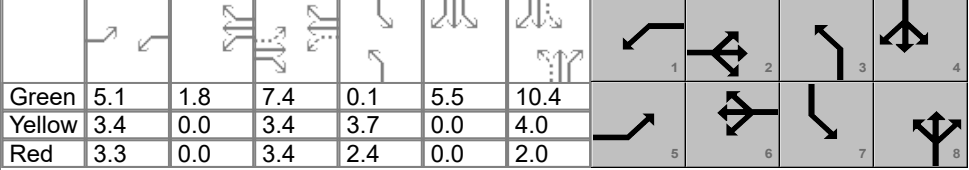
Signal Information																	
Cycle, s	55.9	Reference Phase	2														
Offset, s	0	Reference Point	Begin														
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.1	1.8	7.4	0.1	5.5	10.4	Yellow	3.4	0.0	3.4	3.7	0.0	4.0
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0							

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	3.0	1.1	3.0	1.1	4.0
Phase Duration, s	11.8	14.2	13.6	16.0	6.2	16.4	11.7	21.9
Change Period, (Y+R _c), s	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	4.1	6.9	6.3	5.4	2.0	8.5	4.2	14.0
Green Extension Time (g _e), s	0.1	0.5	0.2	0.5	0.0	1.4	0.1	1.4
Phase Call Probability	0.73	1.00	1.00	1.00	0.02	1.00	0.76	1.00
Max Out Probability	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	83	168		165	126	34	1	232	86	93	422	
Adjusted Saturation Flow Rate (s), veh/h/ln	1781	1845		1781	1870	1585	1781	1870	1585	1781	1842	
Queue Service Time (g _s), s	2.1	4.9		4.3	3.4	1.0	0.0	6.5	2.6	2.2	12.0	
Cycle Queue Clearance Time (g _c), s	2.1	4.9		4.3	3.4	1.0	0.0	6.5	2.6	2.2	12.0	
Green Ratio (g/C)	0.22	0.13		0.25	0.16	0.16	0.18	0.18	0.18	0.29	0.28	
Capacity (c), veh/h	378	245		391	308	261	159	335	284	372	512	
Volume-to-Capacity Ratio (X)	0.220	0.687		0.423	0.410	0.129	0.007	0.692	0.304	0.249	0.825	
Back of Queue (Q), ft/ln (95 th percentile)	36	91.9		72.5	62.4	15.8	0.5	120.9	41	35.5	207.6	
Back of Queue (Q), veh/ln (95 th percentile)	1.4	3.6		2.9	2.5	0.6	0.0	4.8	1.6	1.4	8.2	
Queue Storage Ratio (RQ) (95 th percentile)	0.21	0.00		0.21	0.00	0.00	0.00	0.00	0.19	0.00	0.00	
Uniform Delay (d ₁), s/veh	17.8	23.2		17.7	20.9	20.0	19.4	21.5	20.0	15.2	18.9	
Incremental Delay (d ₂), s/veh	0.1	1.3		0.3	0.3	0.1	0.0	1.0	0.2	0.1	1.3	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	17.9	24.4		17.9	21.3	20.0	19.4	22.5	20.2	15.4	20.2	
Level of Service (LOS)	B	C		B	C	C	B	C	C	B	C	
Approach Delay, s/veh / LOS	22.3	C		19.5	B		21.9	C		19.4	B	
Intersection Delay, s/veh / LOS	20.5						C					

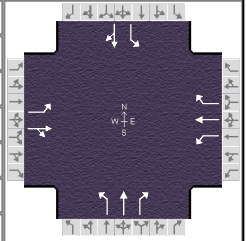
Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information					Intersection Information										
Agency	MEP				Duration, h	0.250									
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other								
Jurisdiction			Time Period	7:00-9:00 AM		PHF	0.95								
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 7:00								
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus											
Project Description	AM Existing														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				79	148	12	157	120	52	1	220	142	88	367	34
Signal Information															
Cycle, s	55.9	Reference Phase	2												
Offset, s	0	Reference Point	Begin												
Uncoordinated	Yes	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green				5.1	1.8	7.4	0.1	5.5	10.4						
Yellow				3.4	0.0	3.4	3.7	0.0	4.0						
Red				3.3	0.0	3.4	2.4	0.0	2.0						
Saturation Flow / Delay				L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	0.984	1.000	0.984	0.984	0.984	0.984	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.987	0.987		0.000	0.847		0.000	0.847		0.985	0.985
Left-Turn Pedestrian Adjustment Factor (f_{LPB})				1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})						1.000			1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1781	1707	138	1781	1870	1585	1781	1870	1585	1781	1686	156
Proportion of Vehicles Arriving on Green (P)				0.09	0.13	0.13	0.12	0.16	0.16	0.00	0.18	0.18	0.10	0.28	0.28
Incremental Delay Factor (k)				0.04	0.04		0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	
Signal Timing / Movement Groups				EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R				
Lost Time (t_L)				6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4	6.4			
Green Ratio (g/C)				0.22	0.13	0.25	0.16	0.18	0.18	0.29	0.28				
Permitted Saturation Flow Rate (s_p), veh/h/ln				1264	0	1217	0	965	0	1149	0				
Shared Saturation Flow Rate (s_{sh}), veh/h/ln															
Permitted Effective Green Time (g_p), s				7.4	0.0	7.4	0.0	10.0	0.0	11.2	0.0				
Permitted Service Time (g_u), s				3.9	0.0	2.6	0.0	1.6	0.0	3.5	0.0				
Permitted Queue Service Time (g_{ps}), s				0.3		0.8		0.0		0.7					
Time to First Blockage (g_t), s				0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Queue Service Time Before Blockage (g_{ts}), s															
Protected Right Saturation Flow (s_R), veh/h/ln							0		0						
Protected Right Effective Green Time (g_R), s							0.0		0.0						
Multimodal				EB			WB			NB			SB		
Pedestrian F_w / F_v															
Pedestrian F_s / F_{delay}															
Pedestrian M_{corner} / M_{cw}															
Bicycle c_b / d_b															
Bicycle F_w / F_v															

HCS7 Signalized Intersection Results Graphical Summary

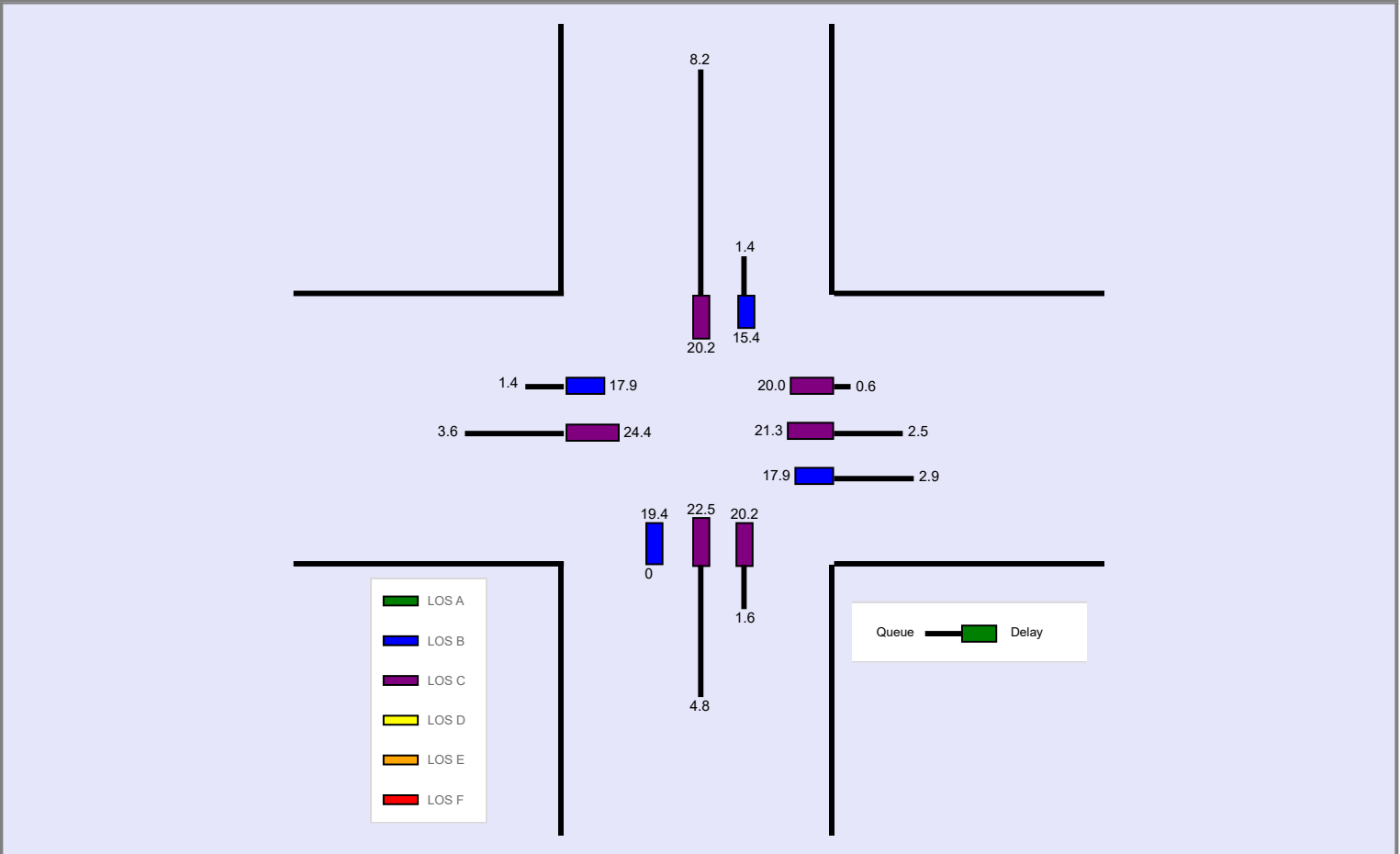
General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM Existing							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	79	148	12	157	120	52	1	220	142	88	367	34

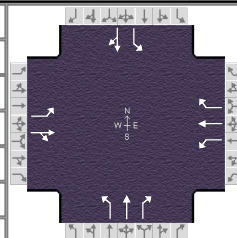
Signal Information				Signal Phases									
Cycle, s	55.9	Reference Phase	2										
Offset, s	0	Reference Point	Begin	Green	5.1	1.8	7.4	0.1	5.5	10.4			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)	36	91.9		72.5	62.4	15.8	0.5	120.9	41	35.5	207.6	
Back of Queue (Q), veh/ln (95 th percentile)	1.4	3.6		2.9	2.5	0.6	0.0	4.8	1.6	1.4	8.2	
Queue Storage Ratio (RQ) (95 th percentile)	0.21	0.00		0.21	0.00	0.00	0.00	0.00	0.19	0.00	0.00	
Control Delay (d), s/veh	17.9	24.4		17.9	21.3	20.0	19.4	22.5	20.2	15.4	20.2	
Level of Service (LOS)	B	C		B	C	C	B	C	C	B	C	
Approach Delay, s/veh / LOS	22.3		C	19.5		B	21.9		C	19.4		B
Intersection Delay, s/veh / LOS	20.5						C					



HCS7 Signalized Intersection Input Data

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM Pre-Development							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	88	164	13	174	133	58	1	244	157	98	407	38

Signal Information															
Cycle, s	59.9	Reference Phase	2	[Diagram: EB Left]		[Diagram: EB Thru]		[Diagram: WB Left]		[Diagram: WB Thru]		[Diagram: NB Left]		[Diagram: NB Thru]	
Offset, s	0	Reference Point	Begin	Green	5.5	1.9	8.4	0.1	5.9	12.4	[Diagram: SB Left]		[Diagram: SB Thru]		
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0	[Diagram: EB Right]		[Diagram: WB Right]		
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0	[Diagram: NB Right]		[Diagram: SB Right]		

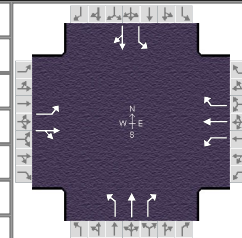
Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	88	164	13	174	133	58	1	244	157	98	407	38
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h	None			None			None			None		
Heavy Vehicles (P _{HV}), %	2	2		2	2	2	2	2	2	2	2	
Ped / Bike / RTOR, /h	0	0	0	0	0	20	0	0	60	0	0	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	
Turn Bay Length, ft	170	0		340	0	0	240	0	220	0	0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	35	35	35	35	35	35	35	35	35	35	35	35

Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	15.0	25.0	20.0	25.0	15.0	45.0	15.0	45.0
Yellow Change Interval (Y), s	3.4	3.4	3.7	4.0	3.7	4.0	4.0	4.4
Red Clearance Interval (R _c), s	3.3	3.4	3.4	2.0	2.4	2.0	2.4	2.0
Minimum Green (G _{min}), s	7	7	7	7	7	10	7	10
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25	0	No	25	0	No	25	0	No	25
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No	0	0	No	0	0	No	0	0	No
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50		No	0.50		No	0.50		No	0.50	

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1> 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM Pre-Development							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	88	164	13	174	133	58	1	244	157	98	407	38

Signal Information																	
Cycle, s	59.9	Reference Phase	2	Green		Yellow		Red		1		2		3		4	
Offset, s	0	Reference Point	Begin	5.5	1.9	8.4	0.1	5.9	12.4	5		6		7		8	
Uncoordinated	Yes	Simult. Gap E/W	On	3.4	0.0	3.4	3.7	0.0	4.0	5		6		7		8	
Force Mode	Fixed	Simult. Gap N/S	On	3.3	0.0	3.4	2.4	0.0	2.0	5		6		7		8	

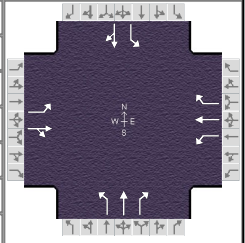
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	3.0	1.1	3.0	1.1	4.0
Phase Duration, s	12.2	15.2	14.1	17.1	6.2	18.4	12.2	24.4
Change Period, (Y+R _c), s	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	4.5	7.8	7.1	6.0	2.0	9.6	4.5	16.3
Green Extension Time (g _e), s	0.1	0.6	0.3	0.6	0.0	1.6	0.1	1.6
Phase Call Probability	0.79	1.00	0.95	1.00	0.02	1.00	0.82	1.00
Max Out Probability	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	93	186		183	140	40	1	257	102	103	468	
Adjusted Saturation Flow Rate (s), veh/h/ln	1781	1846		1781	1870	1585	1781	1870	1585	1781	1842	
Queue Service Time (g _s), s	2.5	5.8		5.1	4.0	1.3	0.0	7.6	3.3	2.5	14.3	
Cycle Queue Clearance Time (g _c), s	2.5	5.8		5.1	4.0	1.3	0.0	7.6	3.3	2.5	14.3	
Green Ratio (g/C)	0.23	0.14		0.26	0.17	0.17	0.20	0.20	0.20	0.32	0.30	
Capacity (c), veh/h	376	259		382	323	274	150	376	319	375	553	
Volume-to-Capacity Ratio (X)	0.247	0.718		0.479	0.434	0.146	0.007	0.683	0.320	0.275	0.847	
Back of Queue (Q), ft/ln (95 th percentile)	43.7	110.9		87.8	75.4	20.3	0.5	142.9	51.7	41.4	241	
Back of Queue (Q), veh/ln (95 th percentile)	1.7	4.4		3.5	3.0	0.8	0.0	5.6	2.0	1.6	9.5	
Queue Storage Ratio (RQ) (95 th percentile)	0.26	0.00		0.26	0.00	0.00	0.00	0.00	0.23	0.00	0.00	
Uniform Delay (d ₁), s/veh	18.8	24.7		18.7	22.2	21.1	20.0	22.2	20.5	15.4	19.7	
Incremental Delay (d ₂), s/veh	0.1	1.4		0.3	0.3	0.1	0.0	0.8	0.2	0.1	1.4	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	18.9	26.1		19.1	22.5	21.2	20.0	23.0	20.7	15.5	21.1	
Level of Service (LOS)	B	C		B	C	C	B	C	C	B	C	
Approach Delay, s/veh / LOS	23.7		C	20.7		C	22.4		C	20.1		C
Intersection Delay, s/veh / LOS	21.4						C					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information					Intersection Information				
Agency	MEP				Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other		
Jurisdiction			Time Period	7:00-9:00 AM		PHF	0.95		
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00		
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus					
Project Description	AM Pre-Development								



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	88	164	13	174	133	58	1	244	157	98	407	38

Signal Information													
Cycle, s	59.9	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.5	1.9	8.4	0.1	5.9	12.4			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

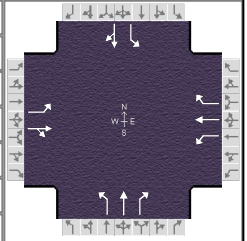
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	0.984	0.984	1.000	0.984	0.984	0.984	0.984	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.987	0.987		0.000	0.847		0.000	0.847		0.985	0.985
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	1781	1710	136	1781	1870	1585	1781	1870	1585	1781	1685	157
Proportion of Vehicles Arriving on Green (P)	0.09	0.14	0.14	0.12	0.17	0.17	0.00	0.20	0.20	0.10	0.30	0.30
Incremental Delay Factor (k)	0.04	0.04		0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Green Ratio (g/C)	0.23	0.14	0.26	0.17	0.20	0.20	0.32	0.30
Permitted Saturation Flow Rate (s_p), veh/h/ln	1249	0	1197	0	925	0	1123	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	8.4	0.0	8.4	0.0	12.1	0.0	13.6	0.0
Permitted Service Time (g_u), s	4.4	0.0	2.7	0.0	1.7	0.0	4.5	0.0
Permitted Queue Service Time (g_{ps}), s	0.3		1.0		0.0		0.9	
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

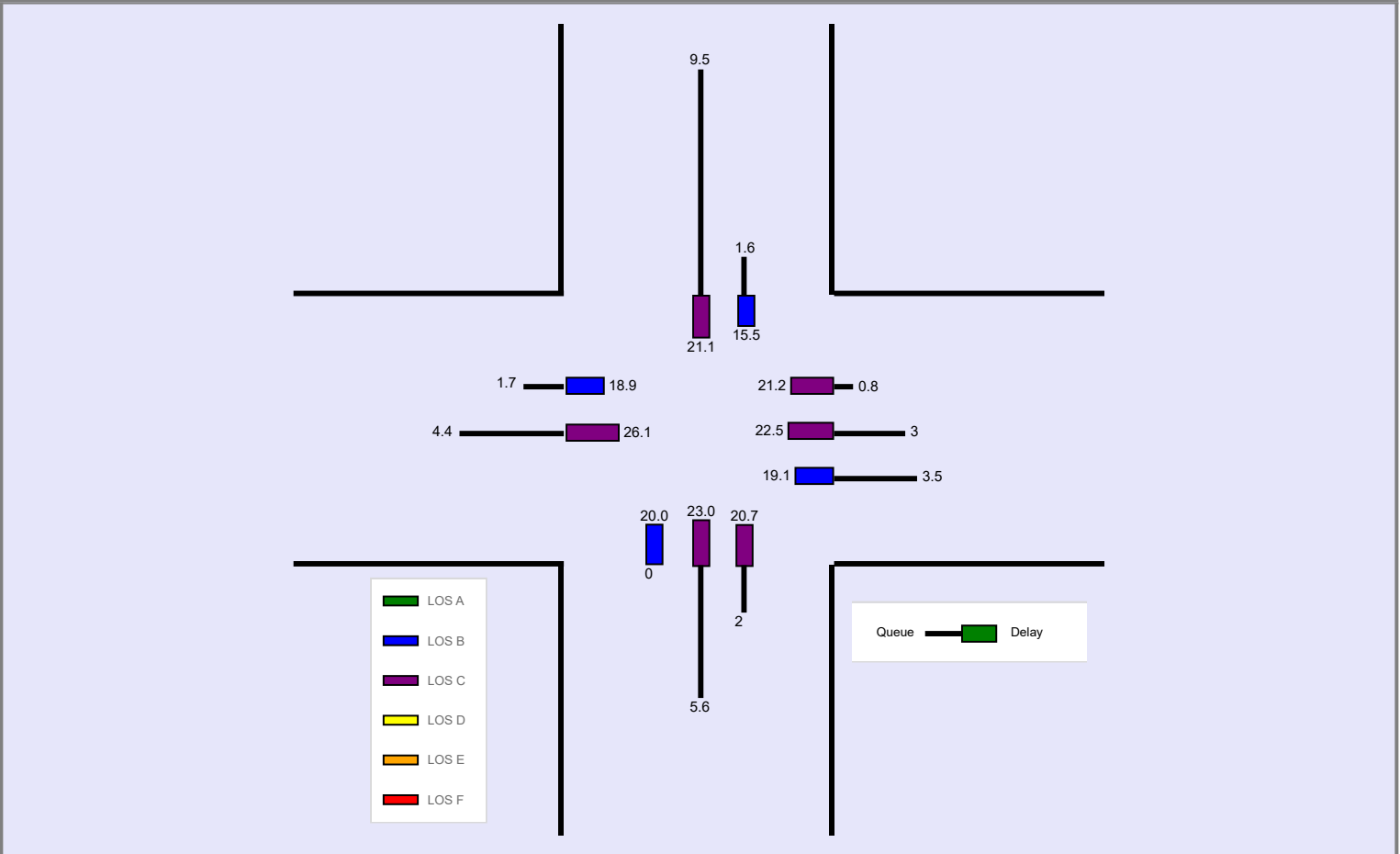
General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Apr 24, 2024		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM Pre-Development							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	88	164	13	174	133	58	1	244	157	98	407	38

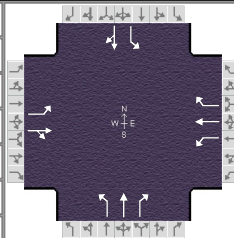
Signal Information				Phase Diagrams										
Cycle, s	59.9	Reference Phase	2											
Offset, s	0	Reference Point	Begin	Green	5.5	1.9	8.4	0.1	5.9	12.4				
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0				
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0				

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)	43.7	110.9		87.8	75.4	20.3	0.5	142.9	51.7	41.4	241	
Back of Queue (Q), veh/ln (95 th percentile)	1.7	4.4		3.5	3.0	0.8	0.0	5.6	2.0	1.6	9.5	
Queue Storage Ratio (RQ) (95 th percentile)	0.26	0.00		0.26	0.00	0.00	0.00	0.00	0.23	0.00	0.00	
Control Delay (d), s/veh	18.9	26.1		19.1	22.5	21.2	20.0	23.0	20.7	15.5	21.1	
Level of Service (LOS)	B	C		B	C	C	B	C	C	B	C	
Approach Delay, s/veh / LOS	23.7		C	20.7		C	22.4		C	20.1		C
Intersection Delay, s/veh / LOS	21.4						C					



HCS7 Signalized Intersection Input Data

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Jun 29, 2023		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	89	164	19	176	135	58	5	253	162	101	418	39

Signal Information																	
Cycle, s	62.2	Reference Phase	2														
Offset, s	0	Reference Point	Begin	Green	5.6	2.1	8.9	0.6	5.6	13.8	Yellow	3.4	0.0	3.4	3.7	0.0	4.0
Uncoordinated	Yes	Simult. Gap E/W	On	Red	3.3	0.0	3.4	2.4	0.0	2.0							
Force Mode	Fixed	Simult. Gap N/S	On														

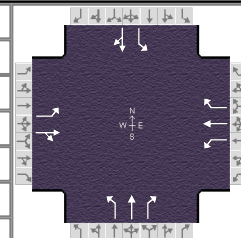
Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	89	164	19	176	135	58	5	253	162	101	418	39
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		None			None			None			None	
Heavy Vehicles (P _{HV}), %	2	2		2	2	2	2	2	2	2	2	
Ped / Bike / RTOR, /h	0	0	0	0	0	20	0	0	60	0	0	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	
Turn Bay Length, ft	170	0		340	0	0	240	0	220	0	0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	35	35	35	35	35	35	35	35	35	35	35	35

Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	15.0	25.0	20.0	25.0	15.0	45.0	15.0	45.0
Yellow Change Interval (Y), s	3.4	3.4	3.7	4.0	3.7	4.0	4.0	4.4
Red Clearance Interval (R _c), s	3.3	3.4	3.4	2.0	2.4	2.0	2.4	2.0
Minimum Green (G _{min}), s	7	7	7	7	7	10	7	10
Start-Up Lost Time (I _t), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25	0	No	25	0	No	25	0	No	25
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No	0	0	No	0	0	No	0	0	No
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50		No	0.50		No	0.50		No	0.50	

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Jun 29, 2023		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	89	164	19	176	135	58	5	253	162	101	418	39

Signal Information													
Cycle, s	62.2	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On										
Force Mode	Fixed	Simult. Gap N/S	On										
		Green		5.6	2.1	8.9	0.6	5.6	13.8				
		Yellow		3.4	0.0	3.4	3.7	0.0	4.0				
		Red		3.3	0.0	3.4	2.4	0.0	2.0				

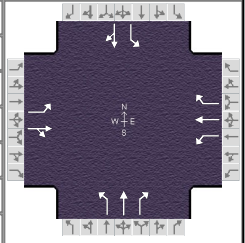
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	3.0	1.1	3.0	1.1	4.0
Phase Duration, s	12.3	15.7	14.4	17.8	6.7	19.8	12.3	25.4
Change Period, (Y+R _c), s	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	4.7	8.3	7.3	6.2	2.1	10.1	4.7	17.3
Green Extension Time (g _e), s	0.1	0.6	0.3	0.6	0.0	1.7	0.1	1.7
Phase Call Probability	0.80	1.00	0.96	1.00	0.09	1.00	0.84	1.00
Max Out Probability	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	94	193		185	142	40	5	266	107	106	481	
Adjusted Saturation Flow Rate (s), veh/h/ln	1781	1836		1781	1870	1585	1781	1870	1585	1781	1842	
Queue Service Time (g _s), s	2.7	6.3		5.3	4.2	1.3	0.1	8.1	3.6	2.7	15.3	
Cycle Queue Clearance Time (g _c), s	2.7	6.3		5.3	4.2	1.3	0.1	8.1	3.6	2.7	15.3	
Green Ratio (g/C)	0.23	0.14		0.26	0.18	0.18	0.23	0.22	0.22	0.33	0.31	
Capacity (c), veh/h	374	263		376	331	280	159	404	342	381	563	
Volume-to-Capacity Ratio (X)	0.250	0.733		0.492	0.430	0.143	0.033	0.659	0.314	0.279	0.854	
Back of Queue (Q), ft/ln (95 th percentile)	46.3	120.4		92.8	79.9	21.1	2.5	152	55.8	44.5	255.4	
Back of Queue (Q), veh/ln (95 th percentile)	1.8	4.7		3.7	3.1	0.8	0.1	6.0	2.2	1.8	10.1	
Queue Storage Ratio (RQ) (95 th percentile)	0.27	0.00		0.27	0.00	0.00	0.01	0.00	0.25	0.00	0.00	
Uniform Delay (d ₁), s/veh	19.5	25.6		19.4	22.9	21.7	19.8	22.3	20.5	15.6	20.3	
Incremental Delay (d ₂), s/veh	0.1	1.5		0.4	0.3	0.1	0.0	0.7	0.2	0.1	1.5	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	19.6	27.0		19.7	23.2	21.7	19.9	23.0	20.7	15.8	21.8	
Level of Service (LOS)	B	C		B	C	C	B	C	C	B	C	
Approach Delay, s/veh / LOS	24.6	C		21.3	C		22.3	C		20.7	C	
Intersection Delay, s/veh / LOS	21.9						C					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Jun 29, 2023		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	89	164	19	176	135	58	5	253	162	101	418	39

Signal Information													
Cycle, s	62.2	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.6	2.1	8.9	0.6	5.6	13.8			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

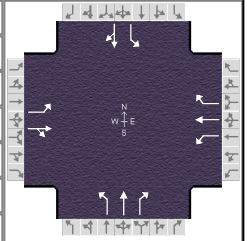
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	0.984	0.984	1.000	0.984	0.984	0.984	0.984	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.982	0.982		0.000	0.847		0.000	0.847		0.985	0.985
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	1781	1645	191	1781	1870	1585	1781	1870	1585	1781	1685	157
Proportion of Vehicles Arriving on Green (P)	0.09	0.14	0.14	0.12	0.18	0.18	0.01	0.22	0.22	0.09	0.31	0.31
Incremental Delay Factor (k)	0.04	0.04		0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Green Ratio (g/C)	0.23	0.14	0.26	0.18	0.23	0.22	0.33	0.31
Permitted Saturation Flow Rate (s_p), veh/h/ln	1246	0	1190	0	914	0	1113	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	8.9	0.0	8.9	0.0	13.5	0.0	14.7	0.0
Permitted Service Time (g_u), s	4.9	0.0	2.7	0.0	1.8	0.0	5.4	0.0
Permitted Queue Service Time (g_{ps}), s	0.3		1.2		0.1		1.0	
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

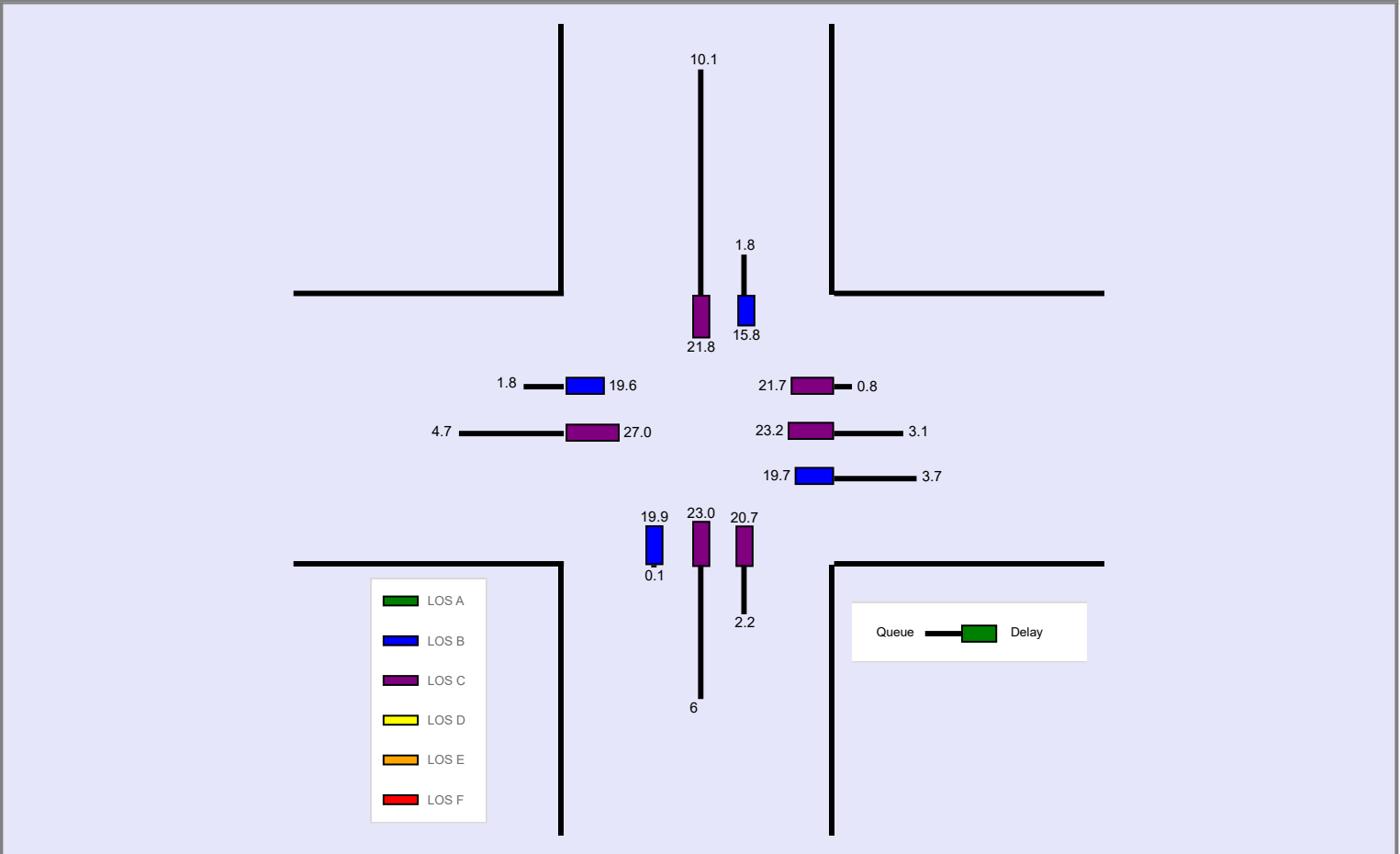
General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	Jun 29, 2023		Area Type	Other	
Jurisdiction				Time Period	7:00-9:00 AM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 7:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie AM.xus				
Project Description	AM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	89	164	19	176	135	58	5	253	162	101	418	39

Signal Information				Phase Diagrams									
Cycle, s	62.2	Reference Phase	2										
Offset, s	0	Reference Point	Begin	Green	5.6	2.1	8.9	0.6	5.6	13.8			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)	46.3	120.4		92.8	79.9	21.1	2.5	152	55.8	44.5	255.4	
Back of Queue (Q), veh/ln (95 th percentile)	1.8	4.7		3.7	3.1	0.8	0.1	6.0	2.2	1.8	10.1	
Queue Storage Ratio (RQ) (95 th percentile)	0.27	0.00		0.27	0.00	0.00	0.01	0.00	0.25	0.00	0.00	
Control Delay (d), s/veh	19.6	27.0		19.7	23.2	21.7	19.9	23.0	20.7	15.8	21.8	
Level of Service (LOS)	B	C		B	C	C	B	C	C	B	C	
Approach Delay, s/veh / LOS	24.6		C	21.3		C	22.3		C	20.7		C
Intersection Delay, s/veh / LOS	21.9						C					



Avonlea 10-11-13-14-15
 PM PEAK HOUR TURNING MOVEMENTS
 EXHIBIT 12
 Dixie Highway & Cardinal Road

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
4:00 PM	4:15 PM	17	29	0	28	38	18	1	80	41	17	42	4
4:15 PM	4:30 PM	13	27	0	20	31	23	0	78	32	23	42	5
4:30 PM	4:45 PM	11	15	0	25	34	17	0	60	24	17	49	2
4:45 PM	5:00 PM	18	33	2	25	33	21	2	85	30	22	48	1
5:00 PM	5:15 PM	14	31	0	31	41	19	2	73	30	18	53	0
5:15 PM	5:30 PM	17	31	0	21	37	19	0	111	31	17	36	0
5:30 PM	5:45 PM	7	24	2	28	24	22	1	73	29	13	41	1
5:45 PM	6:00 PM	8	14	3	28	29	21	2	51	22	17	42	2
6:00 PM	6:15 PM												
6:15 PM	6:30 PM												
6:30 PM	6:45 PM												
6:45 PM	7:00 PM												
		105	204	7	206	267	160	8	611	239	144	353	15

Peak Hour Traffic Volume

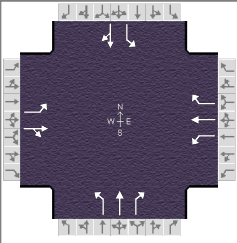










4:45 PM	5:45 PM	56	119	4	105	135	81	5	342	120	70	178	2
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Count Taken: 6/29/2023 PHF 0.95
 Buildout year: 2030
 Growth Rate: 1.3%
 PSCF 1.22

	Cardinal			NE Dixie			NE Dixie			Savanna		
	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/29/2023	56	119	4	105	135	81	5	342	120	70	178	2
Peak Season Factor	12	26	1	23	30	18	1	75	26	15	39	0
Adjusted Volumes	68	145	5	128	165	99	6	417	146	85	217	2
Growth 1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
2030 Volumes	75	161	6	142	183	110	7	462	162	94	241	2
Pre Dev	75	161	6	142	183	110	7	462	162	94	241	2
Project Traffic	3	2	17	6	4	0	11	4	5	1	14	3
Post Dev	78	163	23	148	187	110	18	466	167	95	255	5

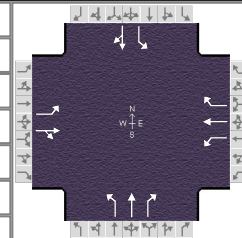
Project Traffic Assignment	In	Out	Out	Out	In	In	In	In	In	0%	0%	
Existing	68	145	5	128	165	99	6	417	146	85	217	2
Pre-Development	75	161	6	142	183	110	7	462	162	94	241	2
Post-Development	78	163	23	148	187	110	18	466	167	95	255	5

HCS7 Signalized Intersection Input Data

General Information					Intersection Information												
Agency	MEP				Duration, h	0.250											
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other										
Jurisdiction			Time Period	4:00-6:00 PM		PHF	0.95										
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 16:00										
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus													
Project Description	PM Existing																
Demand Information					EB			WB			NB			SB			
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h					68	145	5	128	165	99	6	417	146	85	217	2	
Signal Information																	
Cycle, s	64.6	Reference Phase	2														
Offset, s	0	Reference Point	Begin		Green	5.1	1.7	8.0	0.8	5.2	18.3						
Uncoordinated	Yes	Simult. Gap E/W	On		Yellow	3.4	0.0	3.4	3.7	0.0	4.0						
Force Mode	Fixed	Simult. Gap N/S	On		Red	3.3	0.0	3.4	2.4	0.0	2.0						
Traffic Information					EB			WB			NB			SB			
Approach Movement					L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h					68	145	5	128	165	99	6	417	146	85	217	2	
Initial Queue (Q _b), veh/h					0	0	0	0	0	0	0	0	0	0	0	0	
Base Saturation Flow Rate (s ₀), veh/h					1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Parking (N _m), man/h					None			None			None			None			
Heavy Vehicles (P _{HV}), %					2	2		2	2	2	2	2	2	2	2		
Ped / Bike / RTOR, /h					0	0	0	0	0	0	0	0	0	0	0	0	
Buses (N _b), buses/h					0	0	0	0	0	0	0	0	0	0	0	0	
Arrival Type (AT)					3	3	3	3	3	3	3	3	3	3	3	3	
Upstream Filtering (I)					1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Lane Width (W), ft					12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0		
Turn Bay Length, ft					170	0		340	0	0	240	0	220	0	0		
Grade (P _g), %						0			0			0			0		
Speed Limit, mi/h					35	35	35	35	35	35	35	35	35	35	35	35	
Phase Information					EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT					
Maximum Green (G _{max}) or Phase Split, s					15.0	25.0	20.0	25.0	15.0	45.0	15.0	45.0					
Yellow Change Interval (Y), s					3.4	3.4	3.7	4.0	3.7	4.0	4.0	4.4					
Red Clearance Interval (R _c), s					3.3	3.4	3.4	2.0	2.4	2.0	2.4	2.0					
Minimum Green (G _{min}), s					7	7	7	7	7	10	7	10					
Start-Up Lost Time (I _t), s					2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0					
Extension of Effective Green (e), s					2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0					
Passage (PT), s					2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0					
Recall Mode					Off	Min	Off	Min	Off	Off	Off	Off					
Dual Entry					No	Yes	No	Yes	No	Yes	No	Yes					
Walk (Walk), s						0.0		0.0		0.0		0.0					
Pedestrian Clearance Time (PC), s						0.0		0.0		0.0		0.0					
Multimodal Information					EB			WB			NB			SB			
85th % Speed / Rest in Walk / Corner Radius					0	No	25	0	No	25	0	No	25	0	No	25	
Walkway / Crosswalk Width / Length, ft					9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0	
Street Width / Island / Curb					0	0	No	0	0	No	0	0	No	0	0	No	
Width Outside / Bike Lane / Shoulder, ft					12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	
Pedestrian Signal / Occupied Parking					No	0.50	No	0.50	No	0.50	No	0.50	No	0.50			

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1> 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM Existing							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	68	145	5	128	165	99	6	417	146	85	217	2

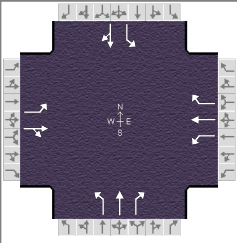
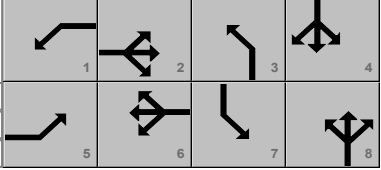
Signal Information													
Cycle, s	64.6	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.1	1.7	8.0	0.8	5.2	18.3			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	3.0	1.1	3.0	1.1	4.0
Phase Duration, s	11.8	14.8	13.5	16.5	6.9	24.3	12.0	29.5
Change Period, (Y+R _c), s	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	4.2	7.3	6.1	7.6	2.2	16.3	4.1	7.9
Green Extension Time (g _e), s	0.1	0.8	0.2	0.7	0.0	1.6	0.1	1.6
Phase Call Probability	0.72	1.00	0.91	1.00	0.11	1.00	0.80	1.00
Max Out Probability	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	72	158		135	174	104	6	439	154	89	231	
Adjusted Saturation Flow Rate (s), veh/h/ln	1781	1859		1781	1870	1585	1781	1870	1585	1781	1867	
Queue Service Time (g _s), s	2.2	5.3		4.1	5.6	3.9	0.2	14.3	5.0	2.1	5.9	
Cycle Queue Clearance Time (g _c), s	2.2	5.3		4.1	5.6	3.9	0.2	14.3	5.0	2.1	5.9	
Green Ratio (g/C)	0.20	0.12		0.22	0.15	0.15	0.29	0.28	0.28	0.38	0.36	
Capacity (c), veh/h	291	231		340	282	239	405	519	440	320	667	
Volume-to-Capacity Ratio (X)	0.246	0.684		0.396	0.617	0.437	0.016	0.845	0.349	0.280	0.346	
Back of Queue (Q), ft/ln (95 th percentile)	38.7	103.6		73.2	109.3	63.2	2.8	250.5	77.7	35.4	102.3	
Back of Queue (Q), veh/ln (95 th percentile)	1.5	4.1		2.9	4.3	2.5	0.1	9.9	3.1	1.4	4.0	
Queue Storage Ratio (RQ) (95 th percentile)	0.23	0.00		0.22	0.00	0.00	0.01	0.00	0.35	0.00	0.00	
Uniform Delay (d ₁), s/veh	21.7	27.1		21.4	25.7	25.0	16.4	22.1	18.7	15.2	15.2	
Incremental Delay (d ₂), s/veh	0.2	1.3		0.3	0.8	0.5	0.0	1.5	0.2	0.2	0.1	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	21.9	28.5		21.6	26.5	25.4	16.5	23.5	18.9	15.4	15.4	
Level of Service (LOS)	C	C		C	C	C	B	C	B	B	B	
Approach Delay, s/veh / LOS	26.4		C	24.7		C	22.3		C	15.4		B
Intersection Delay, s/veh / LOS	22.1						C					

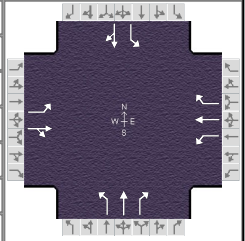
Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information					Intersection Information										
Agency	MEP				Duration, h	0.250									
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other								
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95							
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1> 16:00								
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus											
Project Description	PM Existing														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				68	145	5	128	165	99	6	417	146	85	217	2
Signal Information															
Cycle, s	64.6	Reference Phase	2												
Offset, s	0	Reference Point	Begin												
Uncoordinated	Yes	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green				5.1	1.7	8.0	0.8	5.2	18.3						
Yellow				3.4	0.0	3.4	3.7	0.0	4.0						
Red				3.3	0.0	3.4	2.4	0.0	2.0						
Saturation Flow / Delay				L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})				0.984	0.984	1.000	0.984	0.984	0.984	0.984	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})				0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})					0.994	0.994		0.000	0.847		0.000	0.847		0.998	0.998
Left-Turn Pedestrian Adjustment Factor (f_{LPb})				1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{Rpb})						1.000			1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})				1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h				1781	1797	62	1781	1870	1585	1781	1870	1585	1781	1850	17
Proportion of Vehicles Arriving on Green (P)				0.08	0.12	0.12	0.10	0.15	0.15	0.01	0.28	0.28	0.09	0.36	0.36
Incremental Delay Factor (k)				0.04	0.04		0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	
Signal Timing / Movement Groups				EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R				
Lost Time (t_L)				6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4	6.4			
Green Ratio (g/C)				0.20	0.12	0.22	0.15	0.29	0.28	0.38	0.36				
Permitted Saturation Flow Rate (s_p), veh/h/ln				1211	0	1229	0	1150	0	950	0				
Shared Saturation Flow Rate (s_{sh}), veh/h/ln															
Permitted Effective Green Time (g_p), s				8.0	0.0	8.0	0.0	18.0	0.0	18.7	0.0				
Permitted Service Time (g_u), s				2.1	0.0	2.8	0.0	15.3	0.0	3.7	0.0				
Permitted Queue Service Time (g_{ps}), s				0.4		0.6		0.0		1.6					
Time to First Blockage (g_t), s				0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Queue Service Time Before Blockage (g_{ts}), s															
Protected Right Saturation Flow (s_R), veh/h/ln							0		0						
Protected Right Effective Green Time (g_R), s							0.0		0.0						
Multimodal				EB			WB			NB			SB		
Pedestrian F_w / F_v															
Pedestrian F_s / F_{delay}															
Pedestrian M_{corner} / M_{cw}															
Bicycle c_b / d_b															
Bicycle F_w / F_v															

HCS7 Signalized Intersection Results Graphical Summary

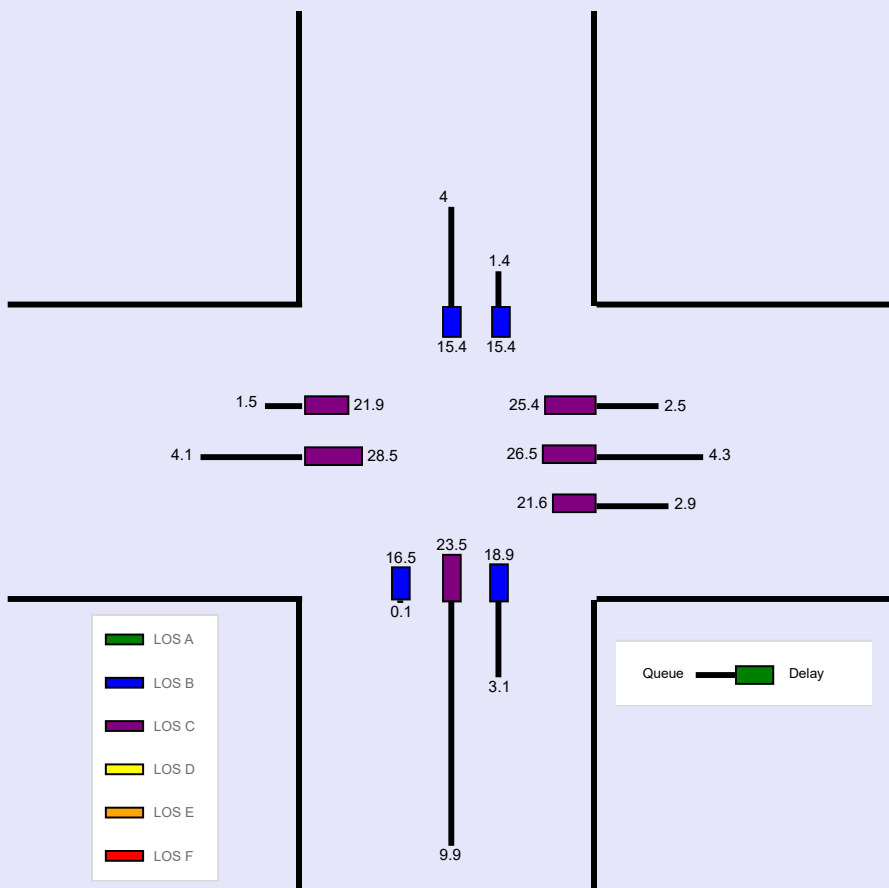
General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM Existing							



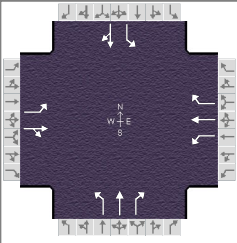










Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	68	145	5	128	165	99	6	417	146	85	217	2

Signal Information				Phase Diagrams									
Cycle, s	64.6	Reference Phase	2										
Offset, s	0	Reference Point	Begin	Green	5.1	1.7	8.0	0.8	5.2	18.3			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)	38.7	103.6		73.2	109.3	63.2	2.8	250.5	77.7	35.4	102.3	
Back of Queue (Q), veh/ln (95 th percentile)	1.5	4.1		2.9	4.3	2.5	0.1	9.9	3.1	1.4	4.0	
Queue Storage Ratio (RQ) (95 th percentile)	0.23	0.00		0.22	0.00	0.00	0.01	0.00	0.35	0.00	0.00	
Control Delay (d), s/veh	21.9	28.5		21.6	26.5	25.4	16.5	23.5	18.9	15.4	15.4	
Level of Service (LOS)	C	C		C	C	C	B	C	B	B	B	
Approach Delay, s/veh / LOS	26.4		C	24.7		C	22.3		C	15.4		B
Intersection Delay, s/veh / LOS	22.1						C					

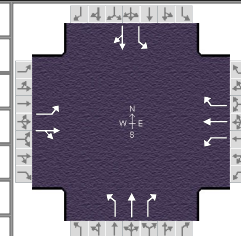


HCS7 Signalized Intersection Input Data

General Information					Intersection Information											
Agency	MEP				Duration, h	0.250										
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other									
Jurisdiction			Time Period	4:00-6:00 PM		PHF	0.95									
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1> 16:00									
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus												
Project Description	PM Pre-Development															
Demand Information				EB			WB			NB			SB			
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h				75	161	6	142	183	110	7	462	162	94	241	2	
Signal Information																
Cycle, s	69.5	Reference Phase	2													
Offset, s	0	Reference Point	Begin	Green	5.5	1.6	9.2	0.9	5.3	21.3						
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0						
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0						
Traffic Information				EB			WB			NB			SB			
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R	
Demand (v), veh/h				75	161	6	142	183	110	7	462	162	94	241	2	
Initial Queue (Q _b), veh/h				0	0	0	0	0	0	0	0	0	0	0	0	
Base Saturation Flow Rate (s ₀), veh/h				1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Parking (N _m), man/h				None			None			None			None			
Heavy Vehicles (P _{HV}), %				2	2		2	2	2	2	2	2	2	2		
Ped / Bike / RTOR, /h				0	0	0	0	0	0	0	0	0	0	0	0	
Buses (N _b), buses/h				0	0	0	0	0	0	0	0	0	0	0	0	
Arrival Type (AT)				3	3	3	3	3	3	3	3	3	3	3	3	
Upstream Filtering (I)				1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Lane Width (W), ft				12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0		
Turn Bay Length, ft				170	0		340	0	0	240	0	220	0	0		
Grade (P _g), %					0			0			0			0		
Speed Limit, mi/h				35	35	35	35	35	35	35	35	35	35	35	35	
Phase Information				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT					
Maximum Green (G _{max}) or Phase Split, s				15.0	25.0	20.0	25.0	15.0	45.0	15.0	45.0					
Yellow Change Interval (Y), s				3.4	3.4	3.7	4.0	3.7	4.0	4.0	4.4					
Red Clearance Interval (R _c), s				3.3	3.4	3.4	2.0	2.4	2.0	2.4	2.0					
Minimum Green (G _{min}), s				7	7	7	7	7	10	7	10					
Start-Up Lost Time (I _t), s				2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0					
Extension of Effective Green (e), s				2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0					
Passage (PT), s				2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0					
Recall Mode				Off	Min	Off	Min	Off	Off	Off	Off					
Dual Entry				No	Yes	No	Yes	No	Yes	No	Yes					
Walk (Walk), s					0.0		0.0		0.0		0.0					
Pedestrian Clearance Time (PC), s					0.0		0.0		0.0		0.0					
Multimodal Information				EB			WB			NB			SB			
85th % Speed / Rest in Walk / Corner Radius				0	No	25	0	No	25	0	No	25	0	No	25	
Walkway / Crosswalk Width / Length, ft				9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0	
Street Width / Island / Curb				0	0	No	0	0	No	0	0	No	0	0	No	
Width Outside / Bike Lane / Shoulder, ft				12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	
Pedestrian Signal / Occupied Parking				No	0.50	No	0.50	No	0.50	No	0.50	No	0.50			

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1> 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM Pre-Development							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	75	161	6	142	183	110	7	462	162	94	241	2

Signal Information													
Cycle, s	69.5	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.5	1.6	9.2	0.9	5.3	21.3			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

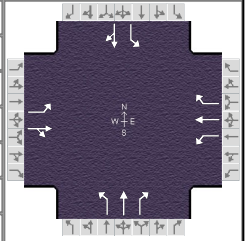
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	3.0	1.1	3.0	1.1	4.0
Phase Duration, s	12.2	16.0	13.8	17.6	7.0	27.3	12.4	32.6
Change Period, (Y+R _c), s	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	4.5	8.3	6.9	8.7	2.2	19.1	4.5	8.9
Green Extension Time (g _e), s	0.1	0.8	0.2	0.8	0.0	1.8	0.1	1.8
Phase Call Probability	0.78	1.00	0.94	1.00	0.13	1.00	0.85	1.00
Max Out Probability	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	79	176		149	193	116	7	486	171	99	256	
Adjusted Saturation Flow Rate (s), veh/h/ln	1781	1858		1781	1870	1585	1781	1870	1585	1781	1868	
Queue Service Time (g _s), s	2.5	6.3		4.9	6.7	4.6	0.2	17.1	5.9	2.5	6.9	
Cycle Queue Clearance Time (g _c), s	2.5	6.3		4.9	6.7	4.6	0.2	17.1	5.9	2.5	6.9	
Green Ratio (g/C)	0.21	0.13		0.23	0.16	0.16	0.31	0.30	0.30	0.40	0.38	
Capacity (c), veh/h	281	245		326	291	247	410	563	477	308	706	
Volume-to-Capacity Ratio (X)	0.281	0.717		0.458	0.661	0.469	0.018	0.864	0.357	0.322	0.362	
Back of Queue (Q), ft/ln (95 th percentile)	46.4	125.8		89.1	133.2	76.8	3.4	290.3	91.5	41.2	121.6	
Back of Queue (Q), veh/ln (95 th percentile)	1.8	5.0		3.5	5.2	3.0	0.1	11.4	3.6	1.6	4.8	
Queue Storage Ratio (RQ) (95 th percentile)	0.27	0.00		0.26	0.00	0.00	0.01	0.00	0.42	0.00	0.00	
Uniform Delay (d ₁), s/veh	23.0	28.9		22.9	27.6	26.7	16.5	22.9	19.0	15.8	15.6	
Incremental Delay (d ₂), s/veh	0.2	1.5		0.4	1.0	0.5	0.0	1.6	0.2	0.2	0.1	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	23.2	30.4		23.3	28.6	27.3	16.5	24.5	19.2	16.1	15.7	
Level of Service (LOS)	C	C		C	C	C	B	C	B	B	B	
Approach Delay, s/veh / LOS	28.2	C		26.5	C		23.1	C		15.8	B	
Intersection Delay, s/veh / LOS	23.2						C					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1> 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM Pre-Development							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	75	161	6	142	183	110	7	462	162	94	241	2

Signal Information													
Cycle, s	69.5	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.5	1.6	9.2	0.9	5.3	21.3			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

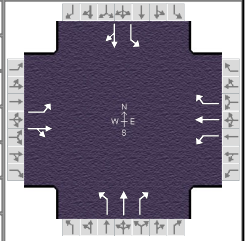
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	0.984	0.984	1.000	0.984	0.984	0.984	0.984	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.994	0.994		0.000	0.847		0.000	0.847		0.999	0.999
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	1781	1792	67	1781	1870	1585	1781	1870	1585	1781	1852	15
Proportion of Vehicles Arriving on Green (P)	0.08	0.13	0.13	0.10	0.16	0.16	0.01	0.30	0.30	0.09	0.38	0.38
Incremental Delay Factor (k)	0.04	0.04		0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Green Ratio (g/C)	0.21	0.13	0.23	0.16	0.31	0.30	0.40	0.38
Permitted Saturation Flow Rate (s_p), veh/h/ln	1190	0	1209	0	1124	0	909	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	9.2	0.0	9.2	0.0	20.9	0.0	21.9	0.0
Permitted Service Time (g_u), s	2.1	0.0	2.9	0.0	17.5	0.0	3.9	0.0
Permitted Queue Service Time (g_{ps}), s	0.5		0.9		0.0		2.2	
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

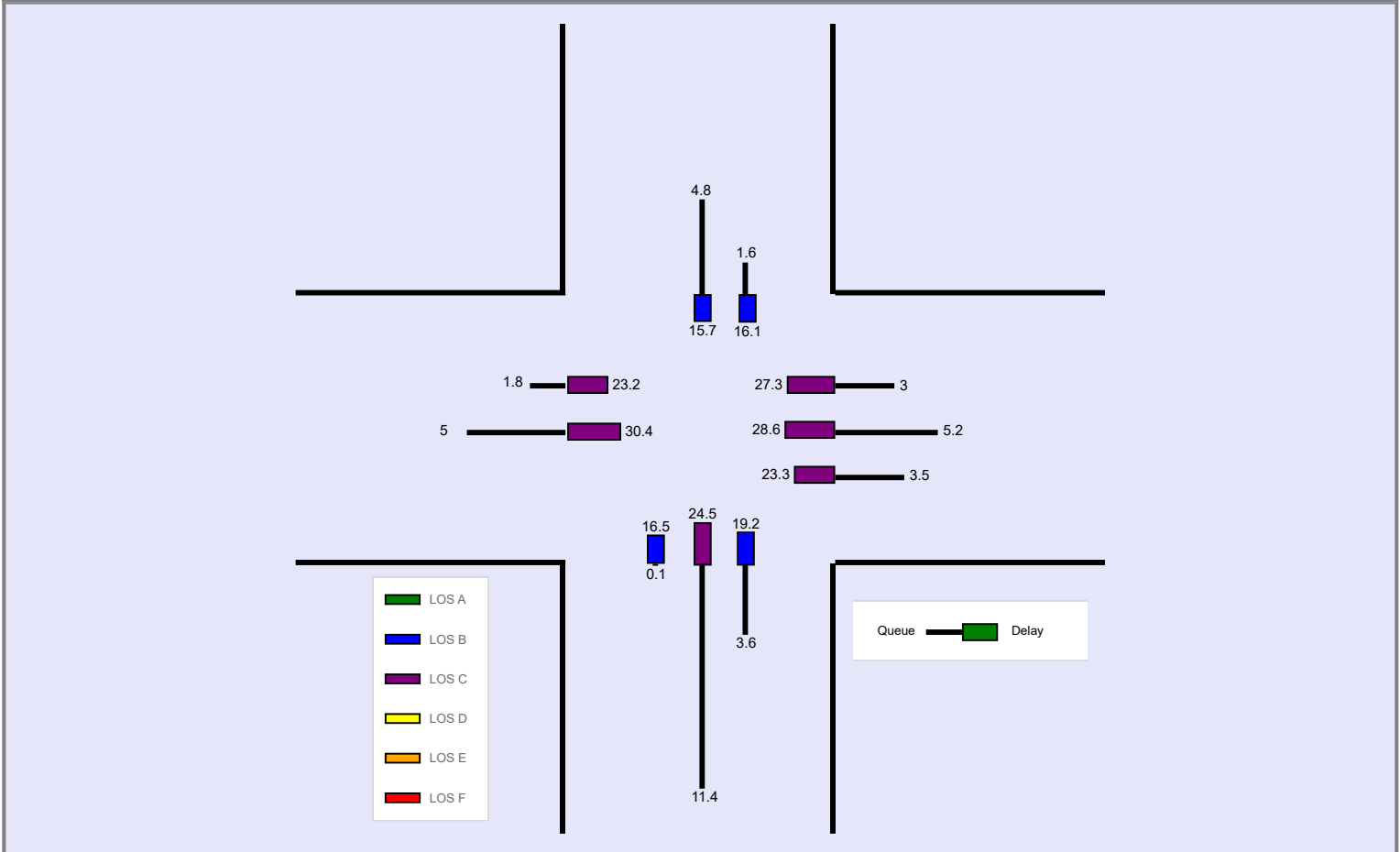
General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2024		Analysis Period	1 > 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM Pre-Development							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	75	161	6	142	183	110	7	462	162	94	241	2

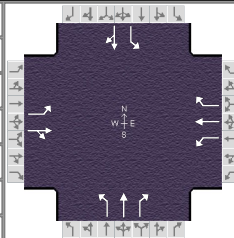
Signal Information				Phase Diagrams									
Cycle, s	69.5	Reference Phase	2										
Offset, s	0	Reference Point	Begin	Green	5.5	1.6	9.2	0.9	5.3	21.3			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)	46.4	125.8		89.1	133.2	76.8	3.4	290.3	91.5	41.2	121.6	
Back of Queue (Q), veh/ln (95 th percentile)	1.8	5.0		3.5	5.2	3.0	0.1	11.4	3.6	1.6	4.8	
Queue Storage Ratio (RQ) (95 th percentile)	0.27	0.00		0.26	0.00	0.00	0.01	0.00	0.42	0.00	0.00	
Control Delay (d), s/veh	23.2	30.4		23.3	28.6	27.3	16.5	24.5	19.2	16.1	15.7	
Level of Service (LOS)	C	C		C	C	C	B	C	B	B	B	
Approach Delay, s/veh / LOS	28.2		C	26.5		C	23.1		C	15.8		B
Intersection Delay, s/veh / LOS	23.2						C					



HCS7 Signalized Intersection Input Data

General Information				Intersection Information			
Agency	MEP			Duration, h	0.250		
Analyst	MEP			Analysis Date	7/24/2023		
Jurisdiction				Area Type	Other		
Urban Street	Cardinal Road/NE Dixie...			Time Period	4:00-6:00 PM		
Intersection	NE Dixie Hwy/Savannah...			PHF	0.95		
Project Description	PM POST			Analysis Year	2030		
				Analysis Period	1> 16:00		
				File Name	Cardinal and Dixie PM.xus		



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	78	163	23	148	187	110	18	466	167	95	255	5

Signal Information											
Cycle, s	71.9	Reference Phase	2								
Offset, s	0	Reference Point	Begin	Green	5.7	1.9	10.3	2.2	4.1	22.1	
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0	
Force Mode	Fixed	Simult. Gap N/S	On	Red	3.3	0.0	3.4	2.4	0.0	2.0	

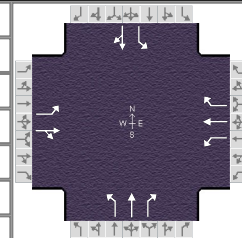
Traffic Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	78	163	23	148	187	110	18	466	167	95	255	5
Initial Queue (Q _b), veh/h	0	0	0	0	0	0	0	0	0	0	0	0
Base Saturation Flow Rate (s ₀), veh/h	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Parking (N _m), man/h		None			None			None			None	
Heavy Vehicles (P _{HV}), %	2	2		2	2	2	2	2	2	2	2	
Ped / Bike / RTOR, /h	0	0	0	0	0	0	0	0	0	0	0	0
Buses (N _b), buses/h	0	0	0	0	0	0	0	0	0	0	0	0
Arrival Type (AT)	3	3	3	3	3	3	3	3	3	3	3	3
Upstream Filtering (I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lane Width (W), ft	12.0	12.0		12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	
Turn Bay Length, ft	170	0		340	0	0	240	0	220	0	0	
Grade (P _g), %		0			0			0			0	
Speed Limit, mi/h	35	35	35	35	35	35	35	35	35	35	35	35

Phase Information	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Maximum Green (G _{max}) or Phase Split, s	15.0	25.0	20.0	25.0	15.0	45.0	15.0	45.0
Yellow Change Interval (Y), s	3.4	3.4	3.7	4.0	3.7	4.0	4.0	4.4
Red Clearance Interval (R _c), s	3.3	3.4	3.4	2.0	2.4	2.0	2.4	2.0
Minimum Green (G _{min}), s	7	7	7	7	7	10	7	10
Start-Up Lost Time (lt), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Extension of Effective Green (e), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Passage (PT), s	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Recall Mode	Off	Min	Off	Min	Off	Off	Off	Off
Dual Entry	No	Yes	No	Yes	No	Yes	No	Yes
Walk (Walk), s		0.0		0.0		0.0		0.0
Pedestrian Clearance Time (PC), s		0.0		0.0		0.0		0.0

Multimodal Information	EB			WB			NB			SB		
85th % Speed / Rest in Walk / Corner Radius	0	No	25	0	No	25	0	No	25	0	No	25
Walkway / Crosswalk Width / Length, ft	9.0	12	0	9.0	12	0	9.0	12	0	9.0	12	0
Street Width / Island / Curb	0	0	No	0	0	No	0	0	No	0	0	No
Width Outside / Bike Lane / Shoulder, ft	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0	12	5.0	2.0
Pedestrian Signal / Occupied Parking	No	0.50		No	0.50		No	0.50		No	0.50	

HCS7 Signalized Intersection Results Summary

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1> 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	78	163	23	148	187	110	18	466	167	95	255	5

Signal Information													
Cycle, s	71.9	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.7	1.9	10.3	2.2	4.1	22.1			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

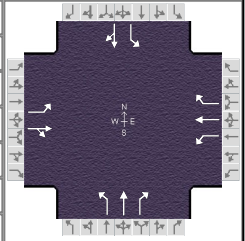
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	4.0	1.1	3.0	1.1	3.0	1.1	4.0
Phase Duration, s	12.4	17.1	14.2	19.0	8.3	28.1	12.5	32.3
Change Period, (Y+R _c), s	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	4.7	9.4	7.2	9.0	2.5	19.8	4.6	9.9
Green Extension Time (g _e), s	0.1	0.9	0.2	0.9	0.0	1.8	0.1	1.9
Phase Call Probability	0.81	1.00	0.96	1.00	0.32	1.00	0.86	1.00
Max Out Probability	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	82	196		156	197	116	19	491	176	100	274	
Adjusted Saturation Flow Rate (s), veh/h/ln	1781	1830		1781	1870	1585	1781	1870	1585	1781	1864	
Queue Service Time (g _s), s	2.7	7.4		5.2	7.0	4.7	0.5	17.8	6.3	2.6	7.9	
Cycle Queue Clearance Time (g _c), s	2.7	7.4		5.2	7.0	4.7	0.5	17.8	6.3	2.6	7.9	
Green Ratio (g/C)	0.22	0.14		0.24	0.17	0.17	0.33	0.30	0.30	0.39	0.36	
Capacity (c), veh/h	293	262		325	317	268	402	566	479	300	671	
Volume-to-Capacity Ratio (X)	0.281	0.747		0.479	0.622	0.432	0.047	0.867	0.367	0.333	0.408	
Back of Queue (Q), ft/ln (95 th percentile)	49.5	145.8		95.3	138.7	78.4	9	303.8	98.8	45.1	143.4	
Back of Queue (Q), veh/ln (95 th percentile)	1.9	5.7		3.8	5.5	3.1	0.4	12.0	3.9	1.8	5.6	
Queue Storage Ratio (RQ) (95 th percentile)	0.29	0.00		0.28	0.00	0.00	0.04	0.00	0.45	0.00	0.00	
Uniform Delay (d ₁), s/veh	23.2	29.6		23.1	27.8	26.8	16.4	23.7	19.7	16.8	17.3	
Incremental Delay (d ₂), s/veh	0.2	1.6		0.4	0.7	0.4	0.0	1.6	0.2	0.2	0.1	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	23.4	31.2		23.5	28.5	27.2	16.4	25.4	19.9	17.1	17.4	
Level of Service (LOS)	C	C		C	C	C	B	C	B	B	B	
Approach Delay, s/veh / LOS	28.9	C		26.5	C		23.7	C		17.3	B	
Intersection Delay, s/veh / LOS	23.9						C					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS				
Bicycle LOS Score / LOS				

HCS7 Signalized Intersection Intermediate Values

General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1> 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	78	163	23	148	187	110	18	466	167	95	255	5

Signal Information													
Cycle, s	71.9	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.7	1.9	10.3	2.2	4.1	22.1			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

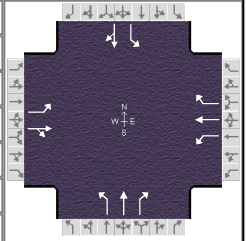
Saturation Flow / Delay	L	T	R	L	T	R	L	T	R	L	T	R
Lane Width Adjustment Factor (f_w)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Heavy Vehicles and Grade Factor (f_{HVg})	0.984	0.984	1.000	0.984	0.984	0.984	0.984	0.984	0.984	0.984	0.984	1.000
Parking Activity Adjustment Factor (f_p)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Bus Blockage Adjustment Factor (f_{bb})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Area Type Adjustment Factor (f_a)	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Lane Utilization Adjustment Factor (f_{LU})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Left-Turn Adjustment Factor (f_{LT})	0.952	0.000		0.952	0.000		0.952	0.000		0.952	0.000	
Right-Turn Adjustment Factor (f_{RT})		0.978	0.978		0.000	0.847		0.000	0.847		0.997	0.997
Left-Turn Pedestrian Adjustment Factor (f_{LPB})	1.000			1.000			1.000			1.000		
Right-Turn Ped-Bike Adjustment Factor (f_{RPB})			1.000			1.000			1.000			1.000
Work Zone Adjustment Factor (f_{wz})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
DDI Factor (f_{DDI})	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000	1.000
Movement Saturation Flow Rate (s), veh/h	1781	1603	226	1781	1870	1585	1781	1870	1585	1781	1828	36
Proportion of Vehicles Arriving on Green (P)	0.08	0.14	0.14	0.10	0.17	0.17	0.03	0.30	0.30	0.08	0.36	0.36
Incremental Delay Factor (k)	0.04	0.04		0.04	0.04	0.04	0.04	0.04	0.04	0.04	0.04	

Signal Timing / Movement Groups	EBL	EBT/R	WBL	WBT/R	NBL	NBT/R	SBL	SBT/R
Lost Time (t_L)	6.7	6.8	7.1	6.8	6.1	6.4	6.4	6.4
Green Ratio (g/C)	0.22	0.14	0.24	0.17	0.33	0.30	0.39	0.36
Permitted Saturation Flow Rate (s_p), veh/h/ln	1186	0	1187	0	1106	0	906	0
Shared Saturation Flow Rate (s_{sh}), veh/h/ln								
Permitted Effective Green Time (g_p), s	10.3	0.0	10.3	0.0	21.8	0.0	21.8	0.0
Permitted Service Time (g_u), s	3.2	0.0	2.9	0.0	16.1	0.0	4.0	0.0
Permitted Queue Service Time (g_{ps}), s	0.5		1.1		0.1		2.2	
Time to First Blockage (g_t), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Queue Service Time Before Blockage (g_{ts}), s								
Protected Right Saturation Flow (s_R), veh/h/ln				0		0		
Protected Right Effective Green Time (g_R), s				0.0		0.0		

Multimodal	EB	WB	NB	SB
Pedestrian F_w / F_v				
Pedestrian F_s / F_{delay}				
Pedestrian M_{corner} / M_{cw}				
Bicycle c_b / d_b				
Bicycle F_w / F_v				

HCS7 Signalized Intersection Results Graphical Summary

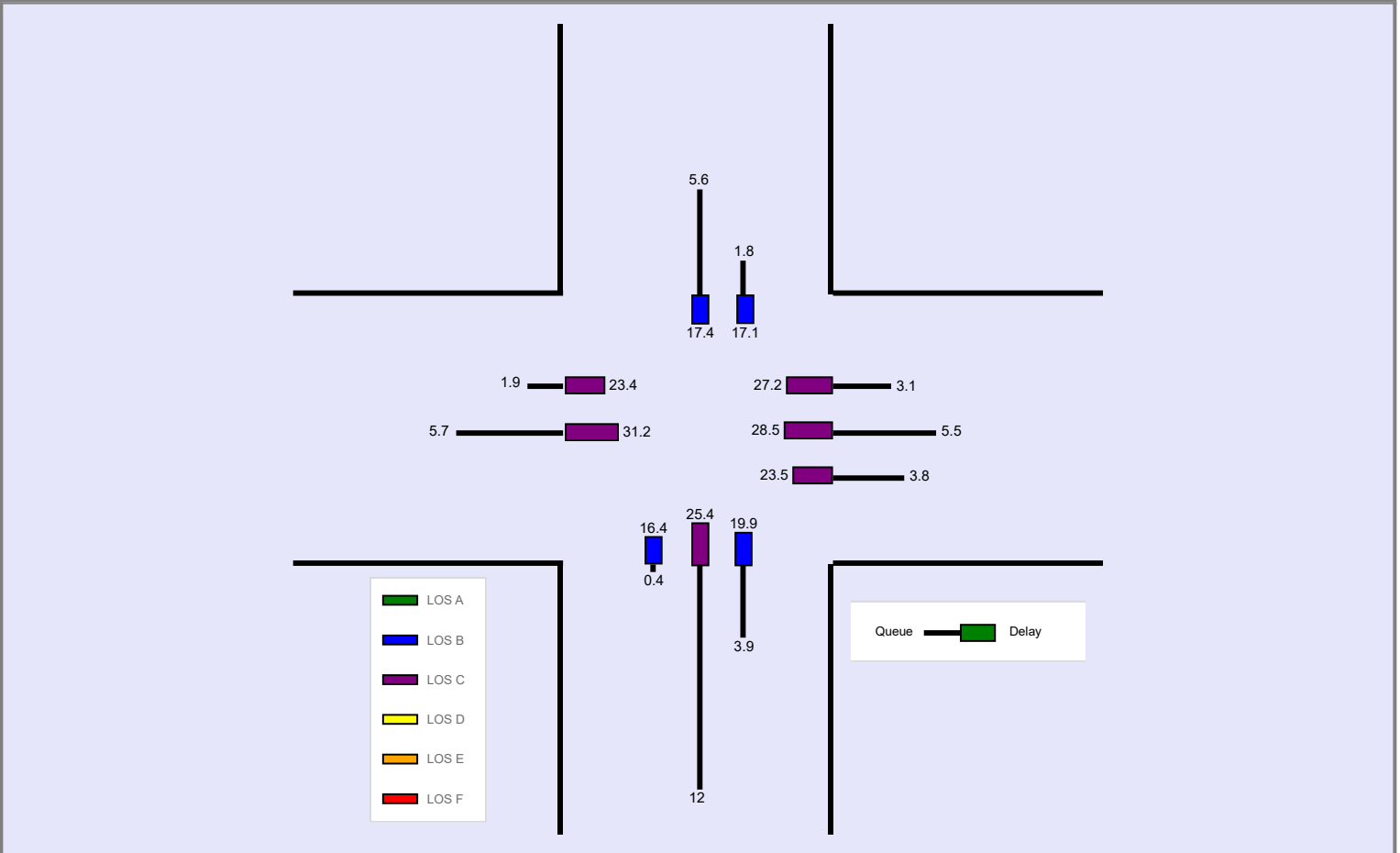
General Information				Intersection Information				
Agency	MEP			Duration, h	0.250			
Analyst	MEP		Analysis Date	7/24/2023		Area Type	Other	
Jurisdiction				Time Period	4:00-6:00 PM		PHF	0.95
Urban Street	Cardinal Road/NE Dixie...		Analysis Year	2030		Analysis Period	1 > 16:00	
Intersection	NE Dixie Hwy/Savannah...		File Name	Cardinal and Dixie PM.xus				
Project Description	PM POST							



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	78	163	23	148	187	110	18	466	167	95	255	5

Signal Information													
Cycle, s	71.9	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.7	1.9	10.3	2.2	4.1	22.1			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	3.4	0.0	3.4	3.7	0.0	4.0			
				Red	3.3	0.0	3.4	2.4	0.0	2.0			

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Back of Queue (Q), ft/ln (95 th percentile)	49.5	145.8		95.3	138.7	78.4	9	303.8	98.8	45.1	143.4	
Back of Queue (Q), veh/ln (95 th percentile)	1.9	5.7		3.8	5.5	3.1	0.4	12.0	3.9	1.8	5.6	
Queue Storage Ratio (RQ) (95 th percentile)	0.29	0.00		0.28	0.00	0.00	0.04	0.00	0.45	0.00	0.00	
Control Delay (d), s/veh	23.4	31.2		23.5	28.5	27.2	16.4	25.4	19.9	17.1	17.4	
Level of Service (LOS)	C	C		C	C	C	B	C	B	B	B	
Approach Delay, s/veh / LOS	28.9		C	26.5		C	23.7		C	17.3		B
Intersection Delay, s/veh / LOS	23.9						C					



Avonlea 10-11-13-14-15

AM PEAK HOUR TURNING MOVEMENTS

EXHIBIT 13

Baker Road & Cardinal Road

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
7:00 AM	7:15 AM	2	0	36	0	8	0	14	3	0	0	9	7
7:15 AM	7:30 AM	3	2	22	0	9	0	12	4	0	0	6	10
7:30 AM	7:45 AM	1	1	40	0	8	1	27	3	0	0	8	11
7:45 AM	8:00 AM	3	1	44	0	13	0	24	2	0	0	13	14
8:00 AM	8:15 AM	3	0	36	0	15	0	16	4	0	0	5	18
8:15 AM	8:30 AM	6	0	38	0	13	0	24	6	0	0	14	14
8:30 AM	8:45 AM	10	1	28	0	15	0	26	4	0	0	11	8
8:45 AM	9:00 AM	10	0	44	0	17	1	30	4	0	0	9	6
9:00 AM	9:15 AM												
9:15 AM	9:30 AM												
9:30 AM	9:45 AM												
9:45 AM	10:00 AM												
		38	5	288	0	98	2	173	30	0	0	75	88

Peak Hour Traffic Volume

8:00 AM	9:00 AM	29	1	146	0	60	1	96	18	0	0	39	46
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Count Taken:	6/29/2023	PHF	0.90	0.92
Buildout year:	2030			
Growth Rate:	1.3%			
PSCF	1.22			

	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/29/2023	29	0	146	0	60	1	96	18	0	0	39	46
Peak Season Factor	6	0	32	0	13	0	21	4	0	0	9	10
Adjusted Volumes	35	0	178	0	73	1	117	22	0	0	48	56
	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
Growth 1.3%	4	0	19	0	8	0	13	2	0	0	5	6
2030 Volumes	39	0	197	0	81	1	130	24	0	0	53	62
Pre Dev	39	0	197	0	81	1	130	24	0	0	53	62
Project Traffic	0	0	11	0	0	0	13	0	0	0	0	0
Post Dev	39	0	208	0	81	1	143	24	0	0	53	62

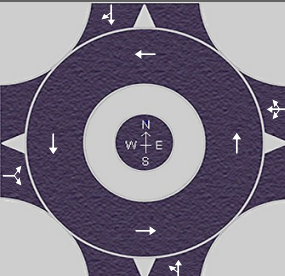
Project Traffic Assignment	In	Out	Out	Out	In	In
	0%	4%	0%	7%	4%	10%
	0%	0%	7%	10%	7%	10%

Existing	35	0	178	0	73	1	117	22	0	0	48	56
Pre-Development	39	0	197	0	81	1	130	24	0	0	53	62
Post-Development	39	0	208	0	81	1	143	24	0	0	53	62

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Baker & Cardinal
Agency or Co.	MEP		E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Cardinal Road
Analysis Year	2024		Analysis Time Period (hrs)	0.25
Time Analyzed	Existing AM Peak Hour		Peak Hour Factor	0.92
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment			LR				LTR				LT				TR	
Volume (V), veh/h	0	35		178	0	0	73	1	0	117	22		0		48	56
Percent Heavy Vehicles, %	3	3		3	3	3	3	3	3	3	3		3		3	3
Flow Rate (v _{PCE}), pc/h	0	39		199	0	0	82	1	0	131	25		0		54	63
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		238			83			156			117	
Entry Volume, veh/h		231			81			151			114	
Circulating Flow (v _c), pc/h	54			195			39			213		
Exiting Flow (v _{ex}), pc/h	0			276			65			253		
Capacity (C _{PCE}), pc/h		1306			1131			1326			1111	
Capacity (c), veh/h		1268			1098			1288			1078	
v/c Ratio (x)		0.18			0.07			0.12			0.11	

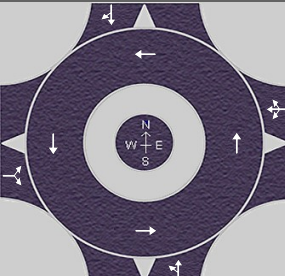
Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		4.4			3.9			3.8			4.3	
Lane LOS		A			A			A			A	
95% Queue, veh		0.7			0.2			0.4			0.4	
Approach Delay, s/veh	4.4			3.9			3.8			4.3		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	4.1						A					

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Baker & Cardinal
Agency or Co.	MEP		E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Cardinal Road
Analysis Year	2030		Analysis Time Period (hrs)	0.25
Time Analyzed	Pre-dev AM Peak Hour		Peak Hour Factor	0.92
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LR				LTR				LT				TR			
Volume (V), veh/h	0	39		197	0	0	81	1	0	130	24		0		53	62
Percent Heavy Vehicles, %	3	3		3	3	3	3	3	3	3	3		3		3	3
Flow Rate (v _{PCE}), pc/h	0	44		221	0	0	91	1	0	146	27		0		59	69
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		265			92			173			128	
Entry Volume, veh/h		257			89			168			124	
Circulating Flow (v _c), pc/h	59			217			44			237		
Exiting Flow (v _{ex}), pc/h	0			306			72			280		
Capacity (C _{PCE}), pc/h		1299			1106			1319			1084	
Capacity (c), veh/h		1262			1074			1281			1052	
v/c Ratio (x)		0.20			0.08			0.13			0.12	

Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		4.6			4.1			3.9			4.5	
Lane LOS		A			A			A			A	
95% Queue, veh		0.8			0.3			0.5			0.4	
Approach Delay, s/veh	4.6			4.1			3.9			4.5		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	4.3						A					

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Baker & Cardinal
Agency or Co.	MEP		E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Cardinal Road
Analysis Year	2030		Analysis Time Period (hrs)	0.25
Time Analyzed	Post-dev AM Peak Hour		Peak Hour Factor	0.92
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment			LR				LTR				LT				TR	
Volume (V), veh/h	0	39		208	0	0	81	1	0	143	24		0		53	62
Percent Heavy Vehicles, %	3	3		3	3	3	3	3	3	3	3		3		3	3
Flow Rate (v _{PCE}), pc/h	0	44		233	0	0	91	1	0	160	27		0		59	69
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		277			92			187			128	
Entry Volume, veh/h		269			89			182			124	
Circulating Flow (v _c), pc/h	59			231			44			251		
Exiting Flow (v _{ex}), pc/h	0			320			72			292		
Capacity (C _{PCE}), pc/h		1299			1090			1319			1068	
Capacity (c), veh/h		1262			1059			1281			1037	
v/c Ratio (x)		0.21			0.08			0.14			0.12	

Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		4.7			4.1			4.0			4.5	
Lane LOS		A			A			A			A	
95% Queue, veh		0.8			0.3			0.5			0.4	
Approach Delay, s/veh	4.7			4.1			4.0			4.5		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	4.4						A					

Avonlea 10-11-13-14-15
 PM PEAK HOUR TURNING MOVEMENTS
 EXHIBIT 14
 Baker Road & Cardinal Road

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
4:00 PM	4:15 PM	14	0	43	0	18	1	35	6	0	0	3	5
4:15 PM	4:30 PM	9	0	39	0	13	2	34	4	0	0	1	4
4:30 PM	4:45 PM	11	0	24	0	15	1	36	7	0	0	4	9
4:45 PM	5:00 PM	13	0	43	1	8	3	26	13	0	0	8	6
5:00 PM	5:15 PM	10	0	39	0	22	2	36	6	0	0	3	7
5:15 PM	5:30 PM	18	1	42	0	13	1	28	8	0	0	5	13
5:30 PM	5:45 PM	13	2	25	1	3	2	25	4	0	0	4	10
5:45 PM	6:00 PM	13	0	28	1	6	1	25	7	0	0	4	7
6:00 PM	6:15 PM												
6:15 PM	6:30 PM												
6:30 PM	6:45 PM												
6:45 PM	7:00 PM												
		101	3	283	3	98	13	245	55	0	0	32	61

Peak Hour Traffic Volume

4:30 PM	5:30 PM	52	1	148	1	58	7	126	34	0	0	20	35
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Count Taken: 6/29/2023 PHF 0.93
 Buildout year: 2030
 Growth Rate: 1.3%
 PSCF 1.22

	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/29/2023	52	0	148	1	58	7	126	34	0	0	20	35
Peak Season Factor	11	0	33	0	13	2	28	7	0	0	4	8
Adjusted Volumes	63	0	181	1	71	9	154	41	0	0	24	43
Growth 1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
2030 Volumes	70	0	201	1	79	10	171	45	0	0	27	48
Pre Dev	70	0	201	1	79	10	171	45	0	0	27	48
Project Traffic	0	0	26	0	0	0	9	0	0	0	0	0
Post Dev	70	0	227	1	79	10	180	45	0	0	27	48

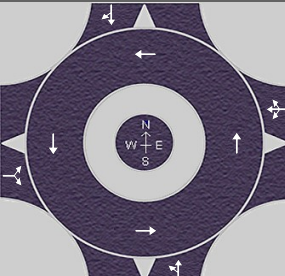
Project Traffic Assignment	In	Out	Out	Out	In	In
	0%	4%	0%	7%	4%	10%
					0%	0%
					7%	10%
					0%	0%

Existing	63	0	181	1	71	9	154	41	0	0	24	43
Pre-Development	70	0	201	1	79	10	171	45	0	0	27	48
Post-Development	70	0	227	1	79	10	180	45	0	0	27	48

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Baker & Cardinal Rd
Agency or Co.	MEP		E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Cardinal Road
Analysis Year	2024		Analysis Time Period (hrs)	0.25
Time Analyzed	Existing PM Peak Hour		Peak Hour Factor	0.93
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment			LR				LTR				LT				TR	
Volume (V), veh/h	0	63		181	0	1	71	9	0	154	41		0		24	43
Percent Heavy Vehicles, %	3	3		3	3	3	3	3	3	3	3		3		3	3
Flow Rate (v _{PCE}), pc/h	0	70		200	0	1	79	10	0	171	45		0		27	48
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		270			90			216			75	
Entry Volume, veh/h		262			87			210			73	
Circulating Flow (v _c), pc/h	28			286			70			251		
Exiting Flow (v _{ex}), pc/h	0			298			125			228		
Capacity (C _{PCE}), pc/h		1341			1031			1285			1068	
Capacity (c), veh/h		1302			1001			1247			1037	
v/c Ratio (x)		0.20			0.09			0.17			0.07	

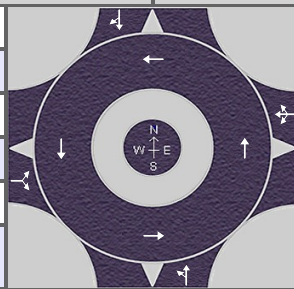
Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		4.5			4.4			4.3			4.1	
Lane LOS		A			A			A			A	
95% Queue, veh		0.8			0.3			0.6			0.2	
Approach Delay, s/veh	4.5			4.4			4.3			4.1		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	4.4						A					

HCS7 Roundabouts Report

General Information

Analyst	MEP
Agency or Co.	MEP
Date Performed	4/24/2024
Analysis Year	2030
Time Analyzed	Pre-Dev PM Peak Hour
Project Description	Avonlea 10-11-13-14-15



Site Information

Intersection	Baker & Cardinal Rd
E/W Street Name	Baker Road
N/S Street Name	Cardinal Road
Analysis Time Period (hrs)	0.25
Peak Hour Factor	0.93
Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment			LR				LTR				LT					TR
Volume (V), veh/h	0	70		201	0	1	79	10	0	171	45		0		27	48
Percent Heavy Vehicles, %	3	3		3	3	3	3	3	3	3	3		3		3	3
Flow Rate (v _{PCE}), pc/h	0	78		223	0	1	87	11	0	189	50		0		30	53
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		301			99			239			83	
Entry Volume, veh/h		292			96			232			81	
Circulating Flow (v _c), pc/h	31			317			78			277		
Exiting Flow (v _{ex}), pc/h	0			329			139			254		
Capacity (C _{PCE}), pc/h		1337			999			1274			1040	
Capacity (c), veh/h		1298			970			1237			1010	
v/c Ratio (x)		0.23			0.10			0.19			0.08	

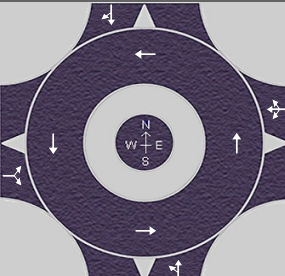
Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		4.7			4.6			4.5			4.3	
Lane LOS		A			A			A			A	
95% Queue, veh		0.9			0.3			0.7			0.3	
Approach Delay, s/veh	4.7			4.6			4.5			4.3		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	4.6						A					

HCS7 Roundabouts Report

General Information

Site Information

Analyst	MEP		Intersection	Baker & Cardinal Rd
Agency or Co.	MEP		E/W Street Name	Baker Road
Date Performed	4/24/2024		N/S Street Name	Cardinal Road
Analysis Year	2030		Analysis Time Period (hrs)	0.25
Time Analyzed	Post-Dev PM Peak Hour		Peak Hour Factor	0.93
Project Description	Avonlea 10-11-13-14-15		Jurisdiction	

Volume Adjustments and Site Characteristics

Approach	EB				WB				NB				SB			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Number of Lanes (N)	0	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0
Lane Assignment	LR				LTR				LT				TR			
Volume (V), veh/h	0	70		227	0	1	79	10	0	180	45		0		27	48
Percent Heavy Vehicles, %	3	3		3	3	3	3	3	3	3	3		3		3	3
Flow Rate (v _{PCE}), pc/h	0	78		251	0	1	87	11	0	199	50		0		30	53
Right-Turn Bypass	None				None				None				None			
Conflicting Lanes	1				1				1				1			
Pedestrians Crossing, p/h	0				0				0				0			

Critical and Follow-Up Headway Adjustment

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Critical Headway (s)		4.9763			4.9763			4.9763			4.9763	
Follow-Up Headway (s)		2.6087			2.6087			2.6087			2.6087	

Flow Computations, Capacity and v/c Ratios

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Entry Flow (v _e), pc/h		329			99			249			83	
Entry Volume, veh/h		319			96			242			81	
Circulating Flow (v _c), pc/h	31			327			78			287		
Exiting Flow (v _{ex}), pc/h	0			339			139			282		
Capacity (C _{PCE}), pc/h		1337			989			1274			1030	
Capacity (c), veh/h		1298			960			1237			1000	
v/c Ratio (x)		0.25			0.10			0.20			0.08	

Delay and Level of Service

Approach	EB			WB			NB			SB		
	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass	Left	Right	Bypass
Lane Control Delay (d), s/veh		4.9			4.7			4.6			4.3	
Lane LOS		A			A			A			A	
95% Queue, veh		1.0			0.3			0.7			0.3	
Approach Delay, s/veh	4.9			4.7			4.6			4.3		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	4.7						A					

Avonlea 10-11-13-14-15
 AM PEAK HOUR TURNING MOVEMENTS
 EXHIBIT 15
 Dixie Highway & DW 5

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
7:00 AM	7:15 AM	0	43	0	0	86	0	0	0	0	0	0	0
7:15 AM	7:30 AM	0	39	0	0	105	0	0	0	0	0	0	0
7:30 AM	7:45 AM	0	50	0	0	107	0	0	0	0	0	0	0
7:45 AM	8:00 AM	0	67	0	0	112	0	0	0	0	0	0	0
8:00 AM	8:15 AM	0	79	0	0	112	0	0	0	0	0	0	0
8:15 AM	8:30 AM	0	76	0	0	120	0	0	0	0	0	0	0
8:30 AM	8:45 AM	0	52	0	0	97	0	0	0	0	0	0	0
8:45 AM	9:00 AM	0	65	0	0	73	0	0	0	0	0	0	0
9:00 AM	9:15 AM												
9:15 AM	9:30 AM												
9:30 AM	9:45 AM												
9:45 AM	10:00 AM												
		0	471	0	0	812	0	0	0	0	0	0	0

Peak Hour Traffic Volume

7:30 AM	8:30 AM	0	272	0	0	451	0	0	0	0	0	0	0
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Count Taken: 6/29/2023 PHF 0.92
 Buildout year: 2030
 Growth Rate: 1.3%
 PSCF 1.22

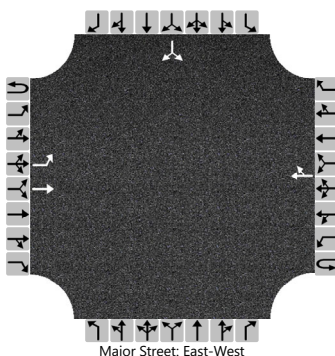
	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/29/2023	0	272	0	0	451	0	0	0	0	0	0	0
Peak Season Factor	0	60	0	0	99	0	0	0	0	0	0	0
Adjusted Volumes	0	332	0	0	550	0	0	0	0	0	0	0
	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
Growth 1.3%	0	36	0	0	60	0	0	0	0	0	0	0
2030 Volumes	0	368	0	0	610	0	0	0	0	0	0	0
Pre Dev	0	368	0	0	610	0	0	0	0	0	0	0
Project Traffic	8	0	0	0	0	5	0	0	0	14	0	14
Post Dev	8	368	0	0	0	5	0	0	0	14	0	14

Project Traffic Assignment	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
Existing	0	332	0	0	550	0	0	0	0	0	0	0
Pre-Development	0	368	0	0	610	0	0	0	0	0	0	0
Post-Development	8	368	0	0	0	5	0	0	0	14	0	14

HCS Two-Way Stop-Control Report

General Information				Site Information			
Analyst	MEP			Intersection	Dixie Hwy & DW 5		
Agency/Co.	MEP			Jurisdiction			
Date Performed	6/17/2025			East/West Street	Dixie Hwy		
Analysis Year	2030			North/South Street	DW 5		
Time Analyzed				Peak Hour Factor	0.92		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	2030 Post AM						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	1	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		L	T					TR							LR	
Volume (veh/h)		8	368				610	5						14		14
Percent Heavy Vehicles (%)		2												2		2
Proportion Time Blocked																
Percent Grade (%)														0		
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.12												6.42		6.22
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.22												3.52		3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)		9														30	
Capacity, c (veh/h)		921														314	
v/c Ratio		0.01														0.10	
95% Queue Length, Q ₉₅ (veh)		0.0														0.3	
95% Queue Length, Q ₉₅ (ft)		0.0														7.6	
Control Delay (s/veh)		8.9														17.7	
Level of Service (LOS)		A														C	
Approach Delay (s/veh)		0.2												17.7			
Approach LOS		A												C			

Avonlea 10-11-13-14-15
 PM PEAK HOUR TURNING MOVEMENTS
 EXHIBIT 16
 Dixie Highway & DW 5

		ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
4:00 PM	4:15 PM	0	94	0	0	80	0	0	0	0	0	0	0
4:15 PM	4:30 PM	0	95	0	0	73	0	0	0	0	0	0	0
4:30 PM	4:45 PM	0	74	0	0	64	0	0	0	0	0	0	0
4:45 PM	5:00 PM	0	102	0	0	84	0	0	0	0	0	0	0
5:00 PM	5:15 PM	0	85	0	0	96	0	0	0	0	0	0	0
5:15 PM	5:30 PM	0	136	0	0	63	0	0	0	0	0	0	0
5:30 PM	5:45 PM	0	89	0	0	61	0	0	0	0	0	0	0
5:45 PM	6:00 PM	0	78	0	0	77	0	0	0	0	0	0	0
6:00 PM	6:15 PM												
6:15 PM	6:30 PM												
6:30 PM	6:45 PM												
6:45 PM	7:00 PM												
		0	753	0	0	598	0	0	0	0	0	0	0

Peak Hour Traffic Volume

4:45 PM	5:45 PM	0	412	0	0	304	0	0	0	0	0	0	0
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Count Taken: 6/29/2023 PHF 0.90 0.92
 Buildout year: 2030
 Growth Rate: 1.3%
 PSCF 1.22

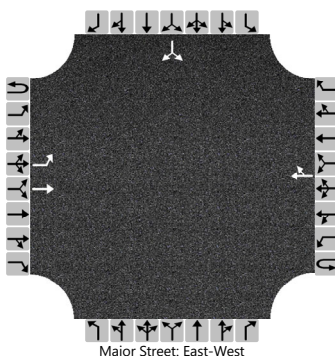
	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
6/28/2023	0	412	0	0	304	0	0	0	0	0	0	0
Peak Season Factor	0	91	0	0	67	0	0	0	0	0	0	0
Adjusted Volumes	0	503	0	0	371	0	0	0	0	0	0	0
	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%	1.3%
Growth 1.3%	0	55	0	0	40	0	0	0	0	0	0	0
2030 Volumes	0	558	0	0	411	0	0	0	0	0	0	0
Pre Dev	0	558	0	0	411	0	0	0	0	0	0	0
Project Traffic	25	0	0	0	0	14	0	0	0	8	0	9
Post Dev	25	558	0	0	411	14	0	0	0	8	0	9

Project Traffic Assignment	ebl	ebt	ebr	wbl	wbt	wbr	nbl	nbt	nbr	sbl	sbt	sbr
Existing	0	503	0	0	371	0	0	0	0	0	0	0
Pre-Development	0	558	0	0	411	0	0	0	0	0	0	0
Post-Development	25	558	0	0	411	14	0	0	0	8	0	9

HCS Two-Way Stop-Control Report

General Information				Site Information			
Analyst	MEP			Intersection	Dixie Hwy & DW 5		
Agency/Co.	MEP			Jurisdiction			
Date Performed	6/17/2025			East/West Street	Dixie Hwy		
Analysis Year	2030			North/South Street	DW 5		
Time Analyzed				Peak Hour Factor	0.92		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	2030 Post PM						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	1	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		L	T					TR							LR	
Volume (veh/h)		25	558				411	14						8		9
Percent Heavy Vehicles (%)		2												2		2
Proportion Time Blocked																
Percent Grade (%)														0		
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.12												6.42		6.22
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.22												3.52		3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)		27														18	
Capacity, c (veh/h)		1099														336	
v/c Ratio		0.02														0.05	
95% Queue Length, Q ₉₅ (veh)		0.1														0.2	
95% Queue Length, Q ₉₅ (ft)		2.5														5.1	
Control Delay (s/veh)		8.4														16.3	
Level of Service (LOS)		A														C	
Approach Delay (s/veh)		0.4												16.3			
Approach LOS		A												C			

Land Use: 230

Low-Rise Residential with Ground-Floor Commercial

Description

Low-rise residential with ground-floor commercial is a mixed-use multifamily housing building with two or three floors of residential living space and commercial space open to the public on the ground level. These facilities are typically found in dense multi-use urban and center city core settings. Multifamily housing (low-rise) (Land Use 220), mid-rise residential with ground floor commercial (Land Use 231), and high-rise residential with ground-floor commercial (Land Use 232) are related land uses.

Land Use Subcategory

The data included in this land use have been stratified into two subcategories: (1) sites with a commercial gross leasable area that ranges between 1,000 and 25,000 square feet (2) sites with a commercial gross leasable area that ranges between 25,000 and 65,000 square feet.

Additional Data

The trips displayed in the data plots represent the total trips that enter or exit the site. Any trips internal to the site between its residential and commercial components are not included.

The ITE *Trip Generation Handbook* (TGH) presents an alternative approach for estimating trips generated by a site that fits this land use. The TGH method involves the estimation of the internal trip-making at a mixed-use site.

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

Additional Data

The sites were surveyed in the 2010s in the District of Columbia and Oregon.

Source Numbers

901, 949, 950

Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 422

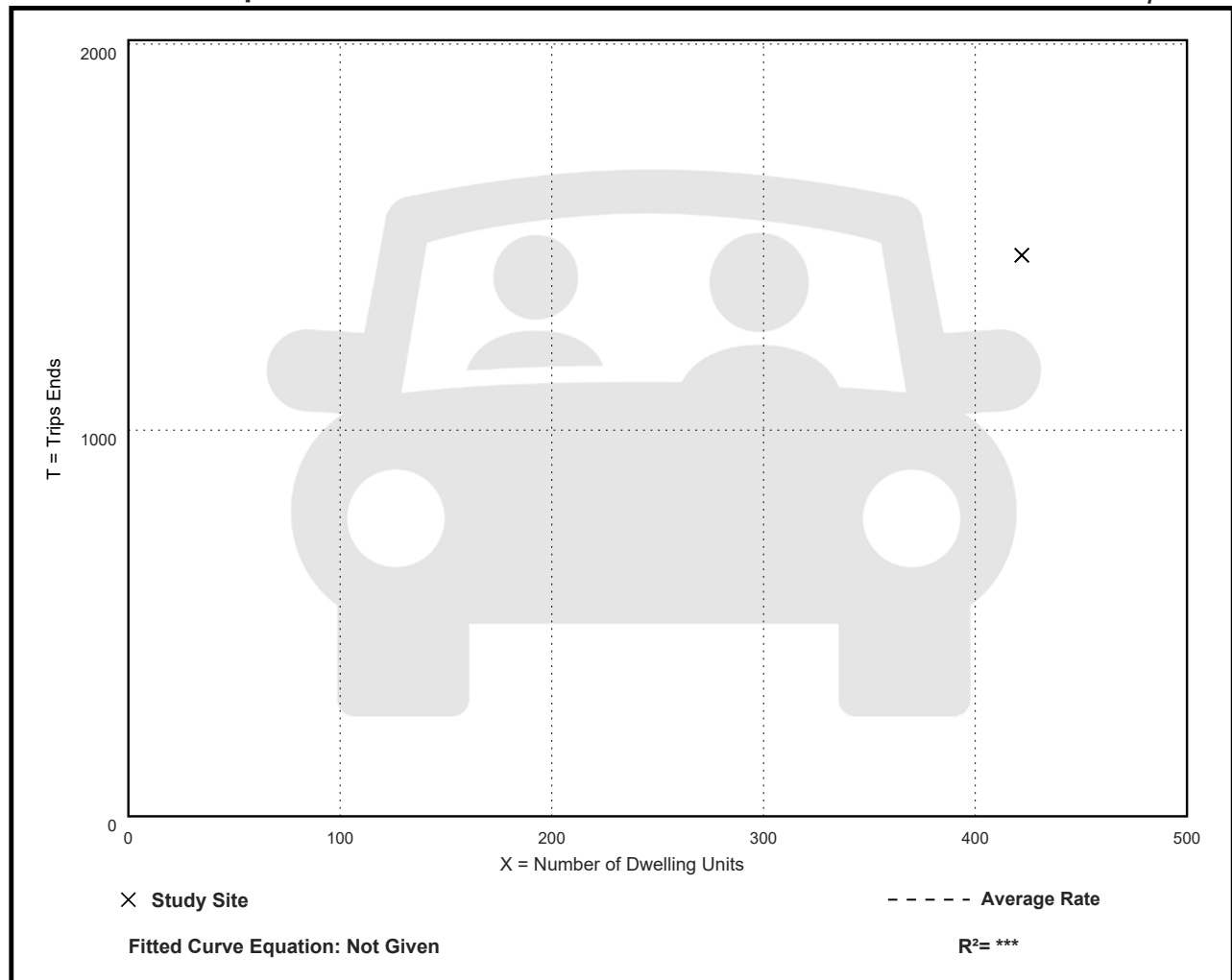
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
3.44	3.44 - 3.44	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Vehicle Trip Ends vs: Dwelling Units

On a: **Weekday,**

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 365

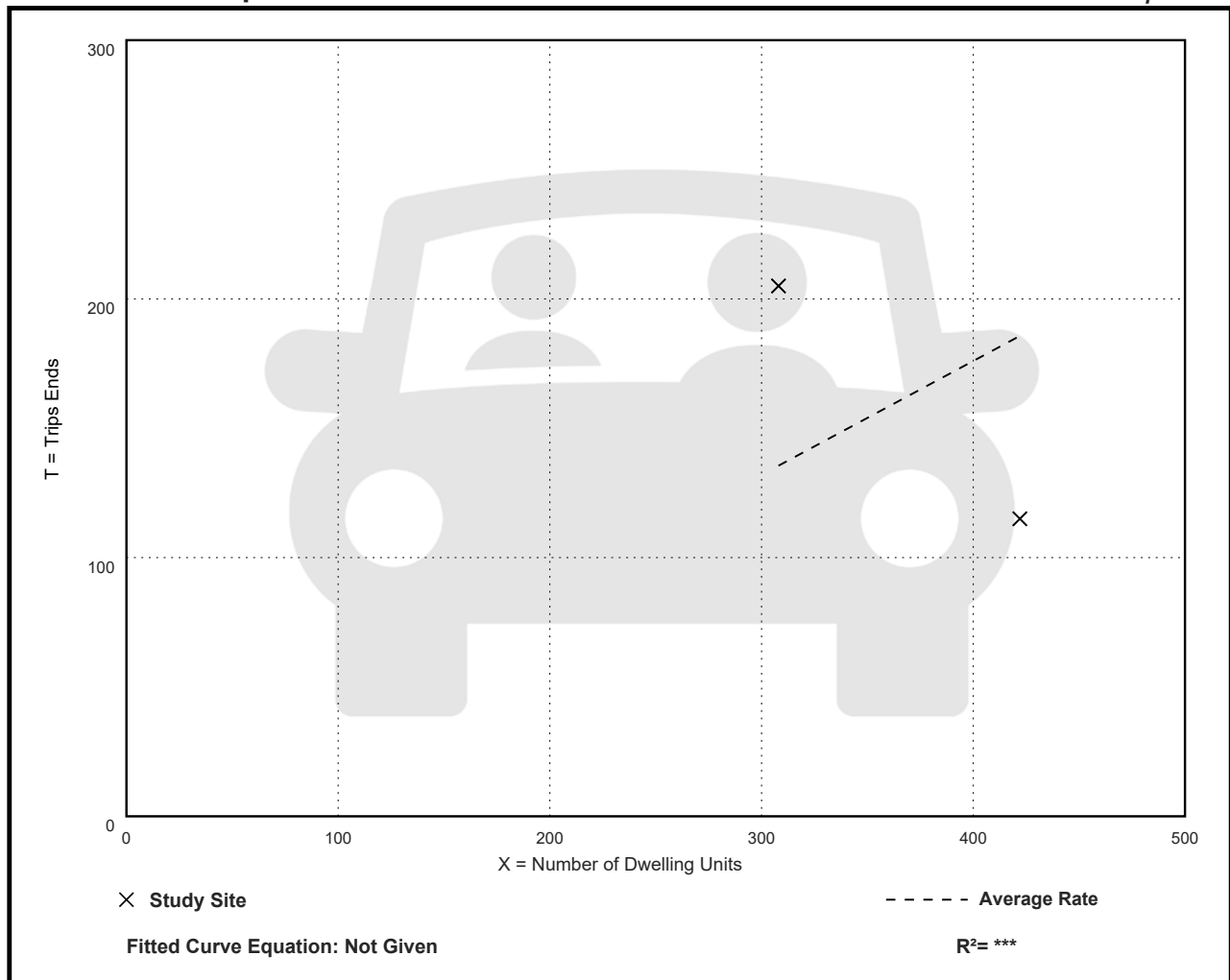
Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.27 - 0.67	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 365

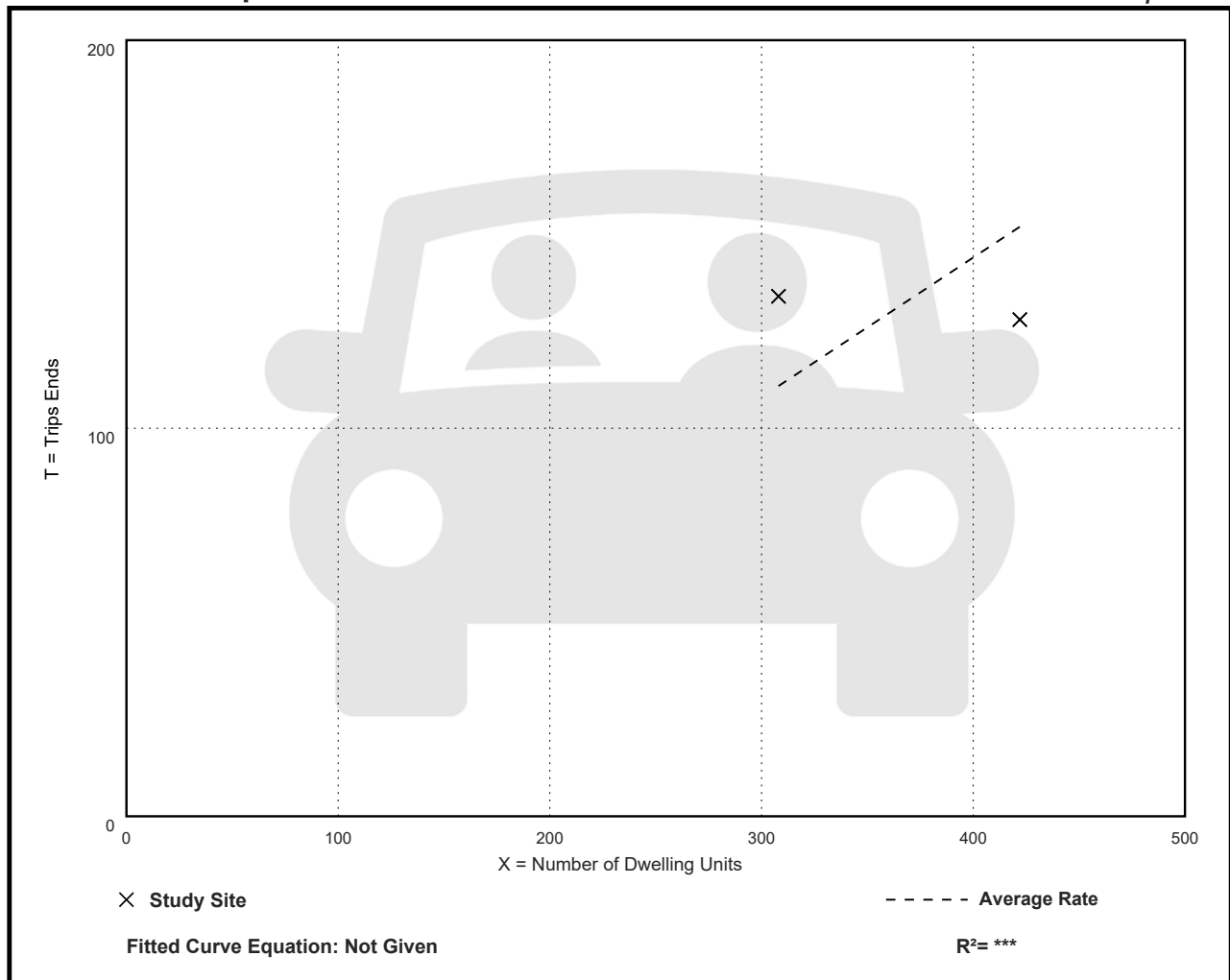
Directional Distribution: 71% entering, 29% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.30 - 0.44	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 365

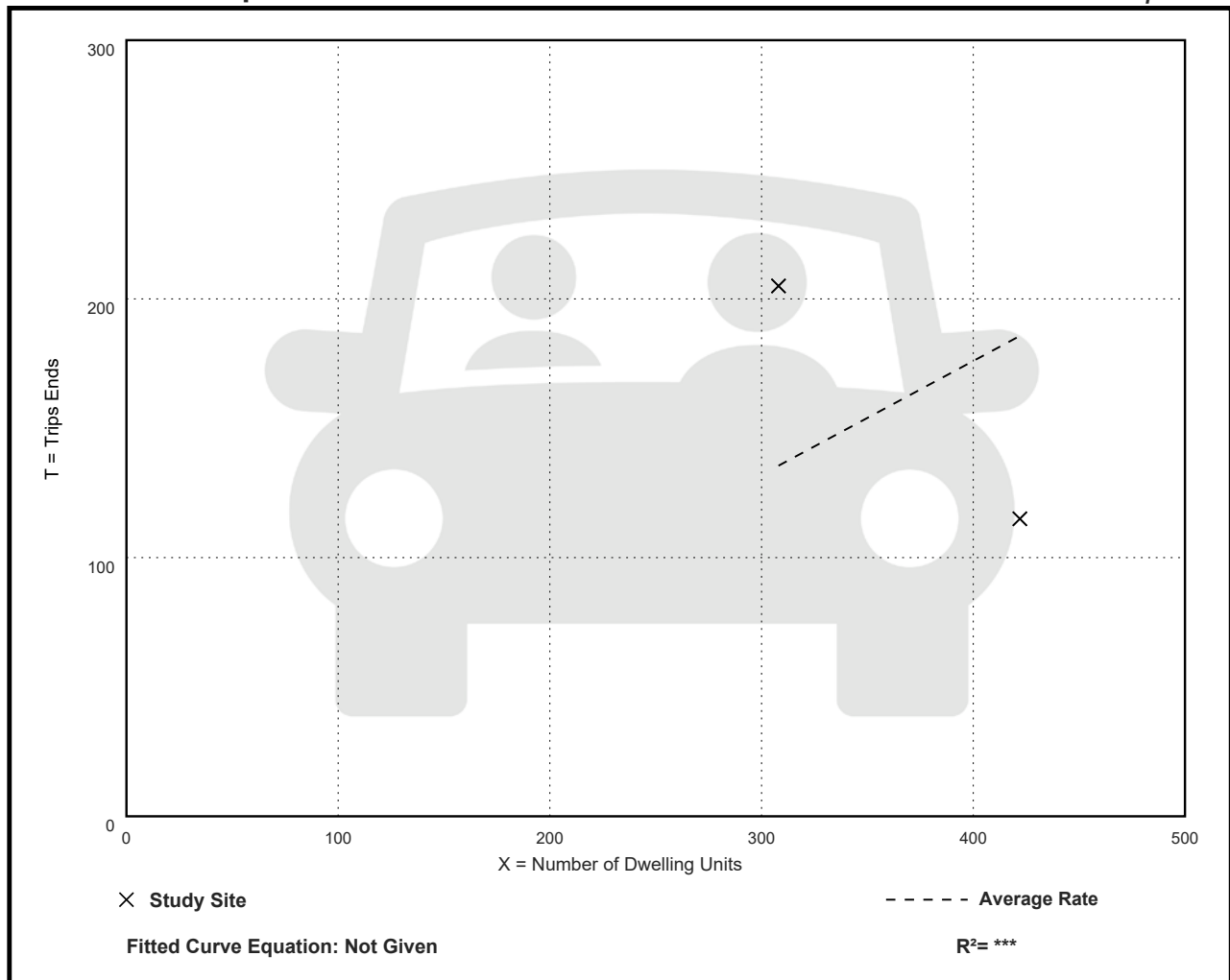
Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.27 - 0.67	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 365

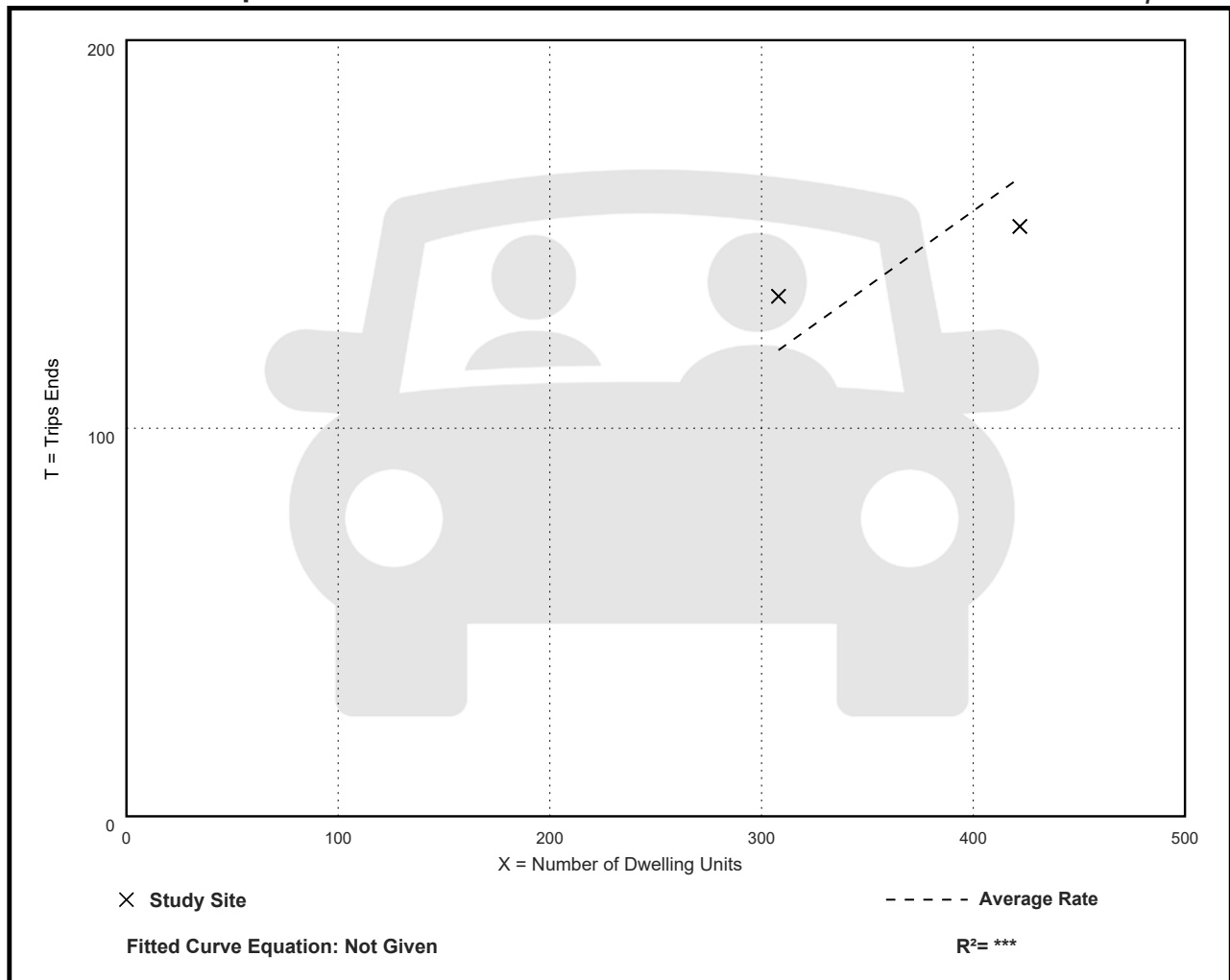
Directional Distribution: 65% entering, 35% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.39	0.36 - 0.44	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 308

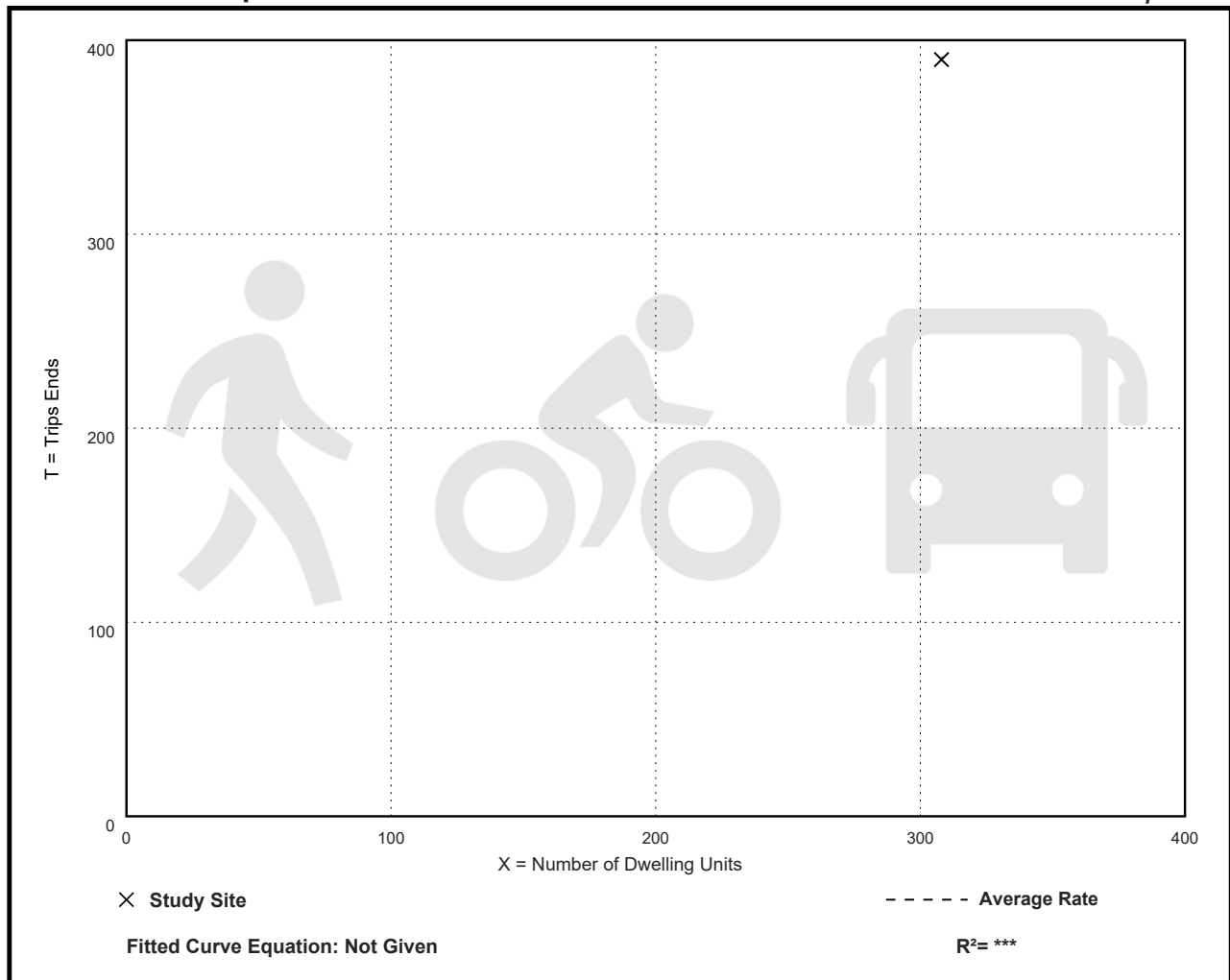
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
1.27	1.27 - 1.27	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 308

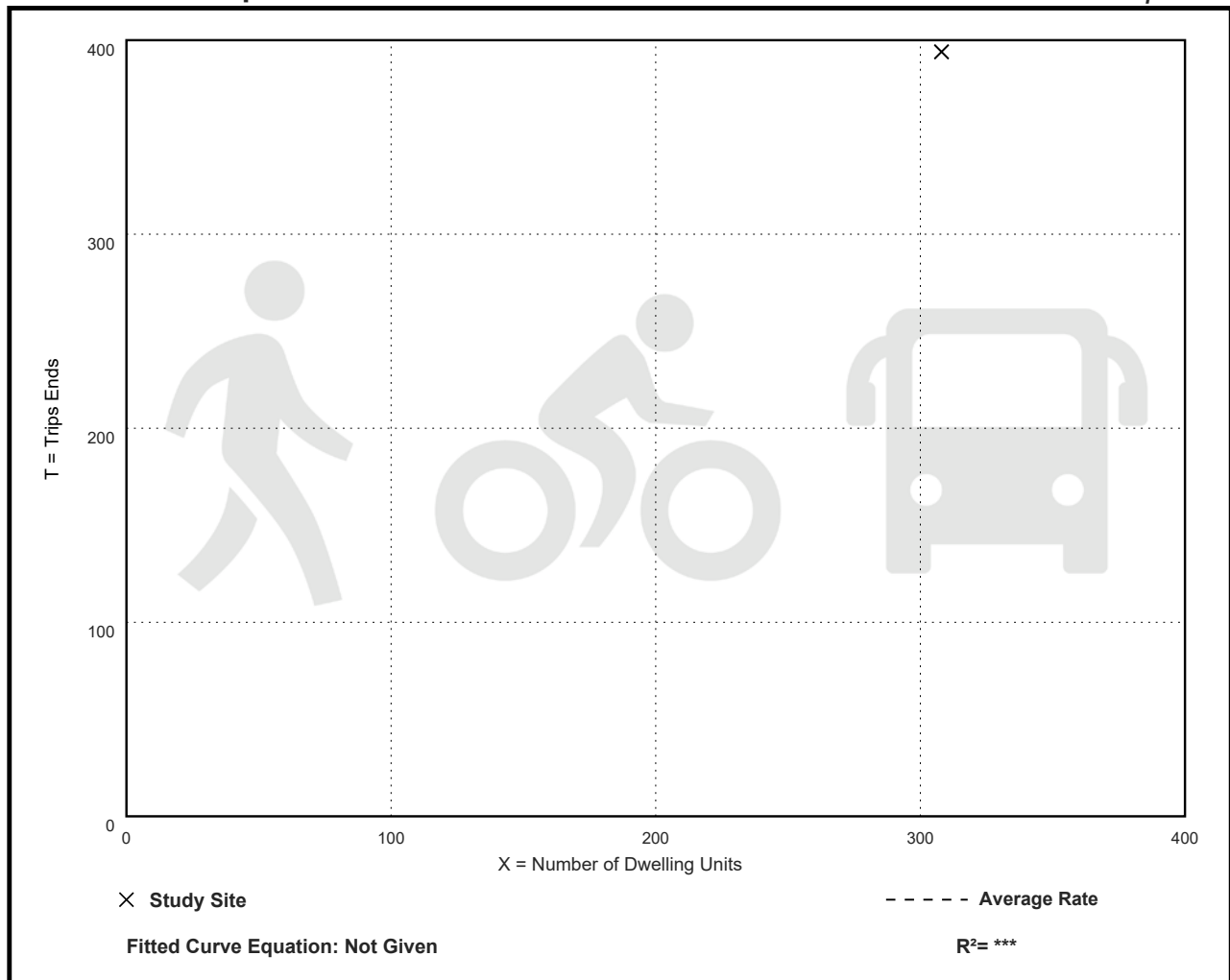
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
1.28	1.28 - 1.28	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 308

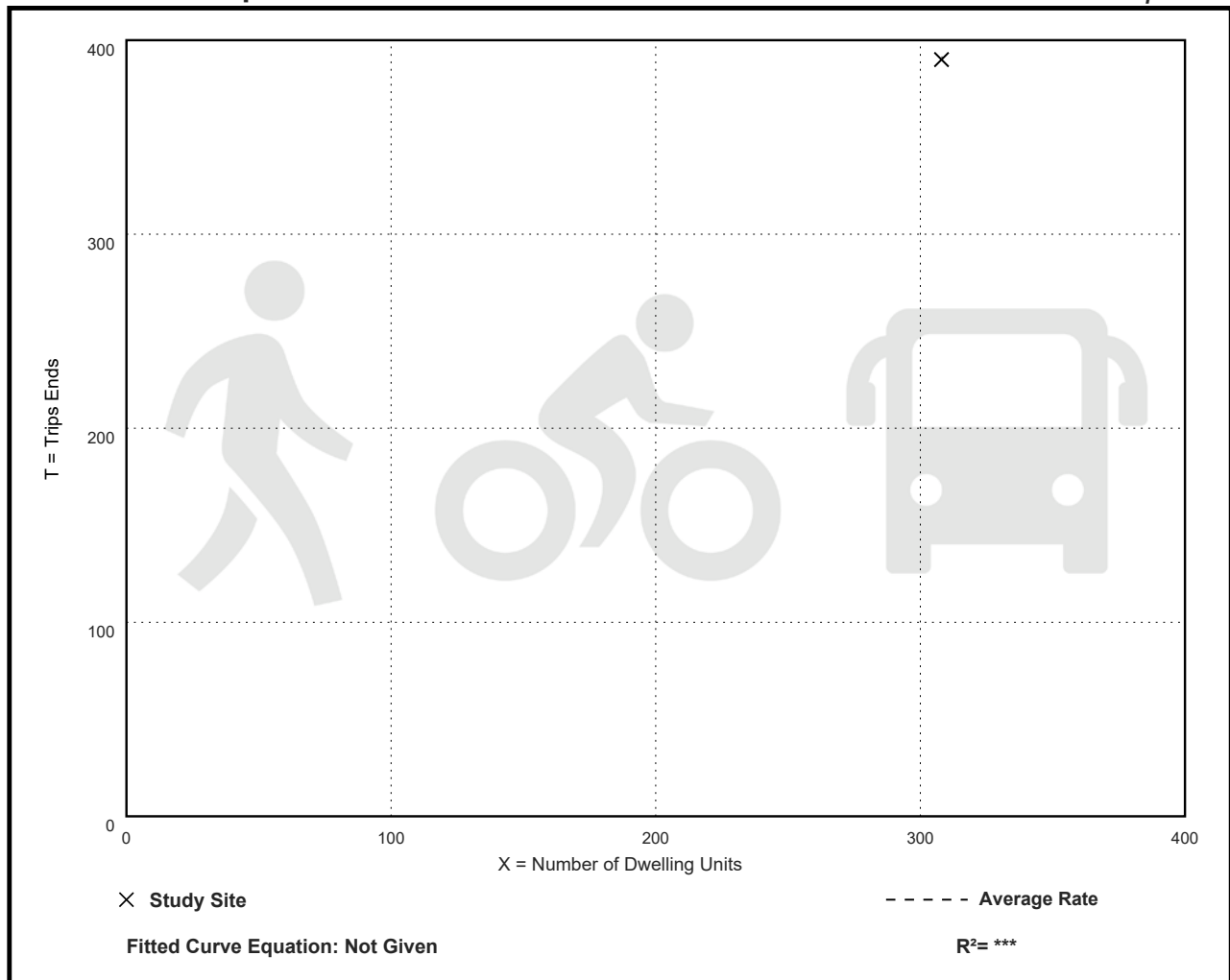
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
1.27	1.27 - 1.27	***

Data Plot and Equation

Caution – Small Sample Size



Low-Rise Residential with Ground-Floor Commercial GFA (1-25k) (230)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 308

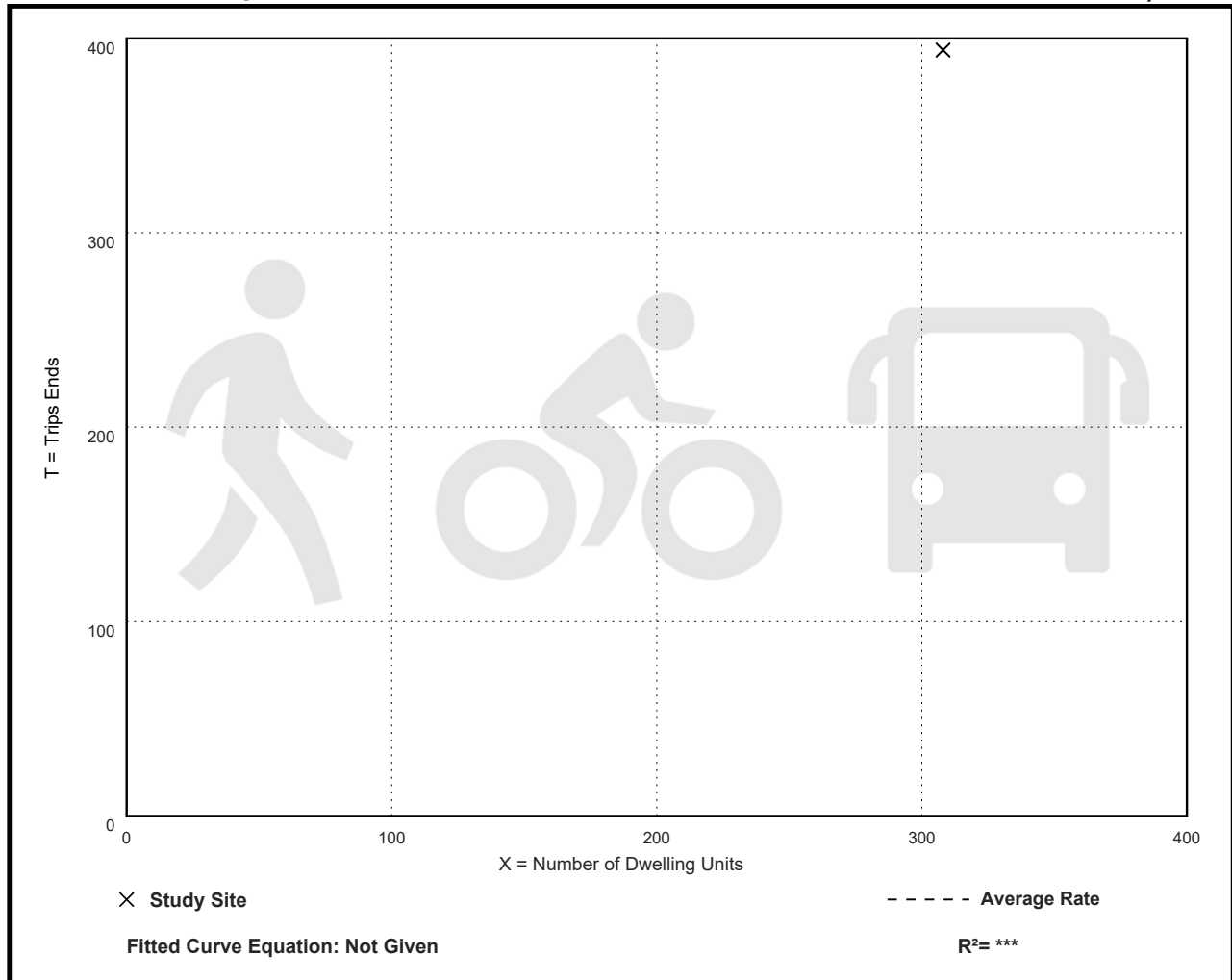
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
1.28	1.28 - 1.28	***

Data Plot and Equation

Caution – Small Sample Size



Land Use: 221

Multifamily Housing (Mid-Rise)

Description

Mid-rise multifamily housing includes apartments and condominiums located in a building that has between four and 10 floors of living space. Access to individual dwelling units is through an outside building entrance, a lobby, elevator, and a set of hallways.

Multifamily housing (low-rise) (Land Use 220), multifamily housing (high-rise) (Land Use 222), off-campus student apartment (mid-rise) (Land Use 226), and mid-rise residential with ground-floor commercial (Land Use 231) are related land uses.

Land Use Subcategory

Data are presented for two subcategories for this land use: (1) not close to rail transit and (2) close to rail transit. A site is considered close to rail transit if the walking distance between the residential site entrance and the closest rail transit station entrance is ½ mile or less.

Additional Data

For the six sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.5 residents per occupied dwelling unit.

For the five sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 96 percent of the total dwelling units were occupied.

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

It is expected that the number of bedrooms and number of residents are likely correlated to the trips generated by a residential site. To assist in future analysis, trip generation studies of all multifamily housing should attempt to obtain information on occupancy rate and on the mix of residential unit sizes (i.e., number of units by number of bedrooms at the site complex).

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in Alberta (CAN), California, District of Columbia, Florida, Georgia, Illinois, Maryland, Massachusetts, Minnesota, Montana, New Jersey, New York, Ontario (CAN), Oregon, Utah, and Virginia.

Source Numbers

168, 188, 204, 305, 306, 321, 818, 857, 862, 866, 901, 904, 910, 949, 951, 959, 963, 964, 966, 967, 969, 970, 1004, 1014, 1022, 1023, 1025, 1031, 1032, 1035, 1047, 1056, 1057, 1058, 1071, 1076

Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 11

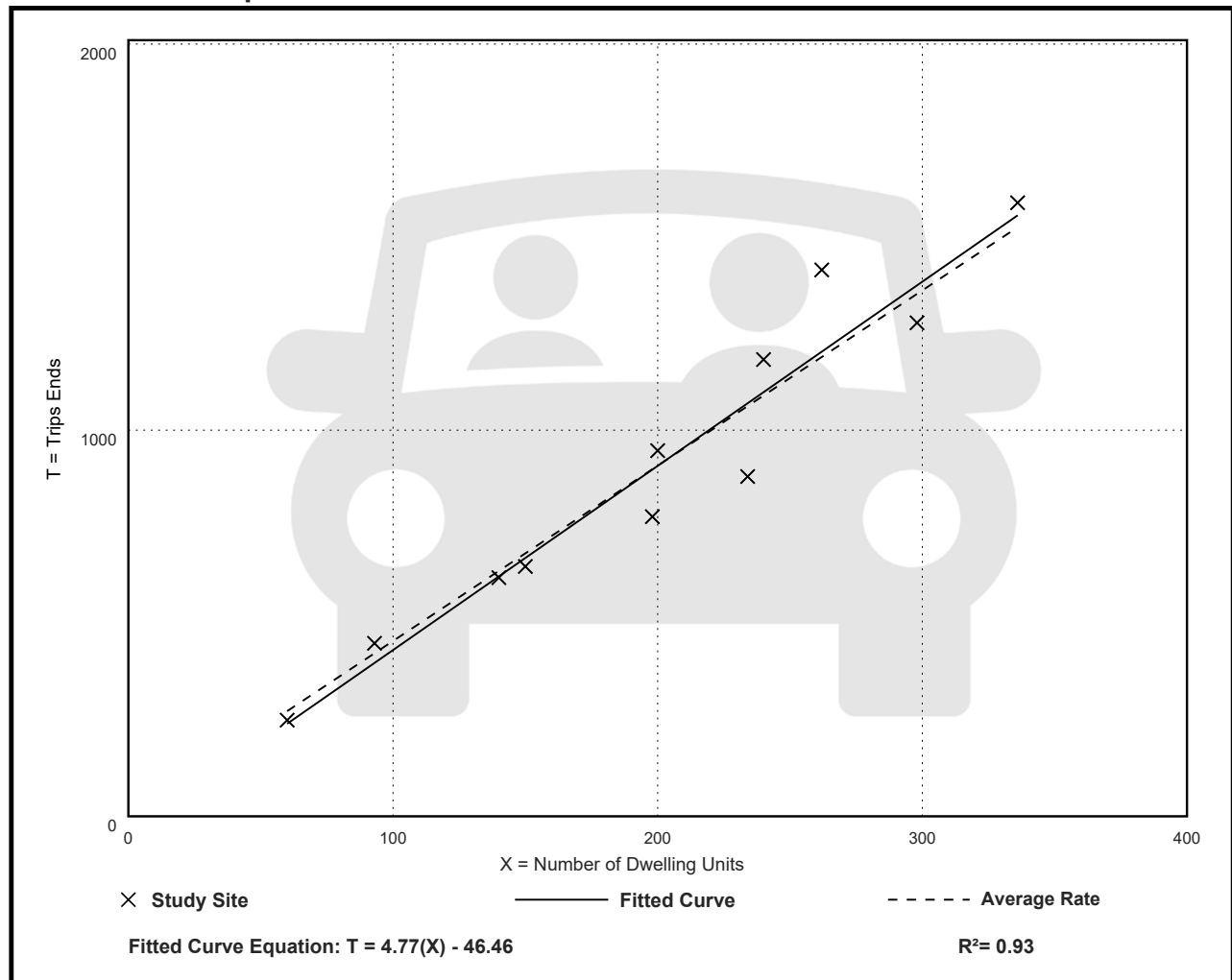
Avg. Num. of Dwelling Units: 201

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.54	3.76 - 5.40	0.51

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 30

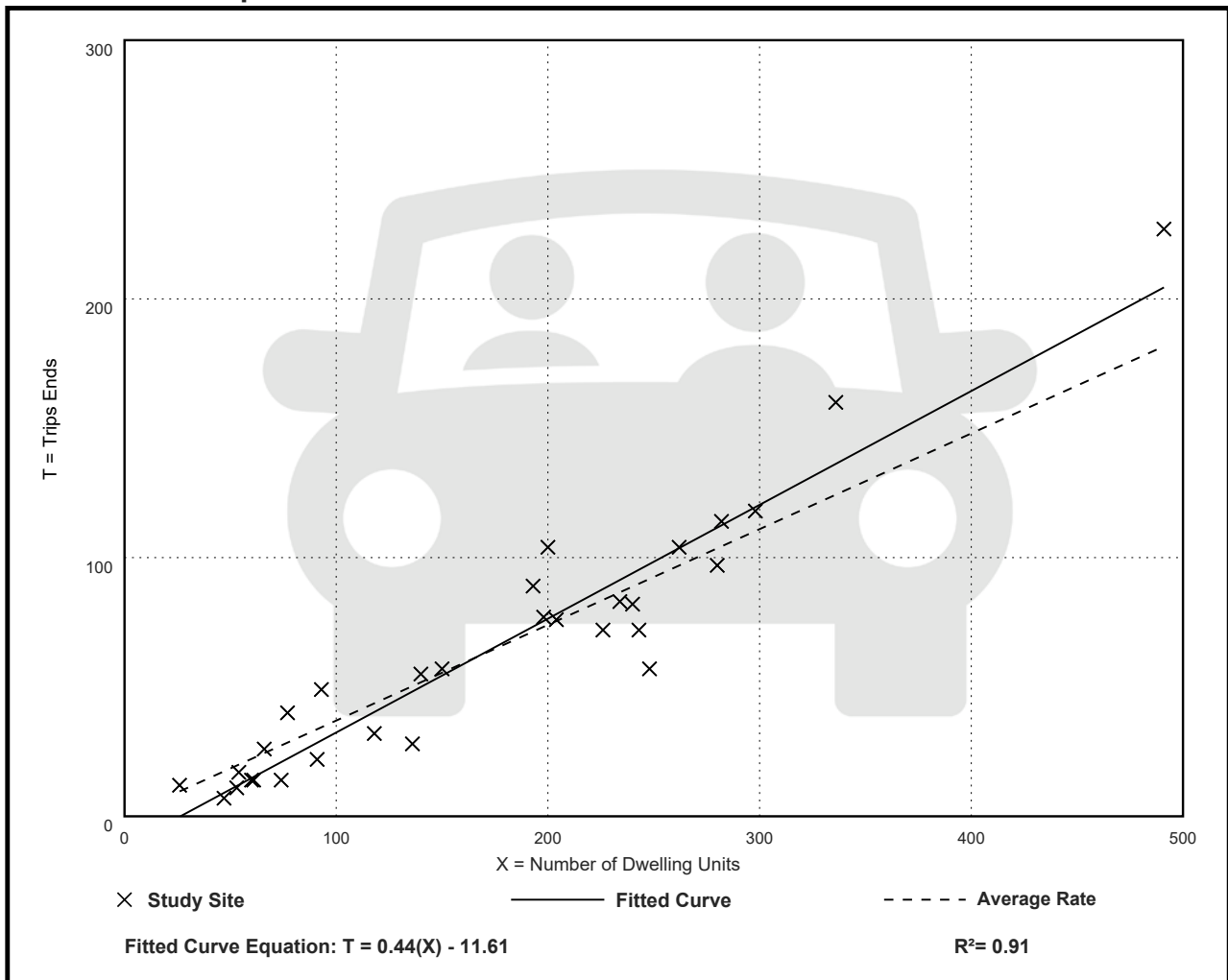
Avg. Num. of Dwelling Units: 173

Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.37	0.15 - 0.53	0.09

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 31

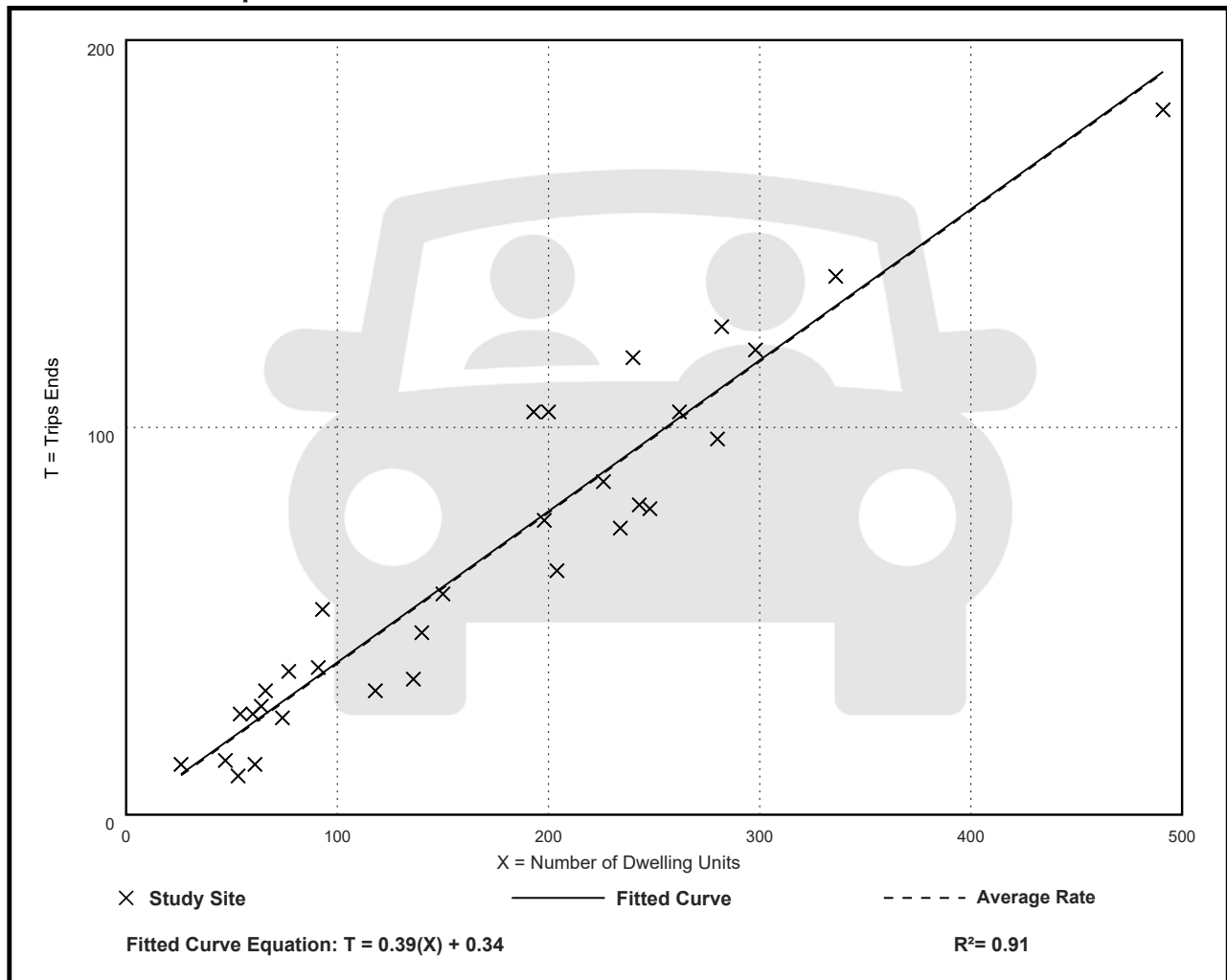
Avg. Num. of Dwelling Units: 169

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.39	0.19 - 0.57	0.08

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 23

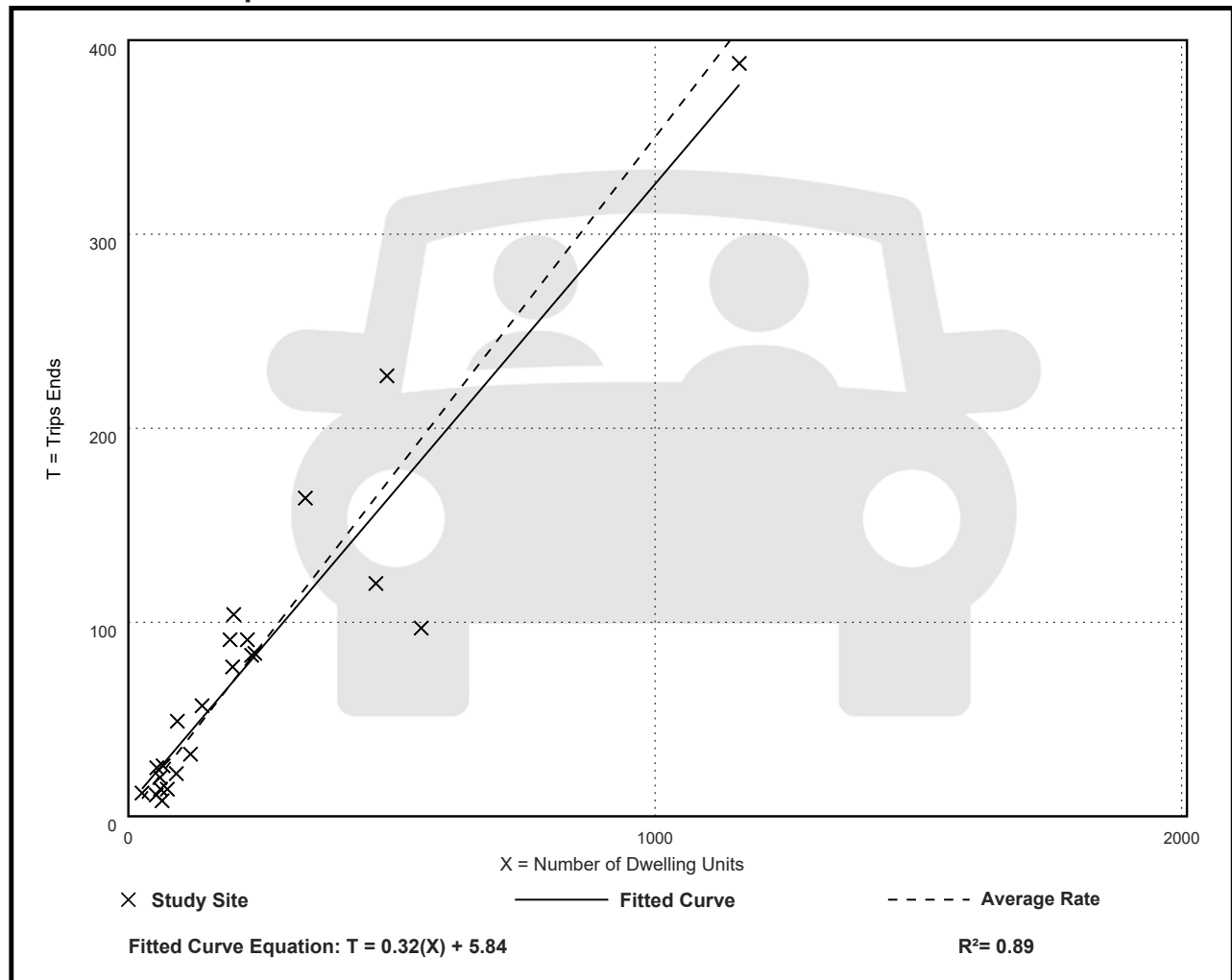
Avg. Num. of Dwelling Units: 226

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.35	0.13 - 0.53	0.11

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 22

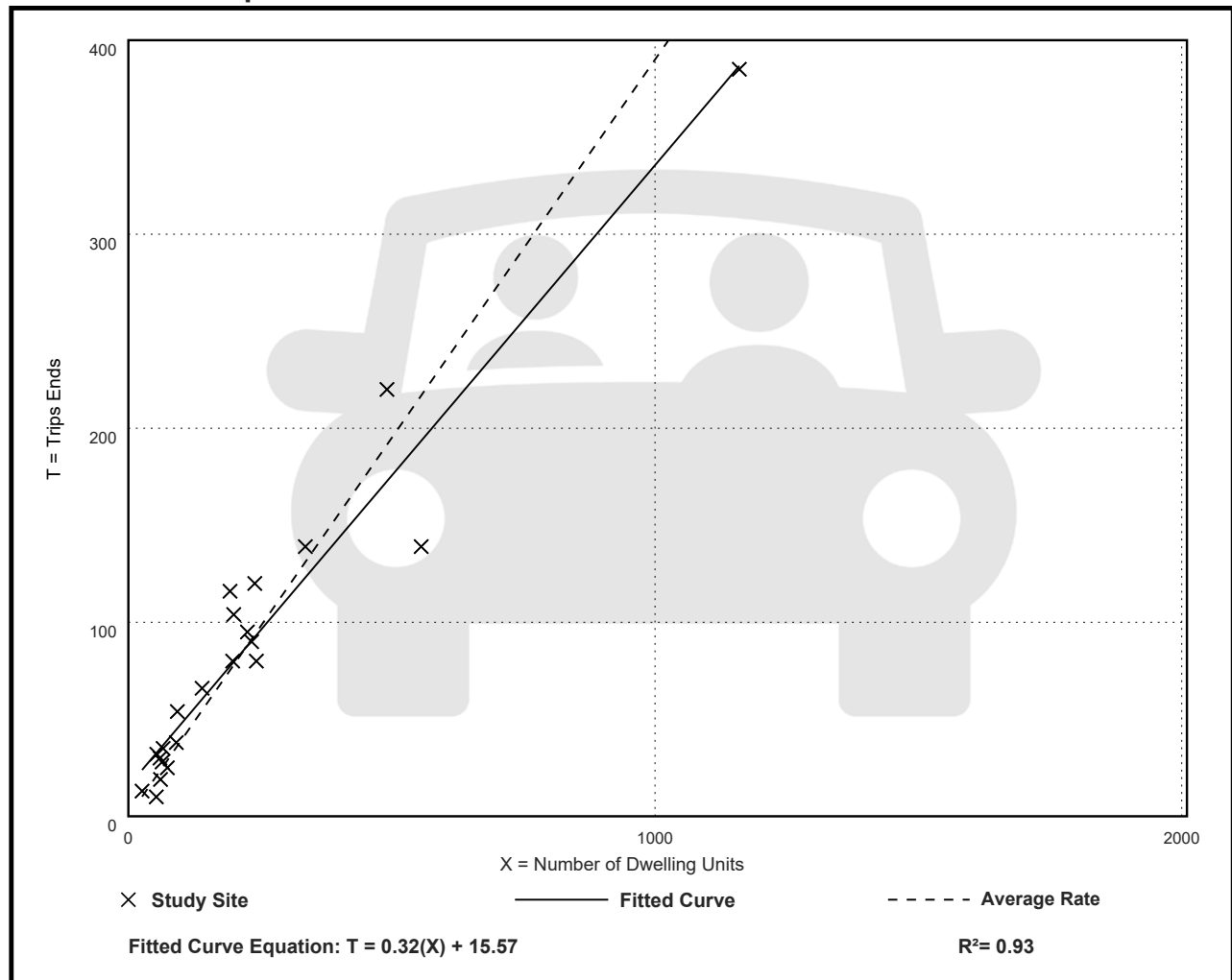
Avg. Num. of Dwelling Units: 221

Directional Distribution: 60% entering, 40% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.39	0.19 - 0.60	0.10

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units
On a: Saturday

Setting/Location: General Urban/Suburban

Number of Studies: 5

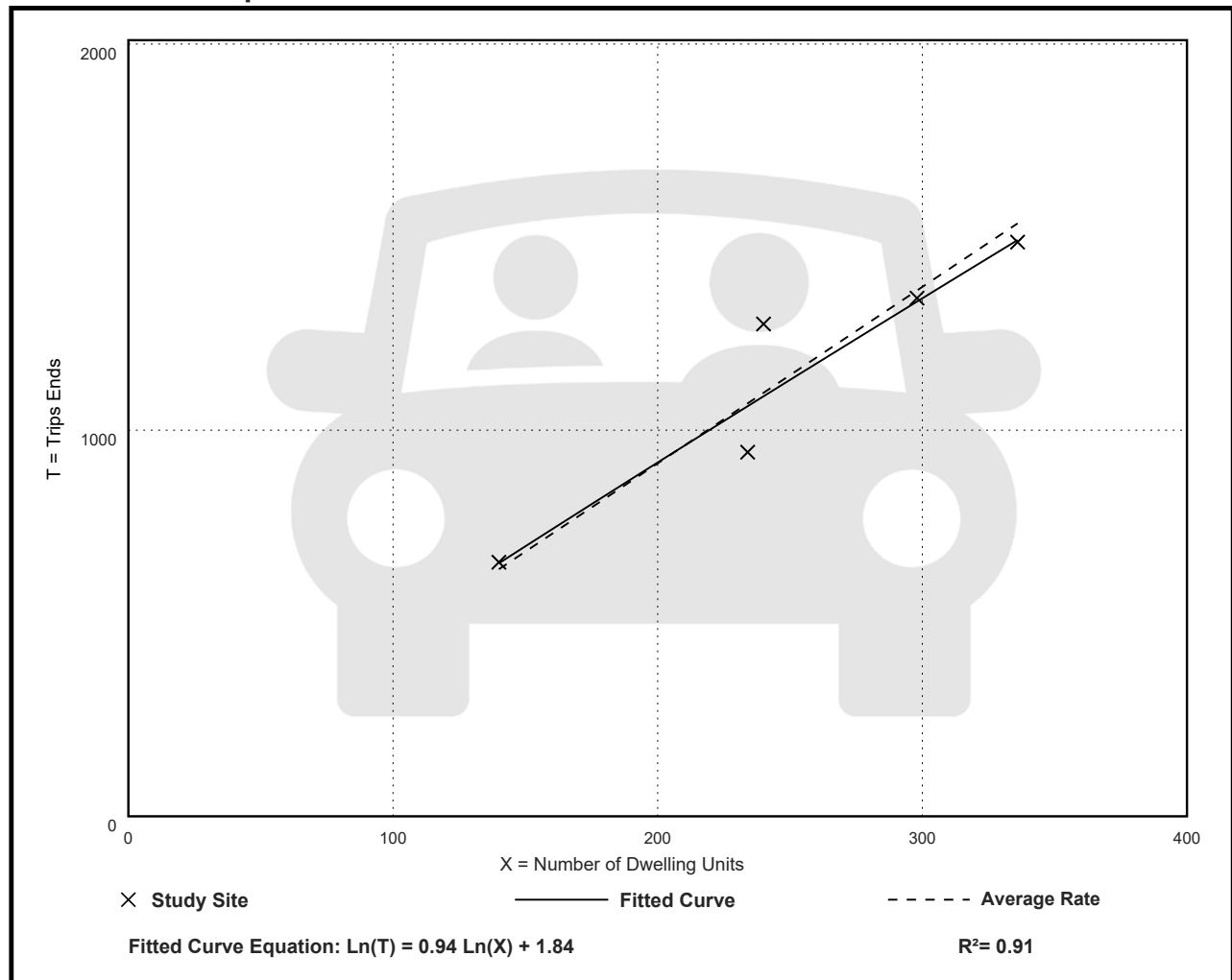
Avg. Num. of Dwelling Units: 250

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.57	4.03 - 5.31	0.46

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

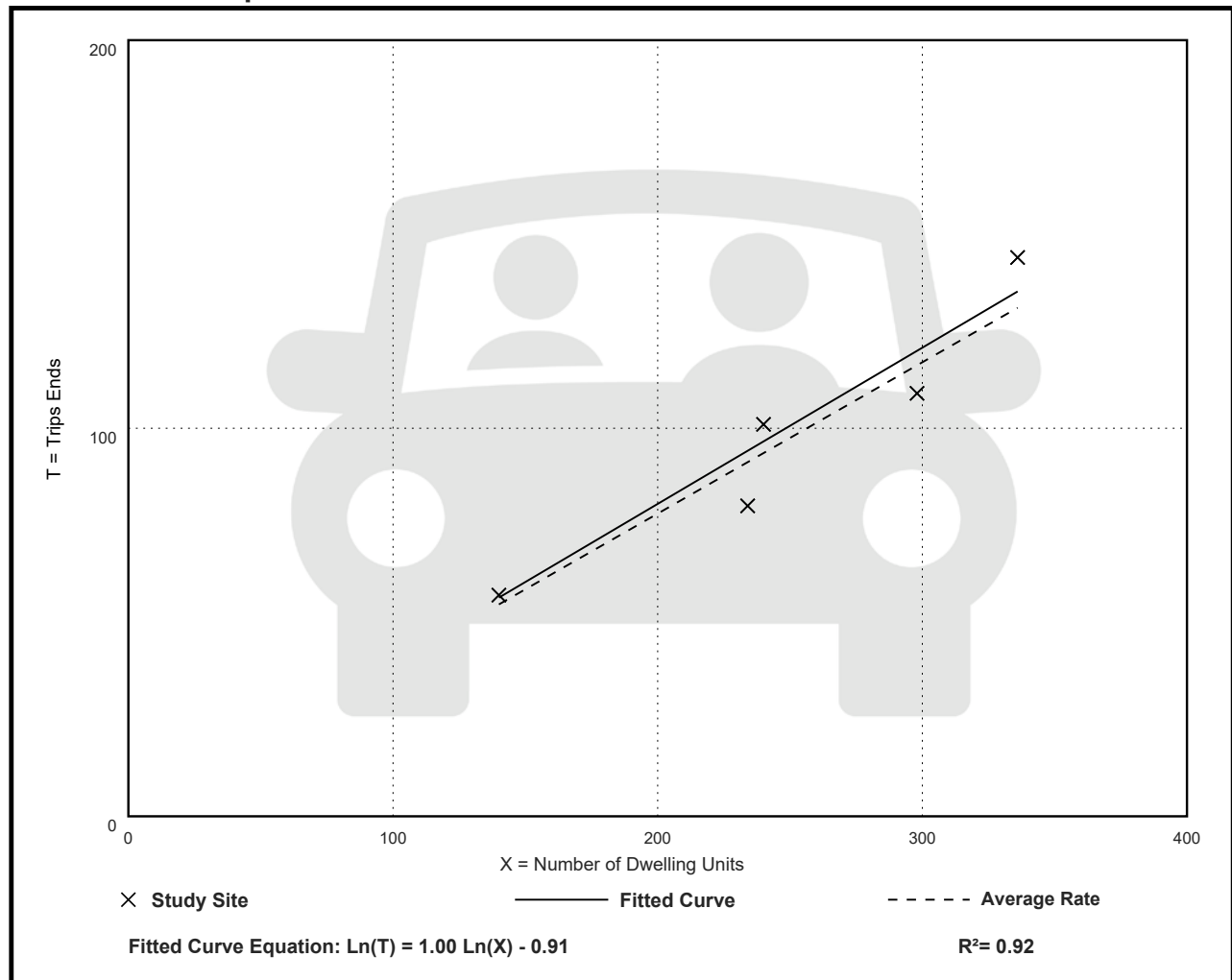
Avg. Num. of Dwelling Units: 250

Directional Distribution: 51% entering, 49% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.39	0.34 - 0.43	0.04

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units
On a: Sunday

Setting/Location: General Urban/Suburban

Number of Studies: 5

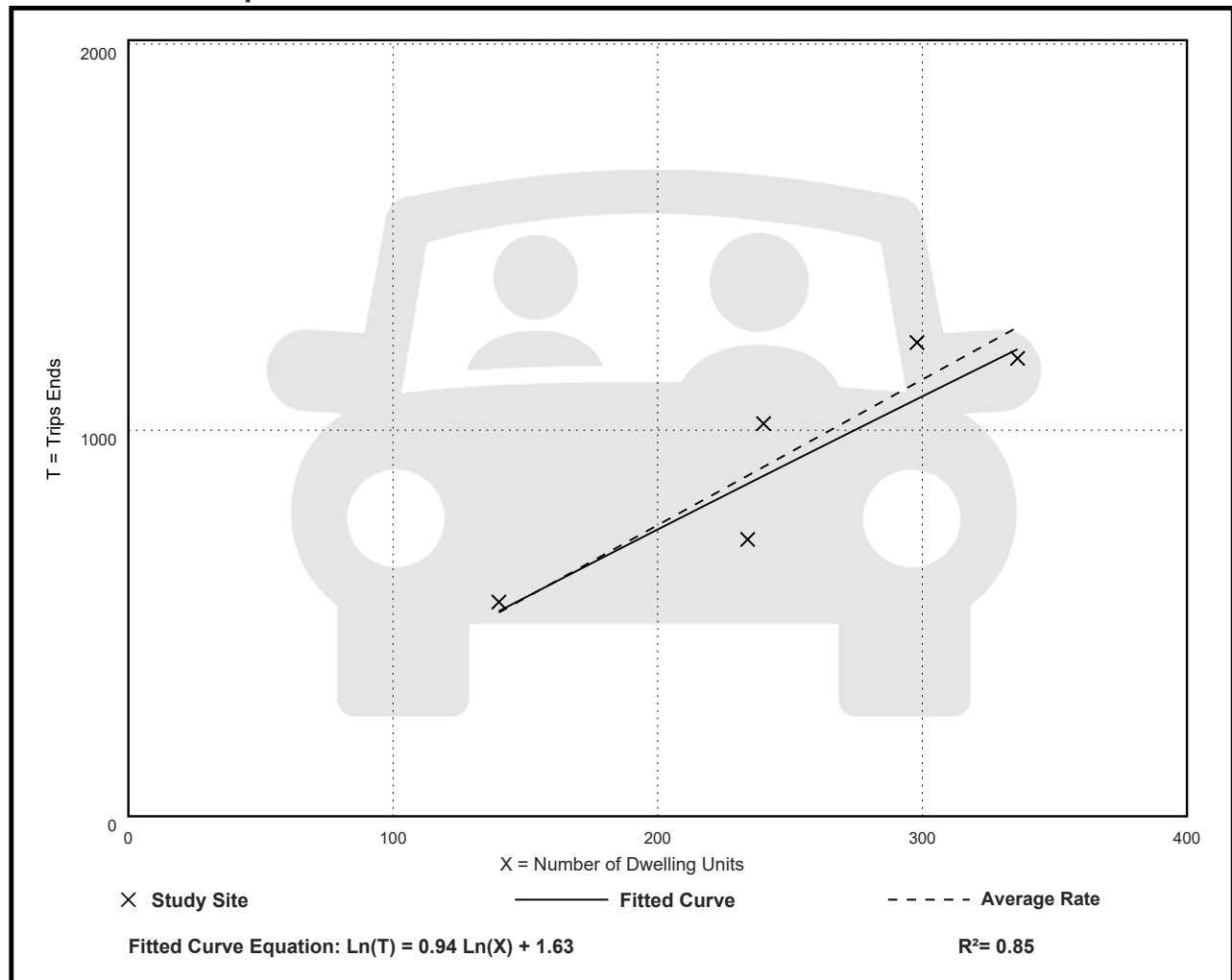
Avg. Num. of Dwelling Units: 250

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
3.77	3.06 - 4.24	0.48

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Sunday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

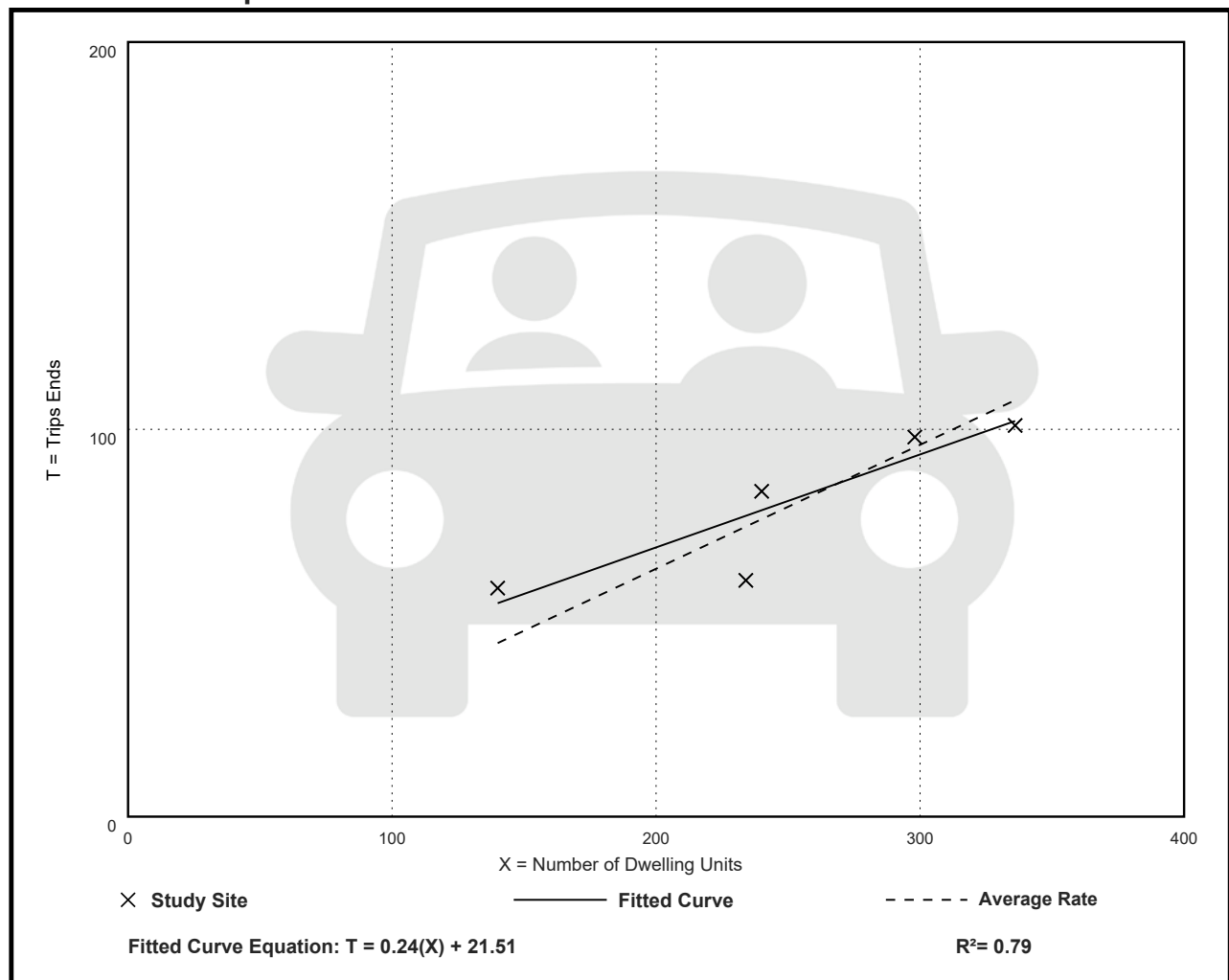
Avg. Num. of Dwelling Units: 250

Directional Distribution: 55% entering, 45% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.32	0.26 - 0.42	0.05

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

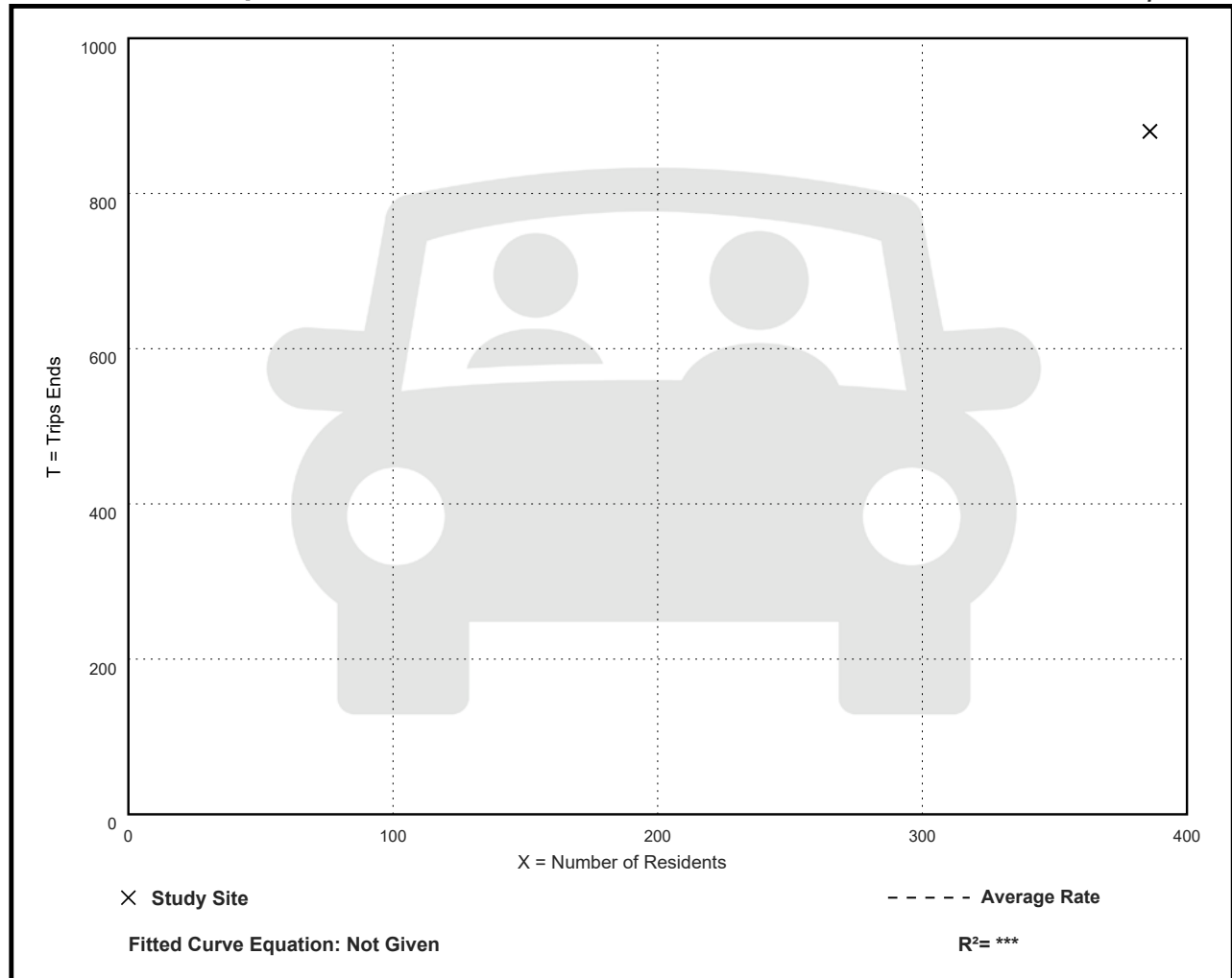
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
2.28	2.28 - 2.28	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

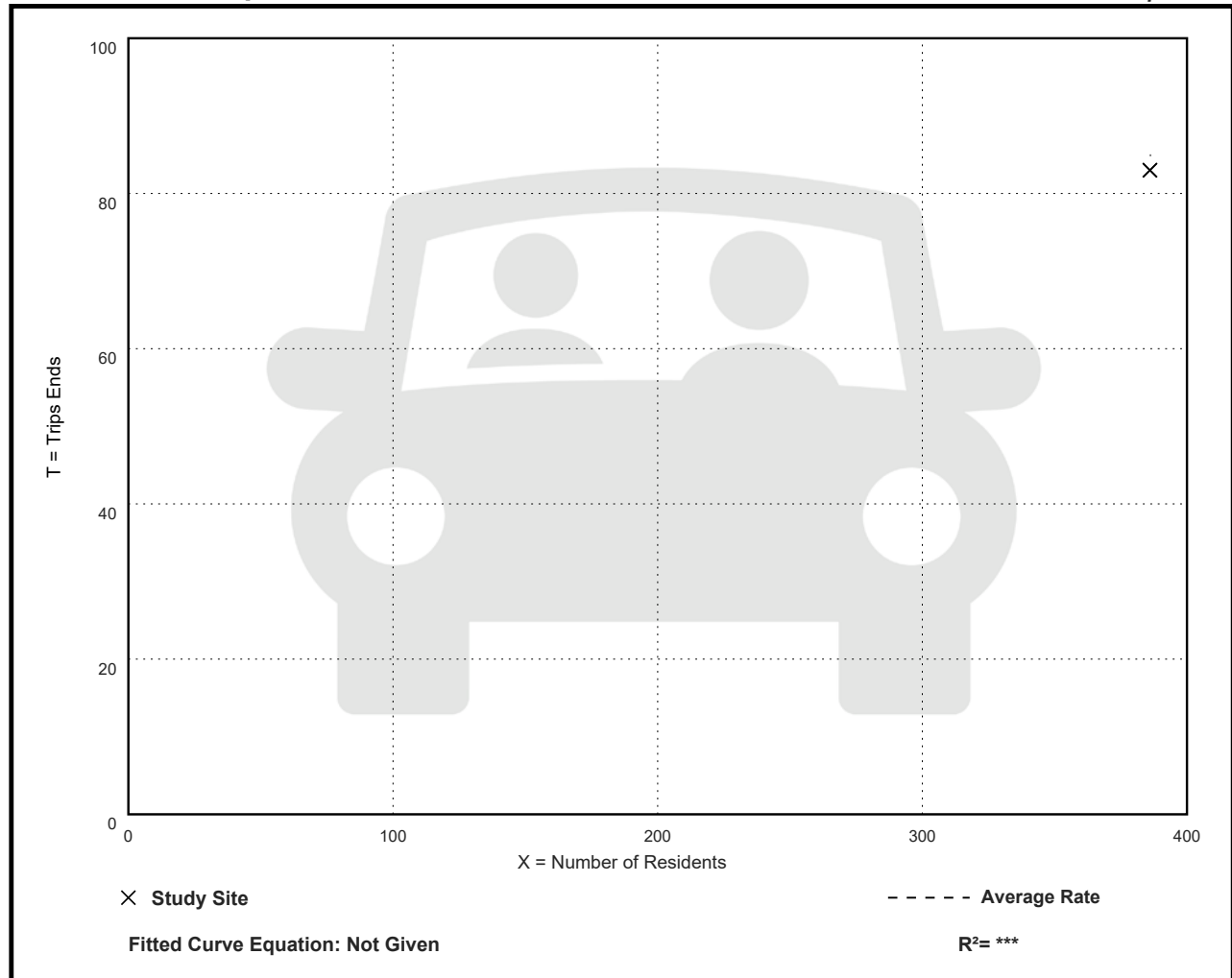
Directional Distribution: 20% entering, 80% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.22	0.22 - 0.22	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

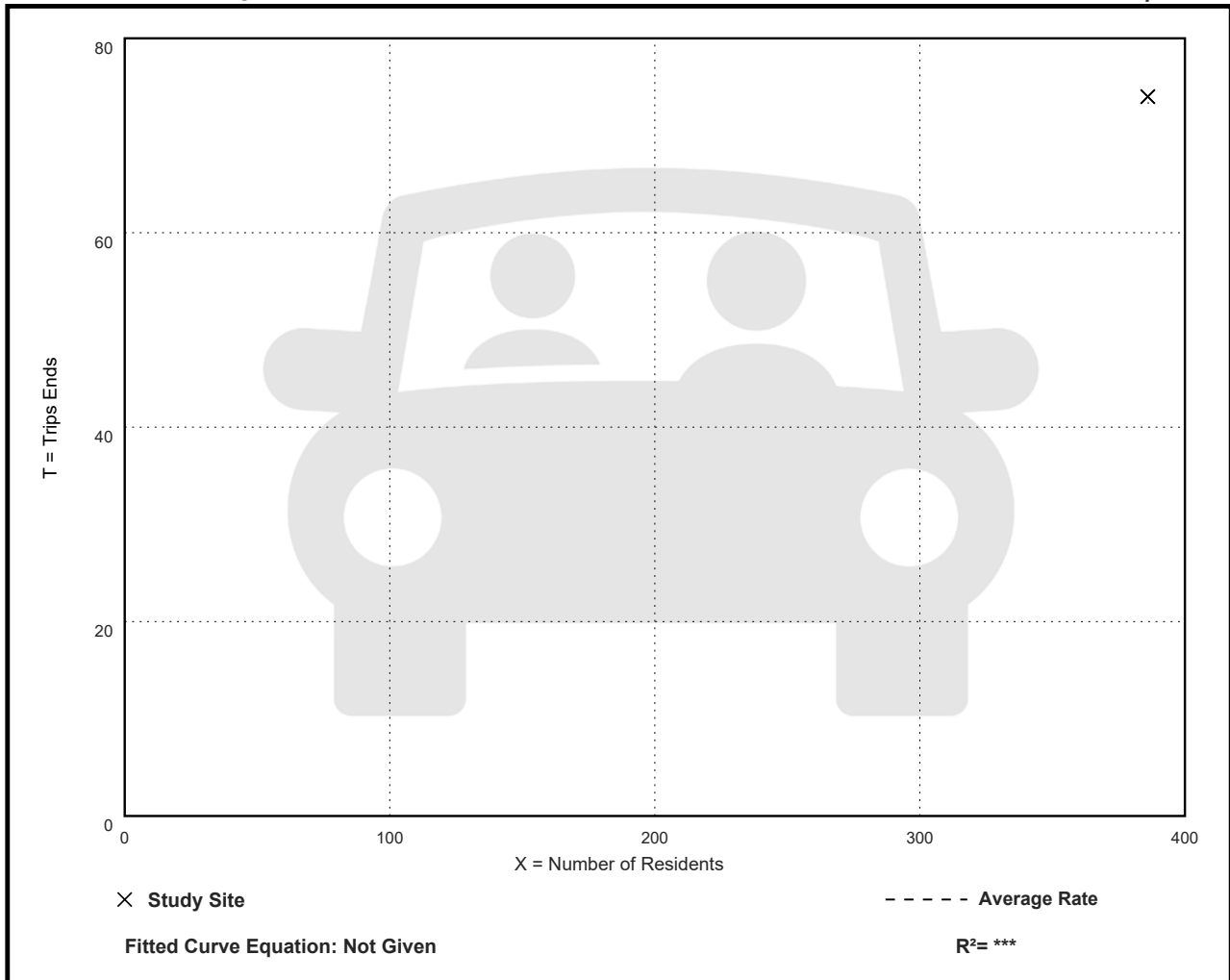
Directional Distribution: 64% entering, 36% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.19	0.19 - 0.19	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

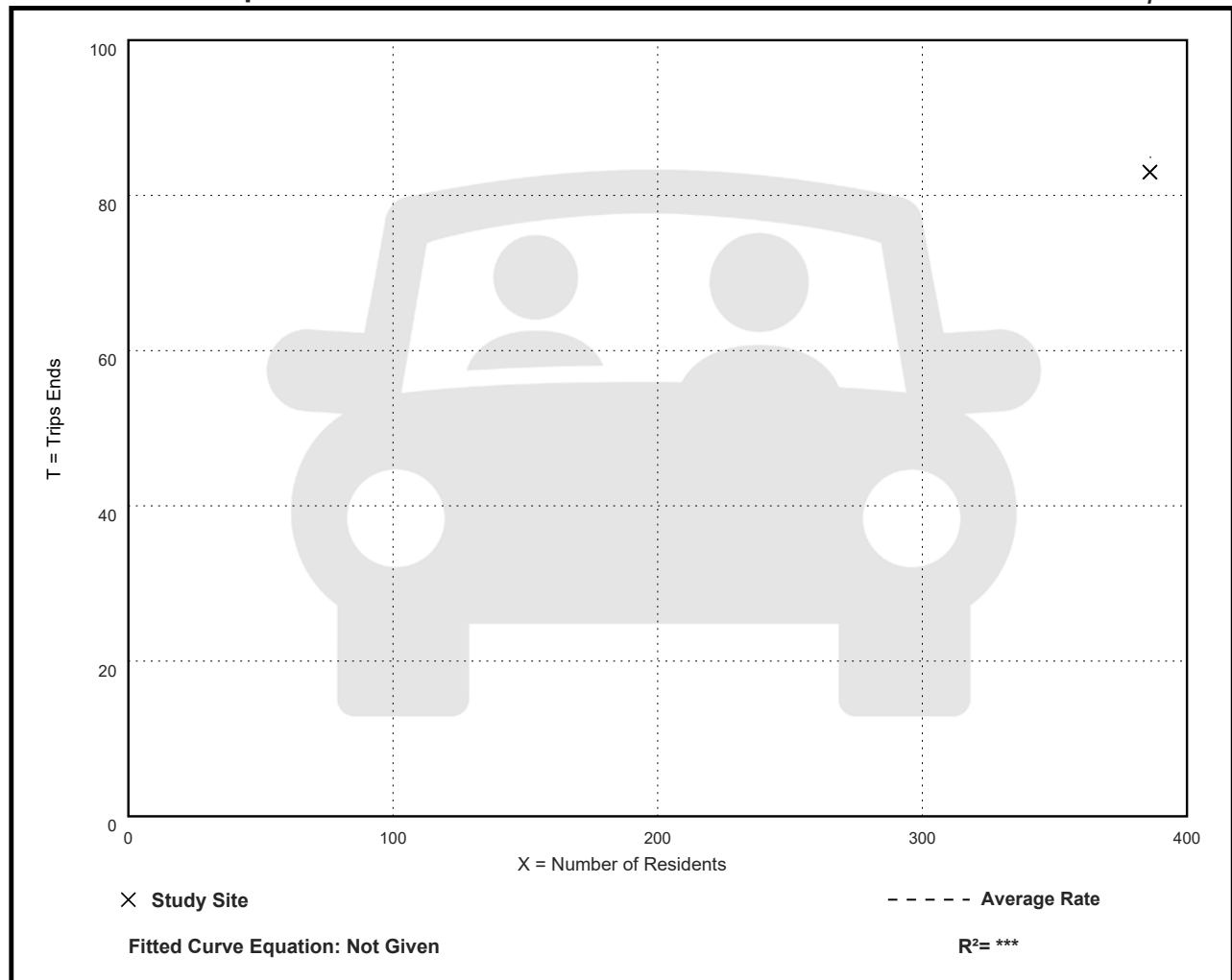
Directional Distribution: 20% entering, 80% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.22	0.22 - 0.22	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

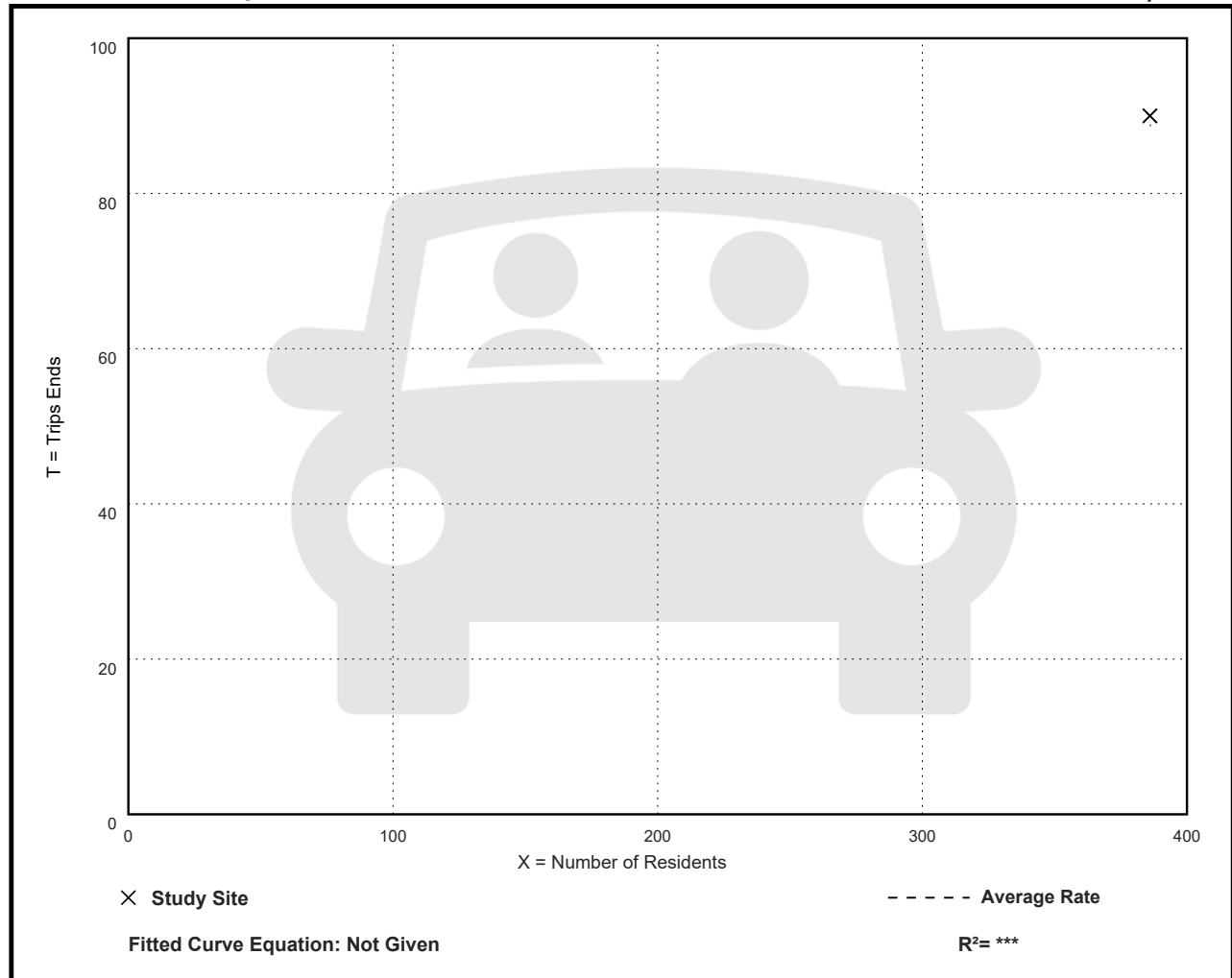
Directional Distribution: 64% entering, 36% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.23	0.23 - 0.23	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents
On a: Saturday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

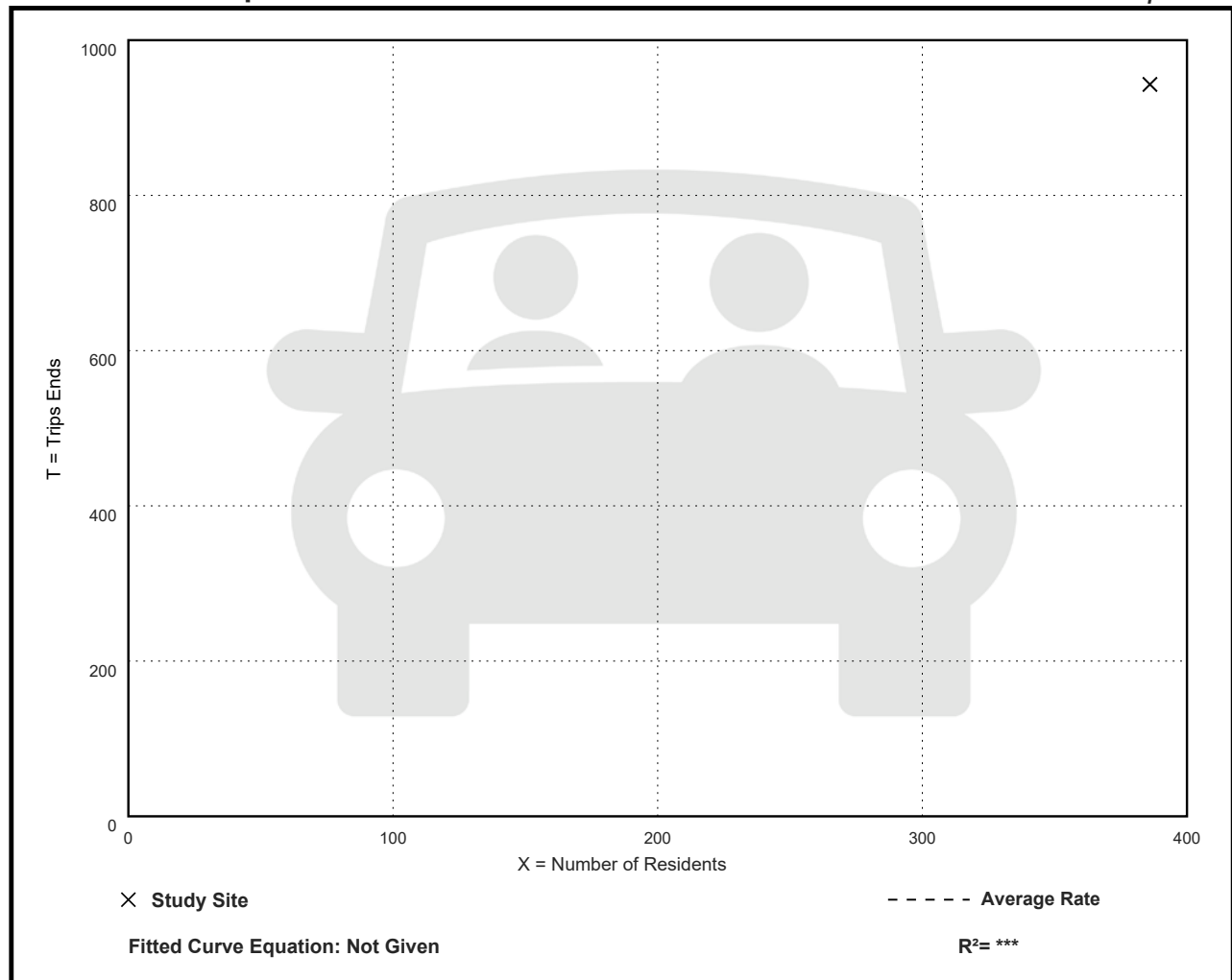
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
2.44	2.44 - 2.44	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

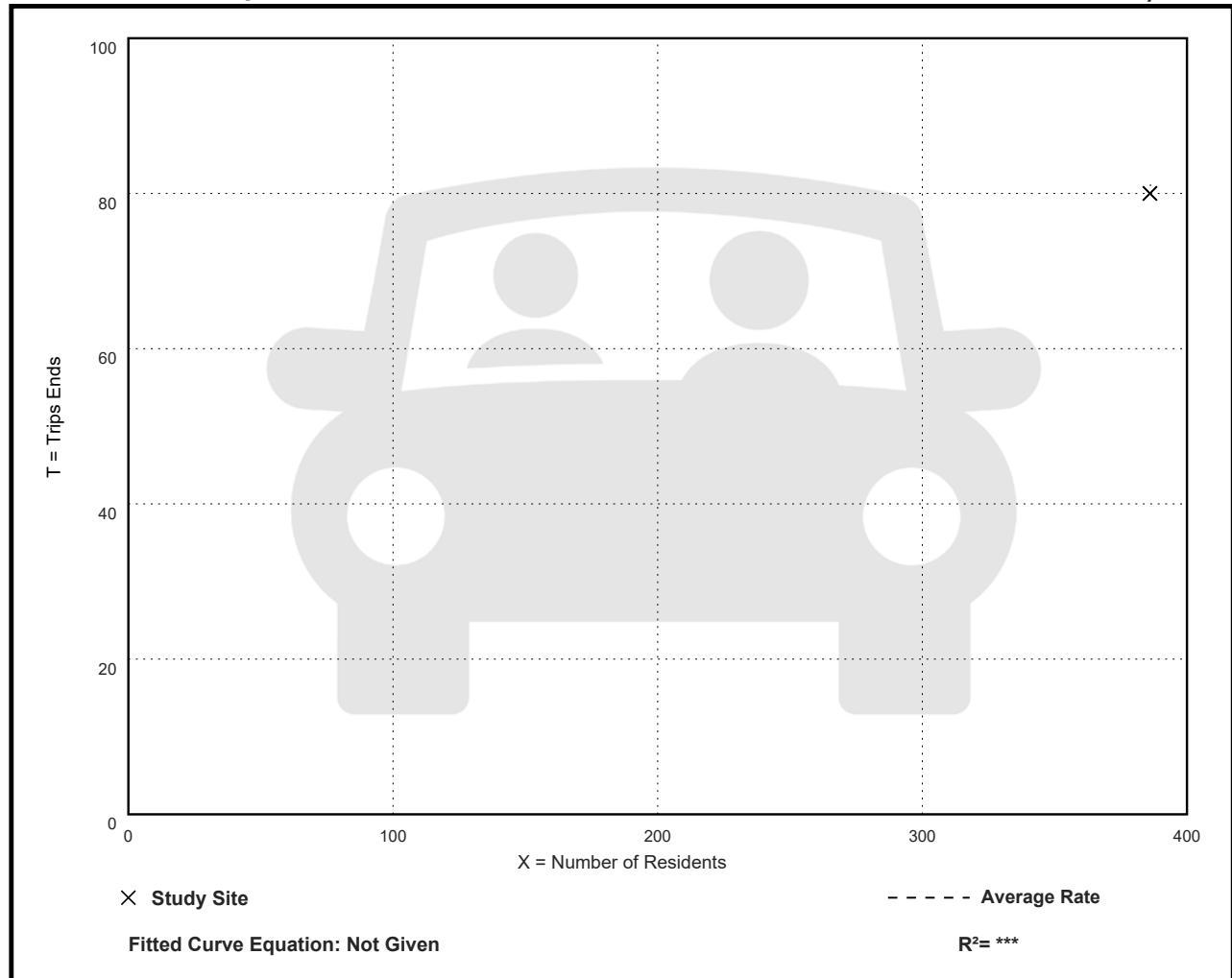
Directional Distribution: 53% entering, 47% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.21	0.21 - 0.21	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents
On a: Sunday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

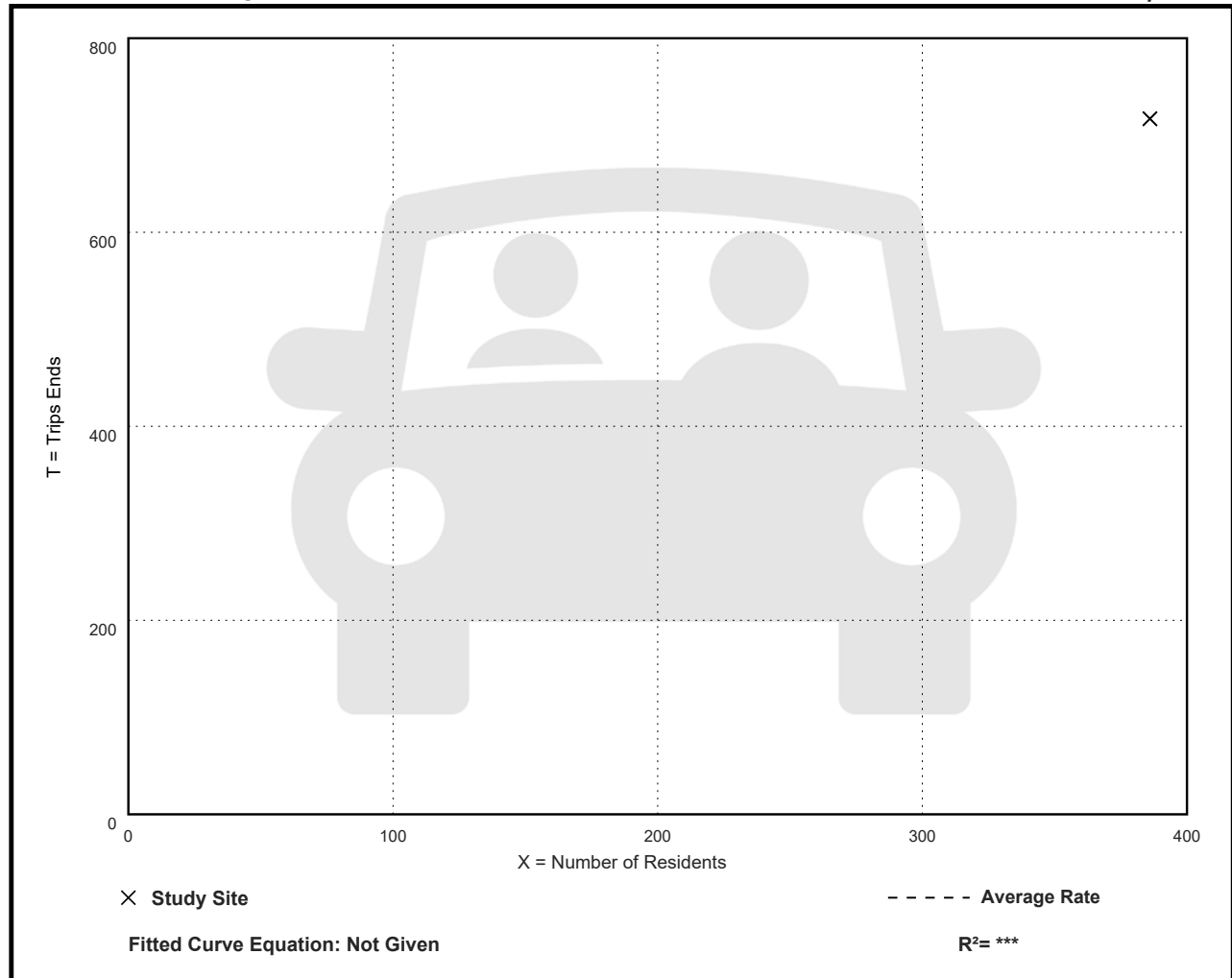
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
1.86	1.86 - 1.86	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Vehicle Trip Ends vs: Residents

On a: Sunday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Residents: 386

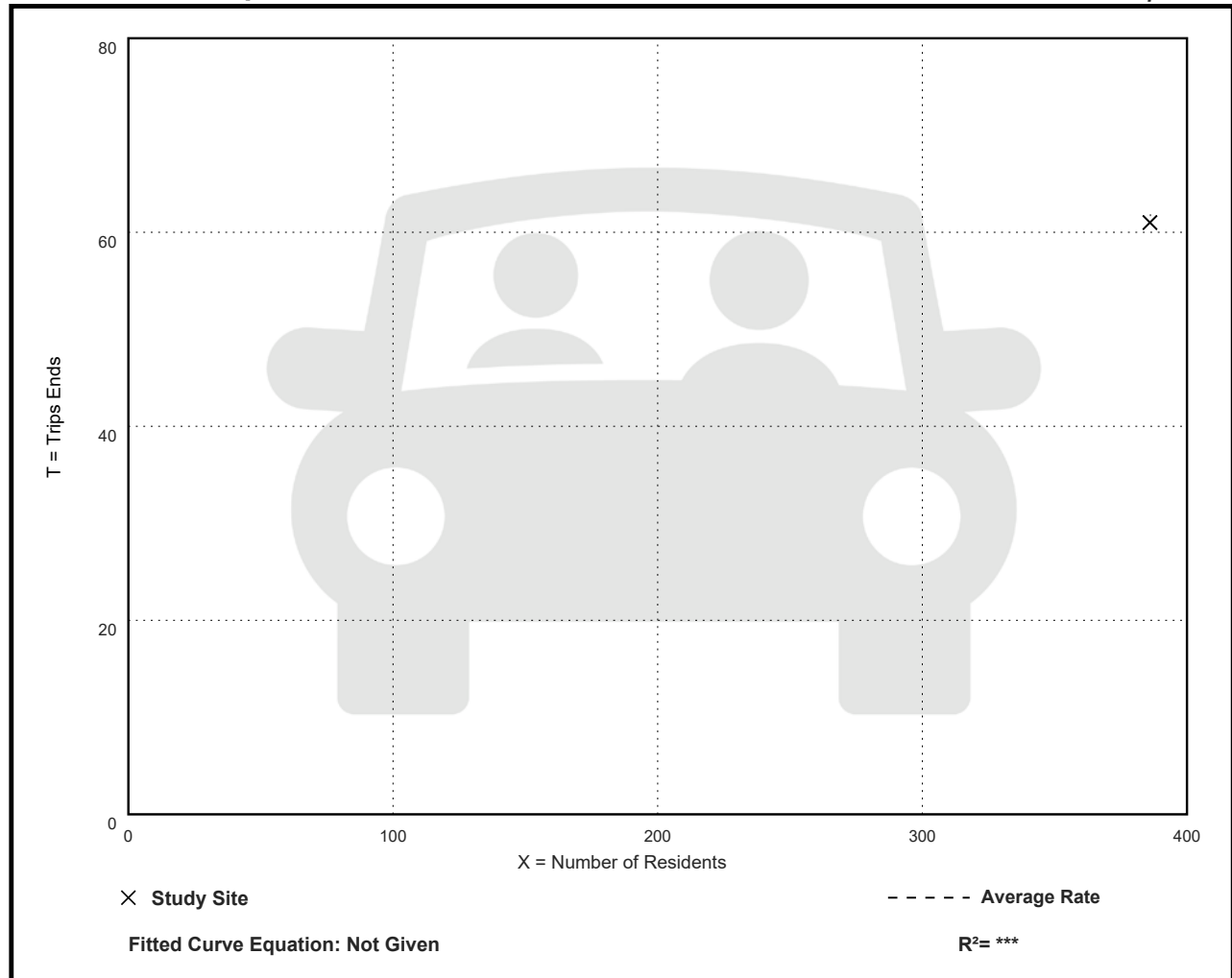
Directional Distribution: 62% entering, 38% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.16	0.16 - 0.16	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 7

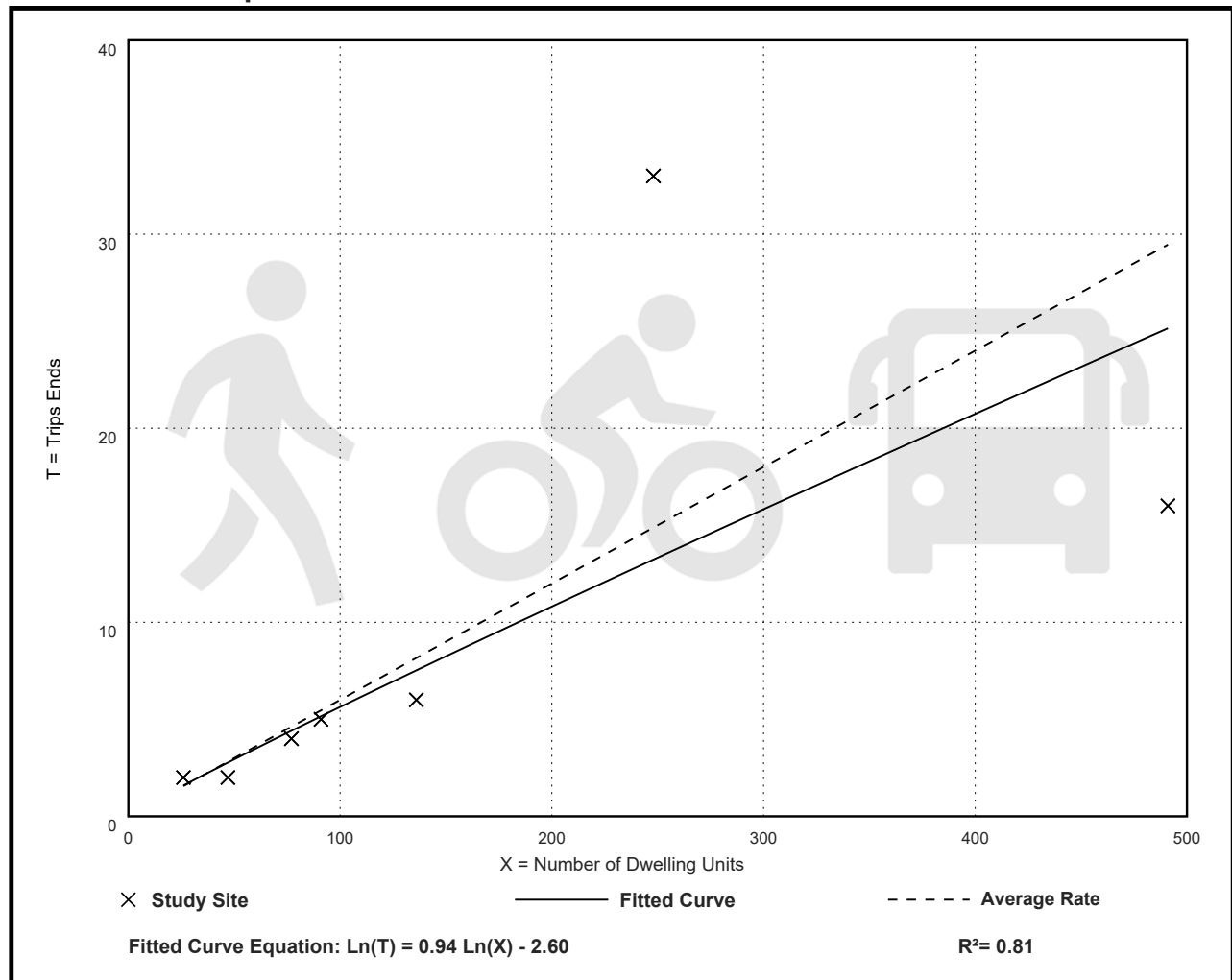
Avg. Num. of Dwelling Units: 159

Directional Distribution: 27% entering, 73% exiting

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.06	0.03 - 0.13	0.04

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 8

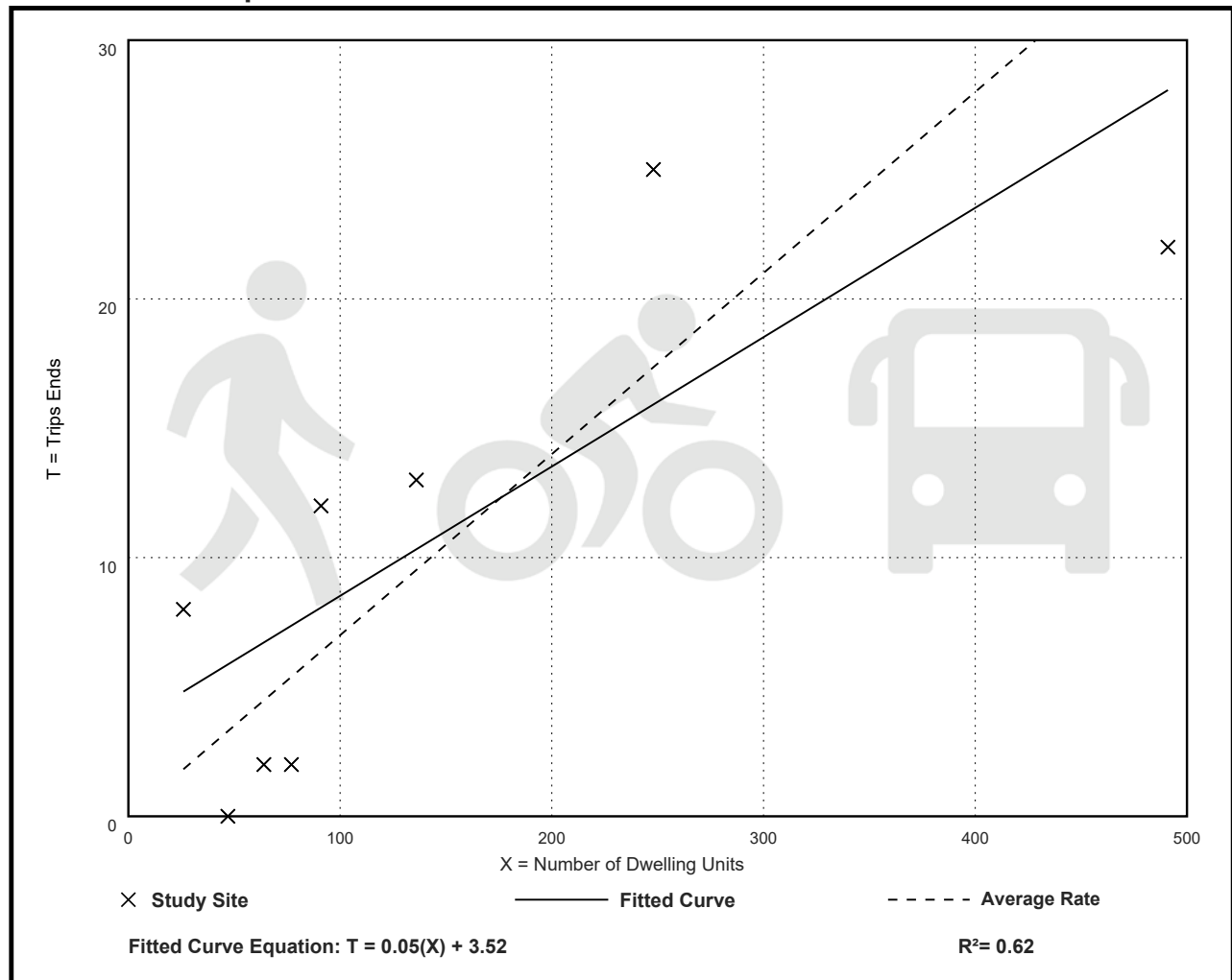
Avg. Num. of Dwelling Units: 148

Directional Distribution: 55% entering, 45% exiting

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.07	0.00 - 0.31	0.05

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

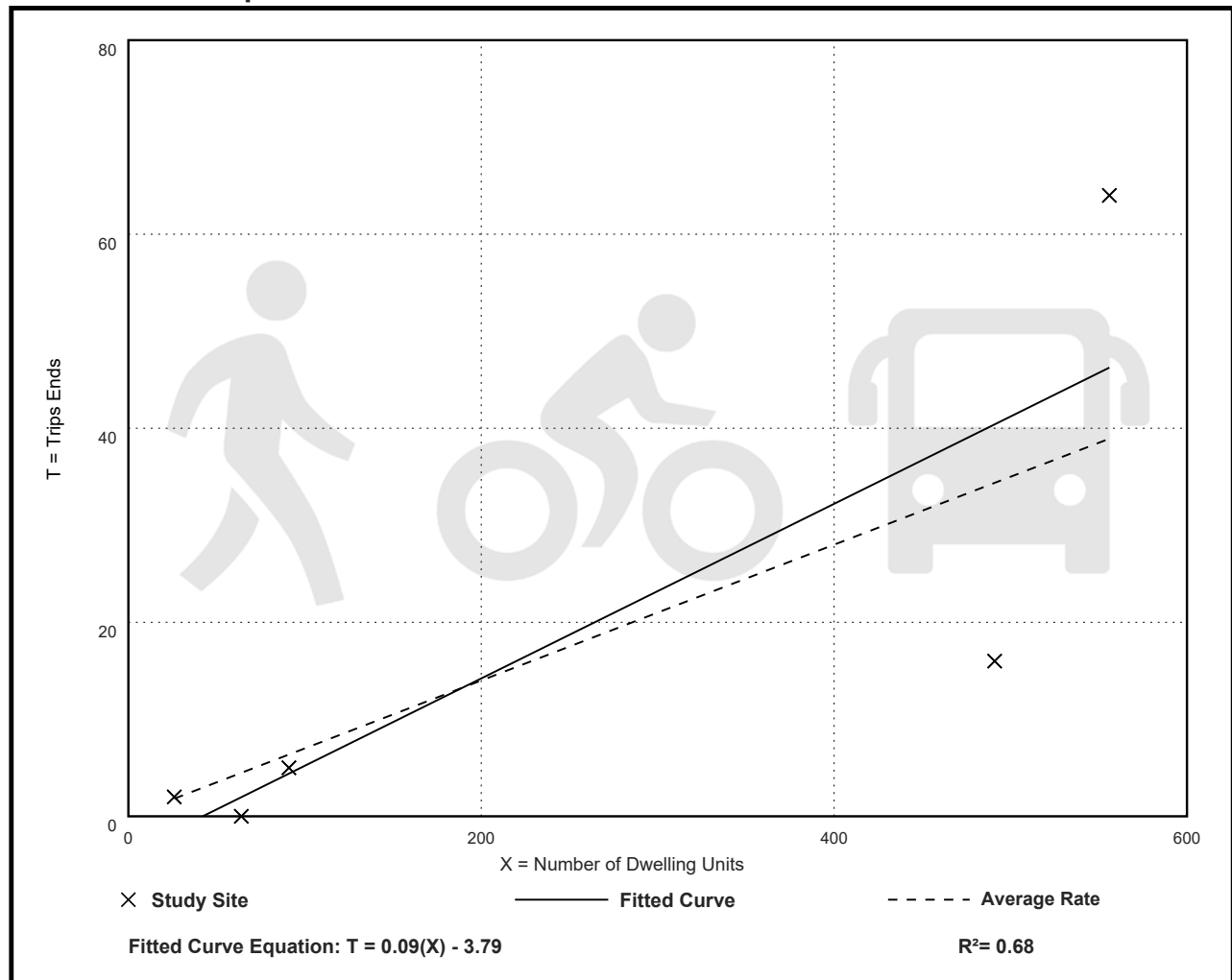
Avg. Num. of Dwelling Units: 246

Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.07	0.00 - 0.12	0.05

Data Plot and Equation



Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,
PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

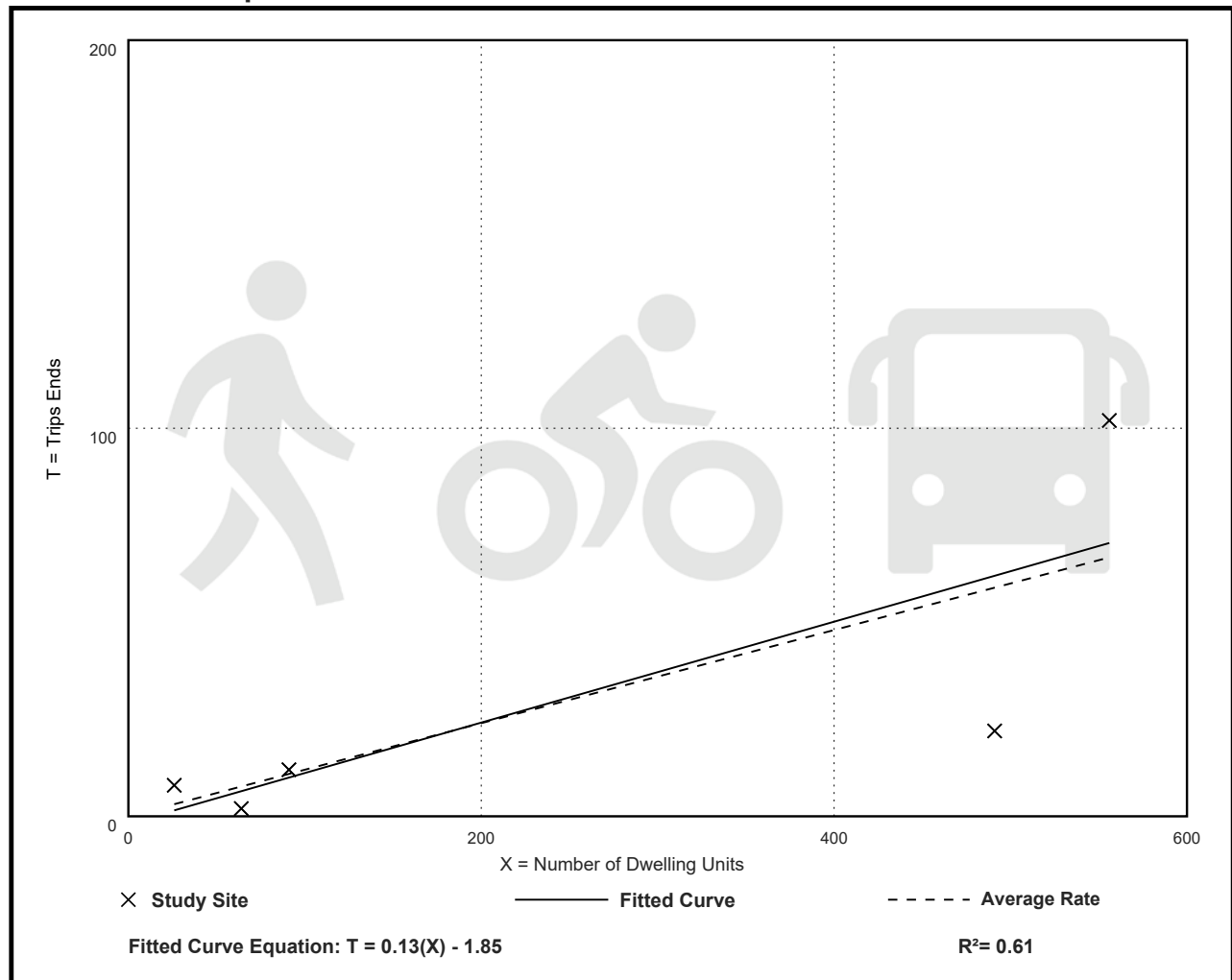
Avg. Num. of Dwelling Units: 246

Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.12	0.03 - 0.31	0.08

Data Plot and Equation



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 393

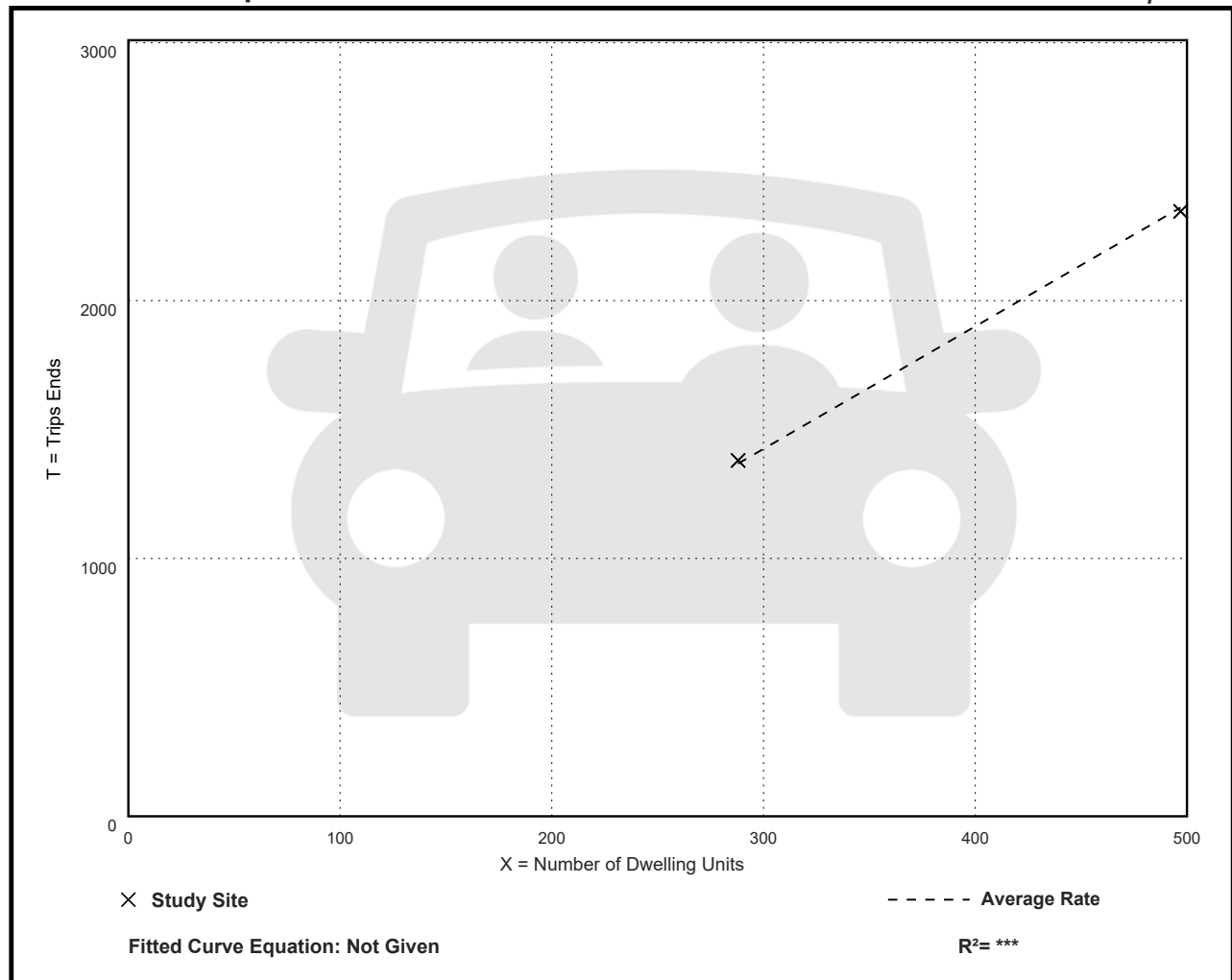
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.75	4.72 - 4.79	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 7

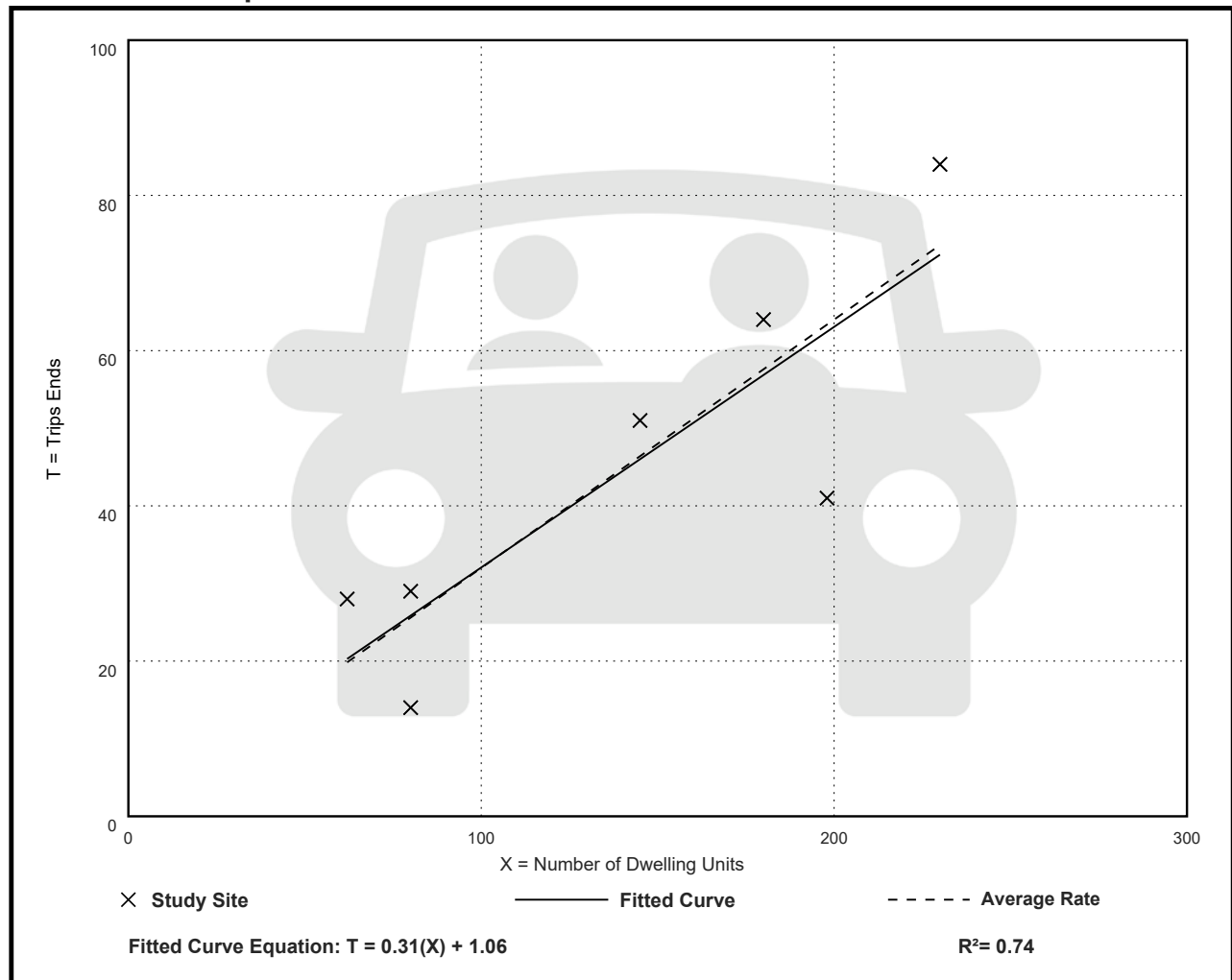
Avg. Num. of Dwelling Units: 139

Directional Distribution: 56% entering, 44% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.32	0.18 - 0.45	0.09

Data Plot and Equation



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 7

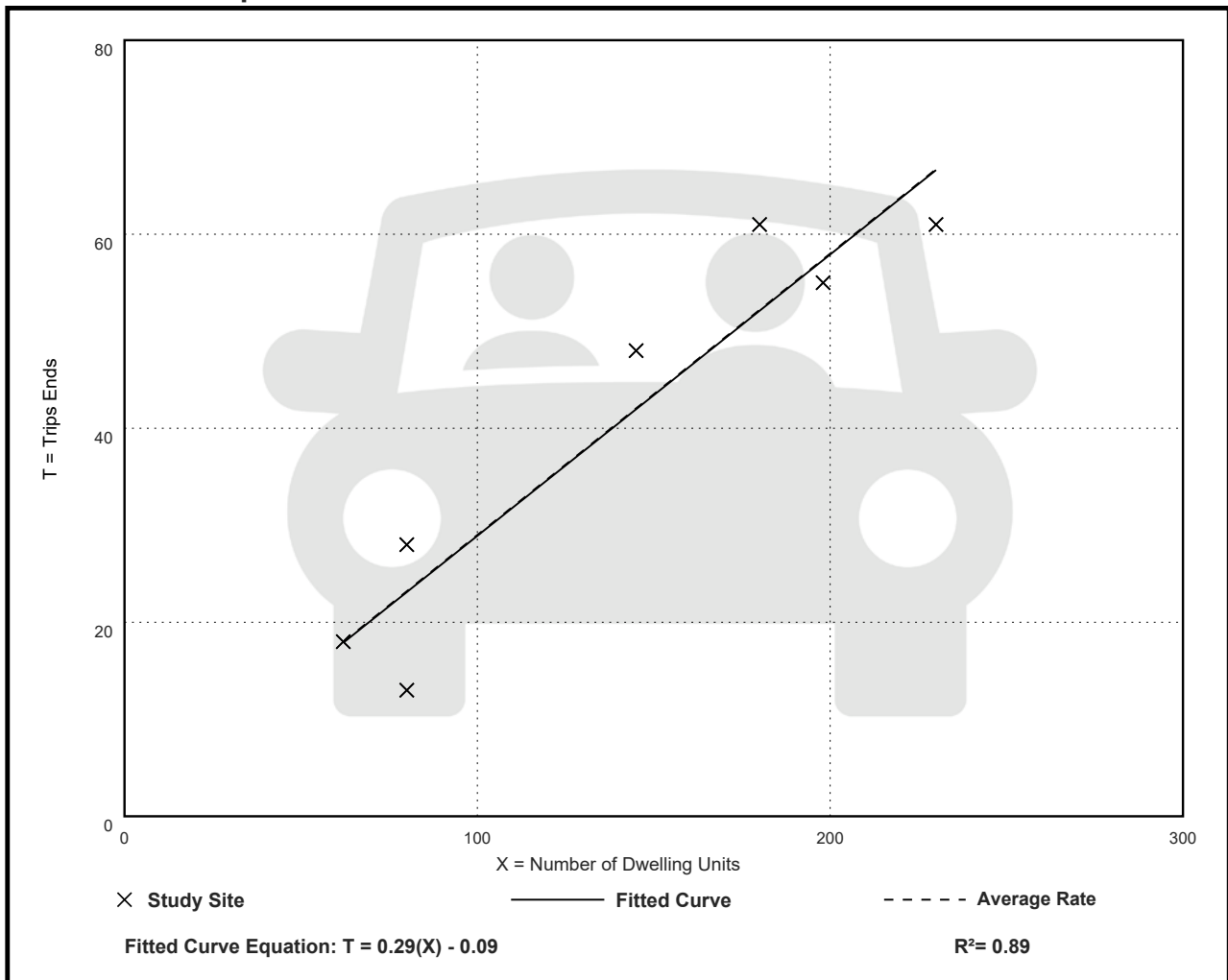
Avg. Num. of Dwelling Units: 139

Directional Distribution: 43% entering, 57% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.29	0.16 - 0.35	0.05

Data Plot and Equation



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 130

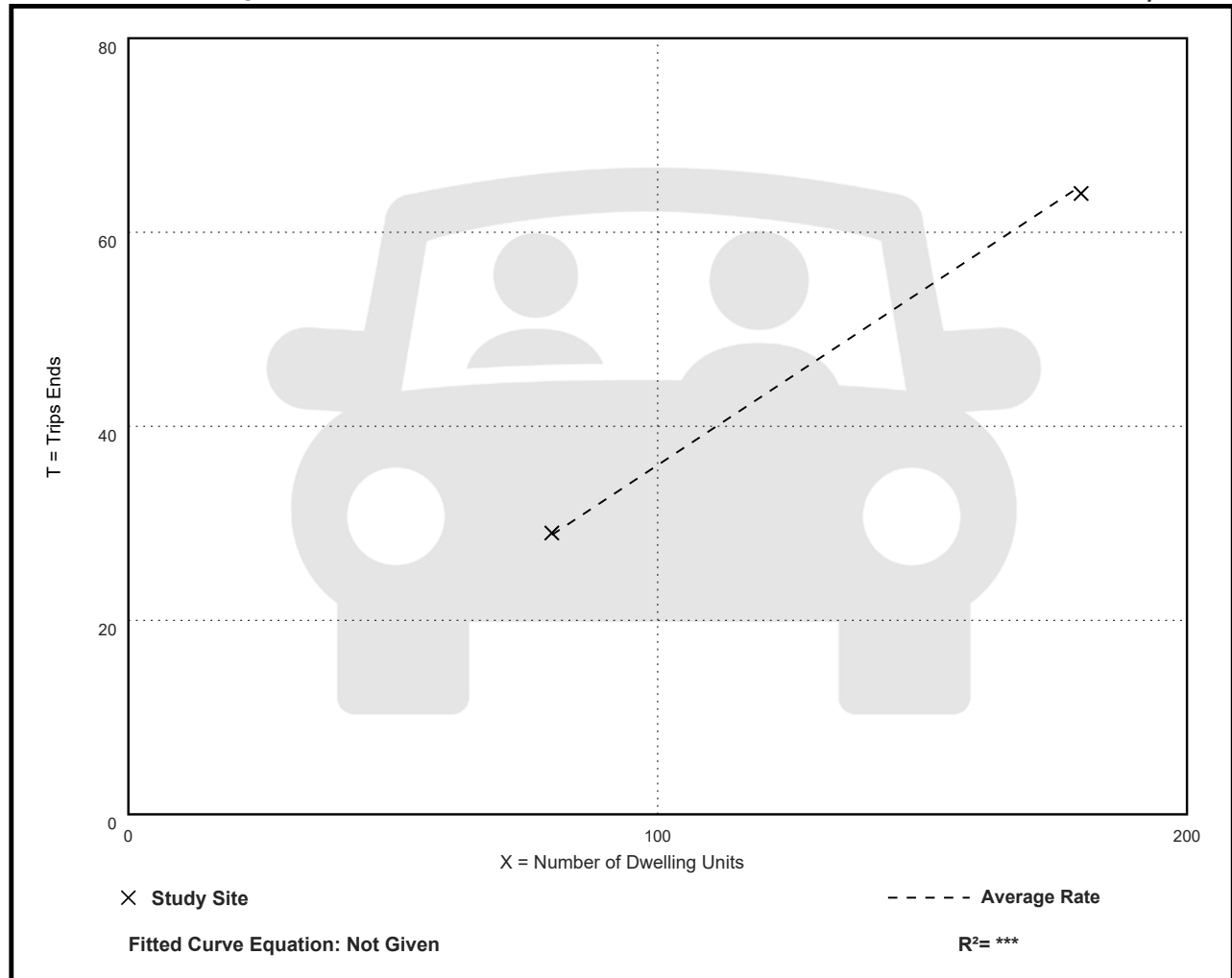
Directional Distribution: 38% entering, 62% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.36 - 0.36	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Dwelling Units: 130

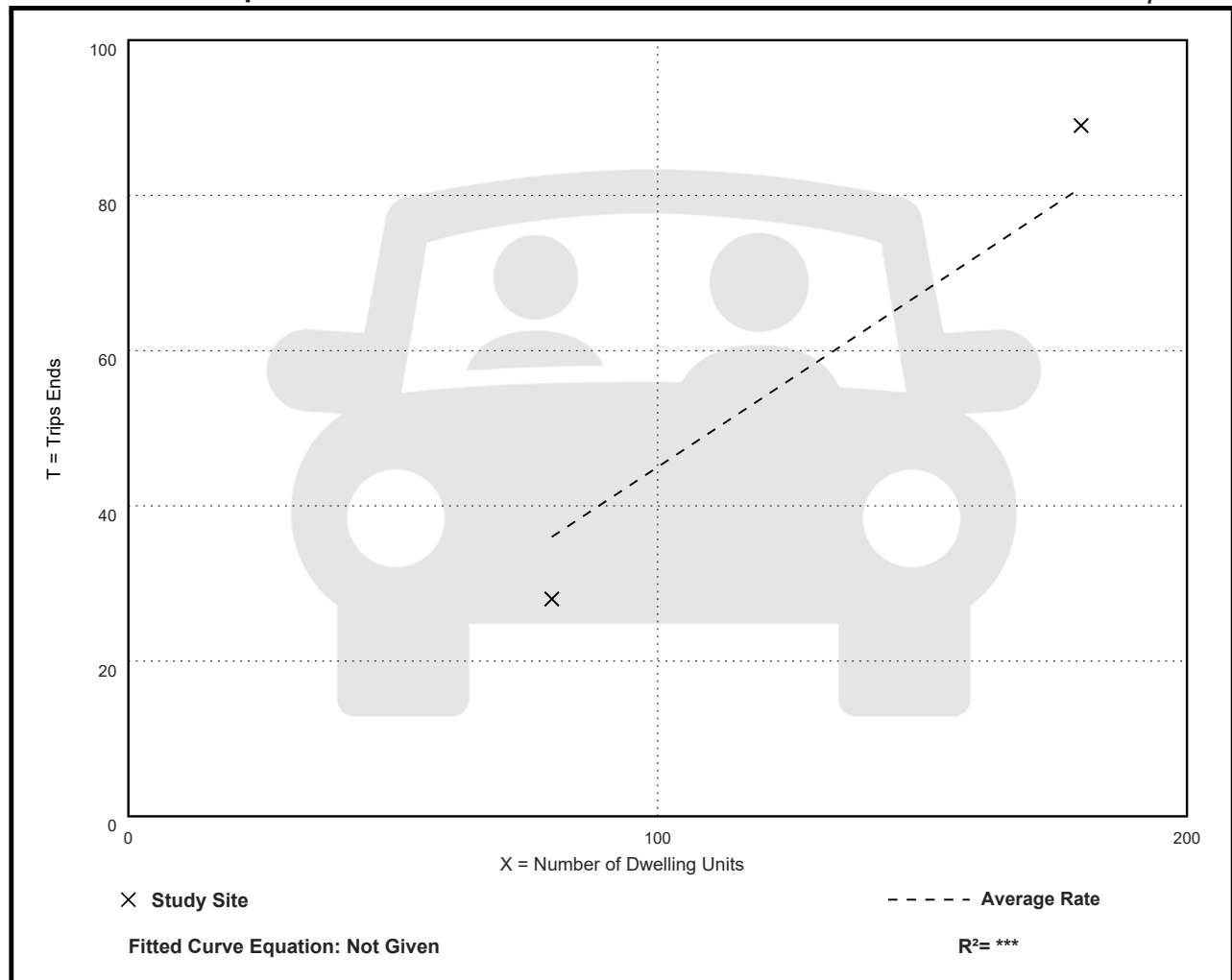
Directional Distribution: 75% entering, 25% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.45	0.35 - 0.49	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

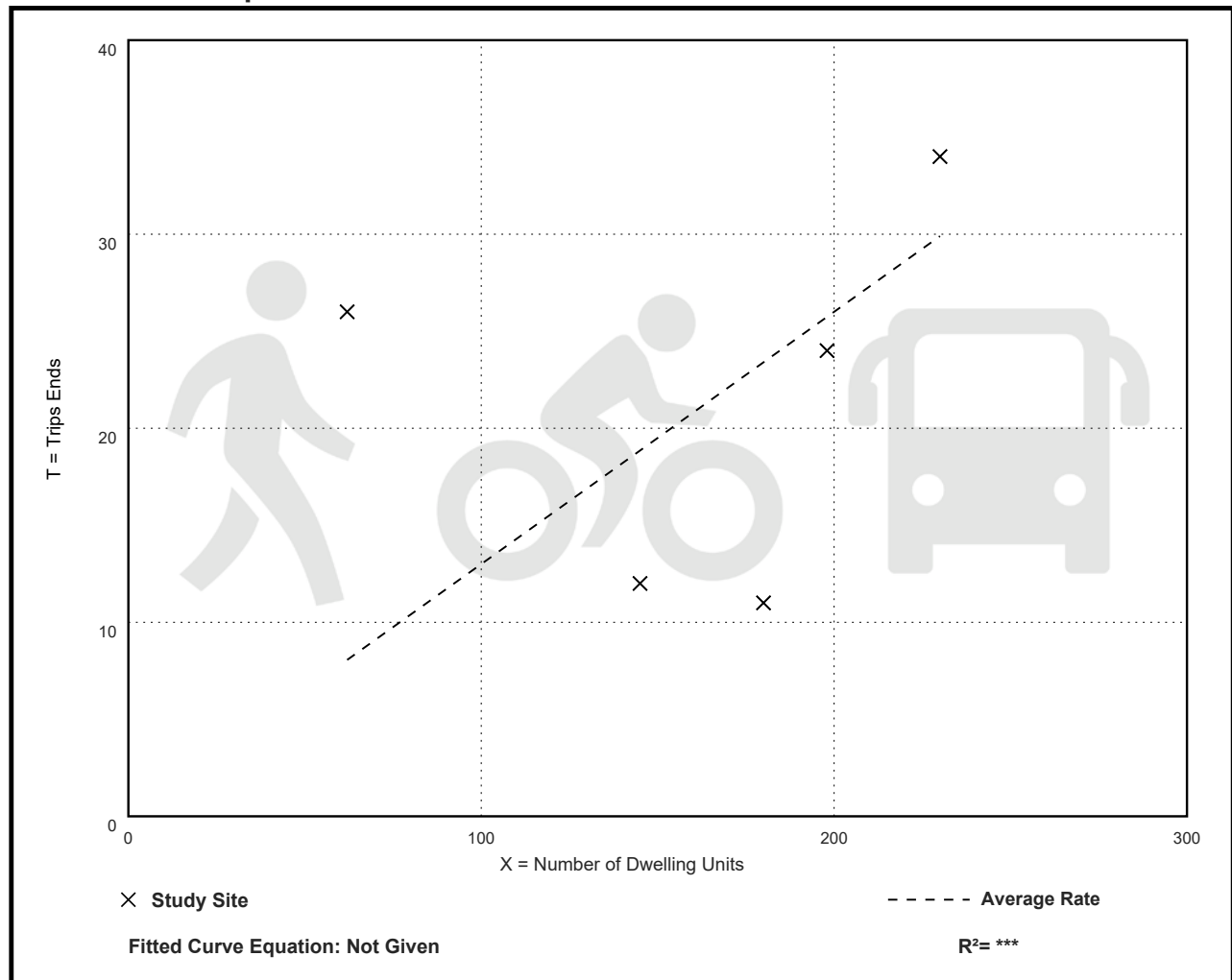
Avg. Num. of Dwelling Units: 163

Directional Distribution: 57% entering, 43% exiting

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.13	0.06 - 0.42	0.10

Data Plot and Equation



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

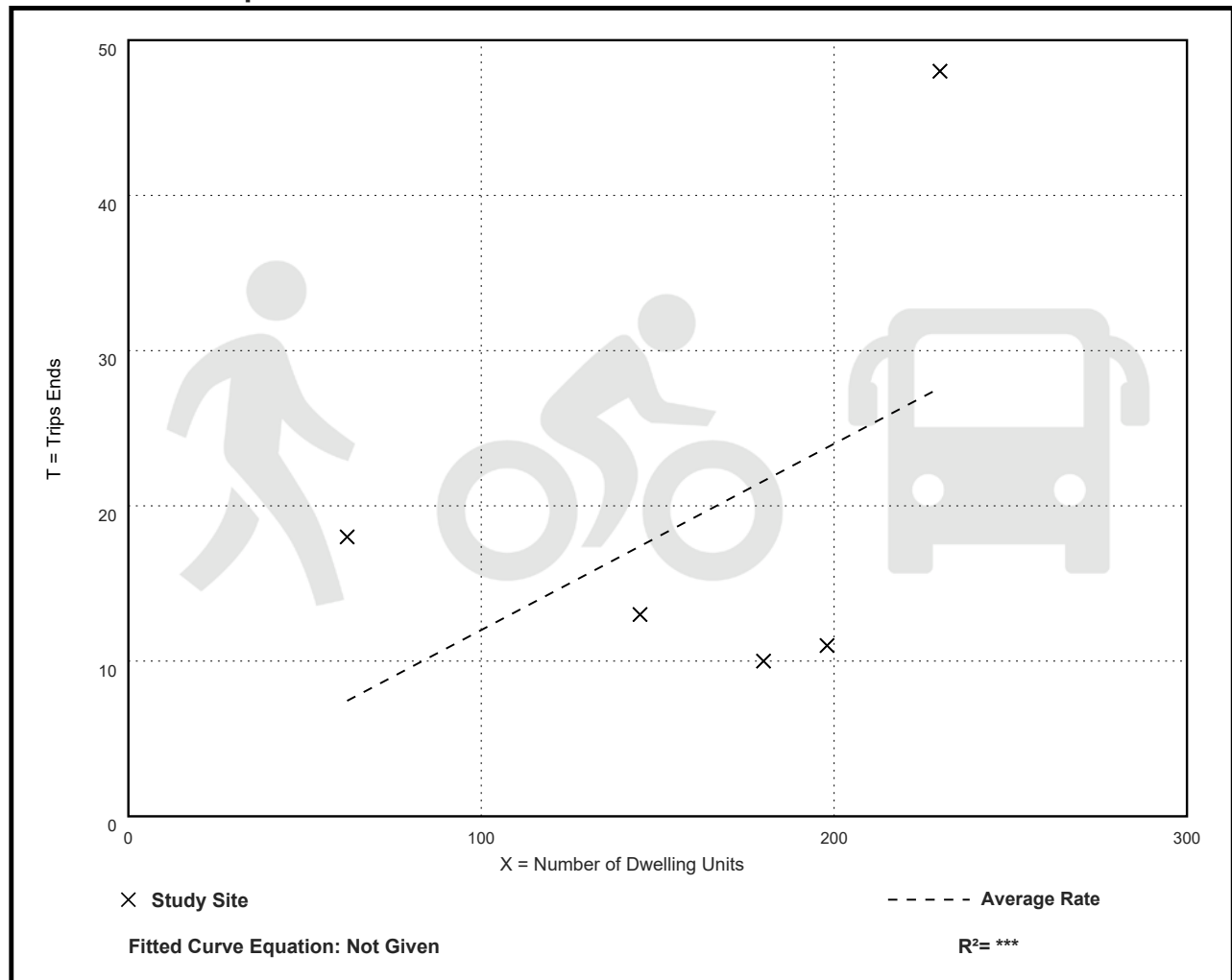
Avg. Num. of Dwelling Units: 163

Directional Distribution: 42% entering, 58% exiting

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.12	0.06 - 0.29	0.09

Data Plot and Equation



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 180

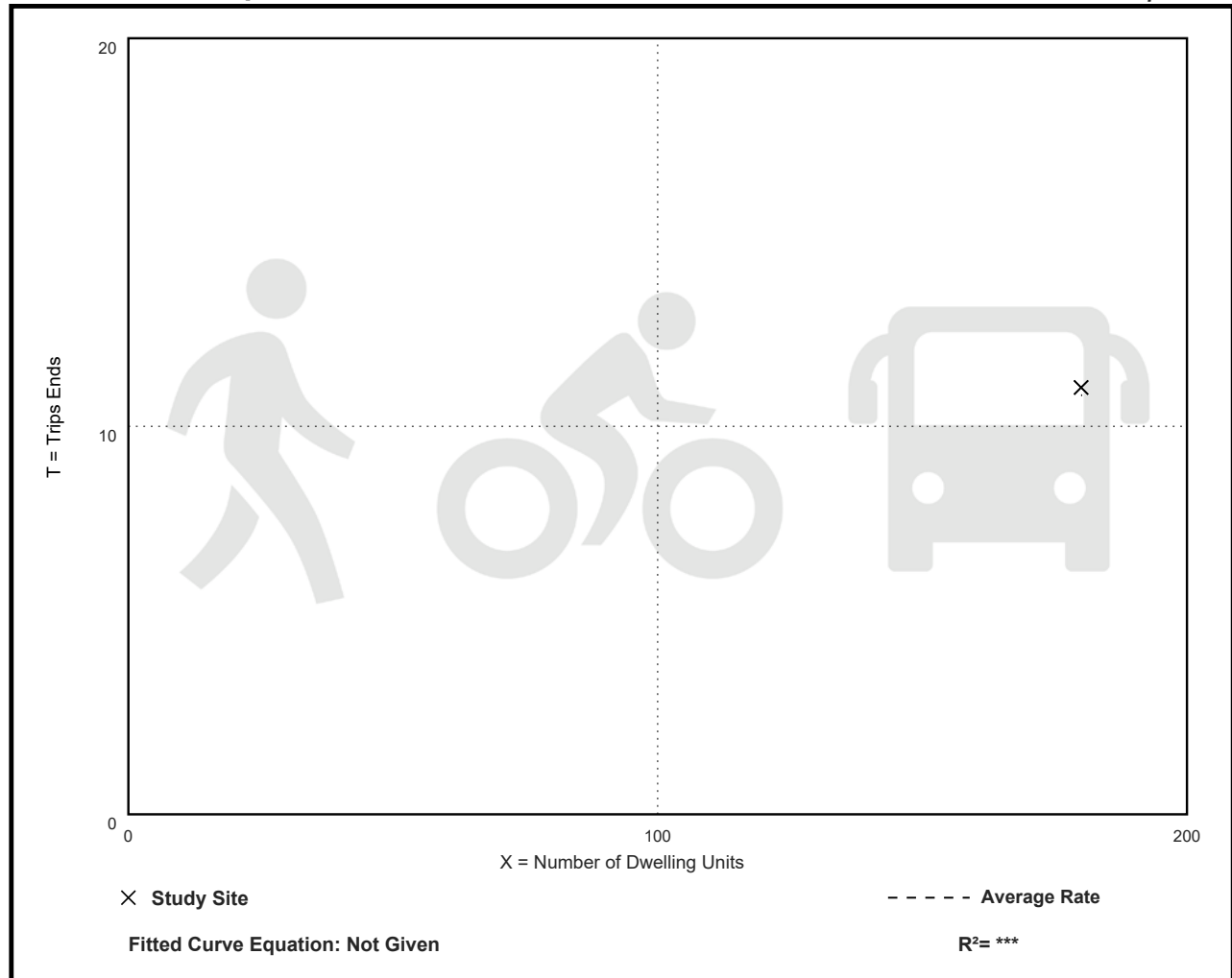
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.06	0.06 - 0.06	***

Data Plot and Equation

Caution – Small Sample Size



Multifamily Housing (Mid-Rise) Close to Rail Transit (221)

Walk+Bike+Transit Trip Ends vs: Dwelling Units

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Dwelling Units: 180

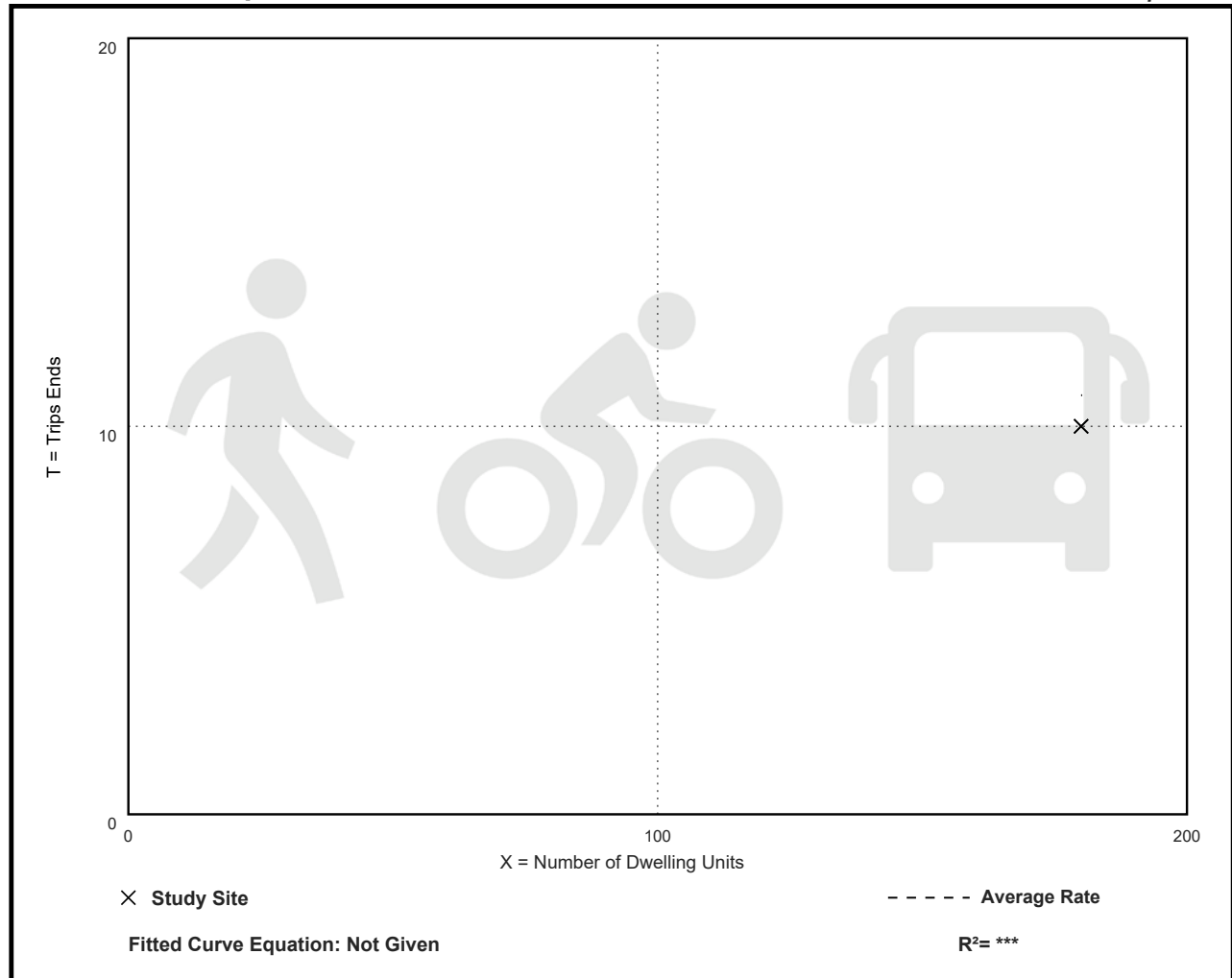
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.06	0.06 - 0.06	***

Data Plot and Equation

Caution – Small Sample Size



Land Use: 822

Strip Retail Plaza (<40k)

Description

A strip retail plaza is an integrated group of commercial establishments that is planned, developed, owned, and managed as a unit. Each study site in this land use has less than 40,000 square feet of gross leasable area (GLA). Because a strip retail plaza is open-air, the GLA is the same as the gross floor area of the building.

The 40,000 square feet GFA threshold between strip retail plaza and shopping plaza (Land Use 821) was selected based on an examination of the overall shopping center/plaza database. No shopping plaza with a supermarket as its anchor is smaller than 40,000 square feet GLA.

Shopping center (>150k) (Land use 820), shopping plaza (40-150k) (Land Use 821), and factory outlet center (Land Use 823) are related uses.

Additional Data

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Delaware, Florida, New Jersey, Ontario (CAN), South Dakota, Vermont, Washington, and Wisconsin.

Source Numbers

304, 358, 423, 428, 437, 507, 715, 728, 936, 960, 961, 974, 1009

Strip Retail Plaza (<40k) (822)

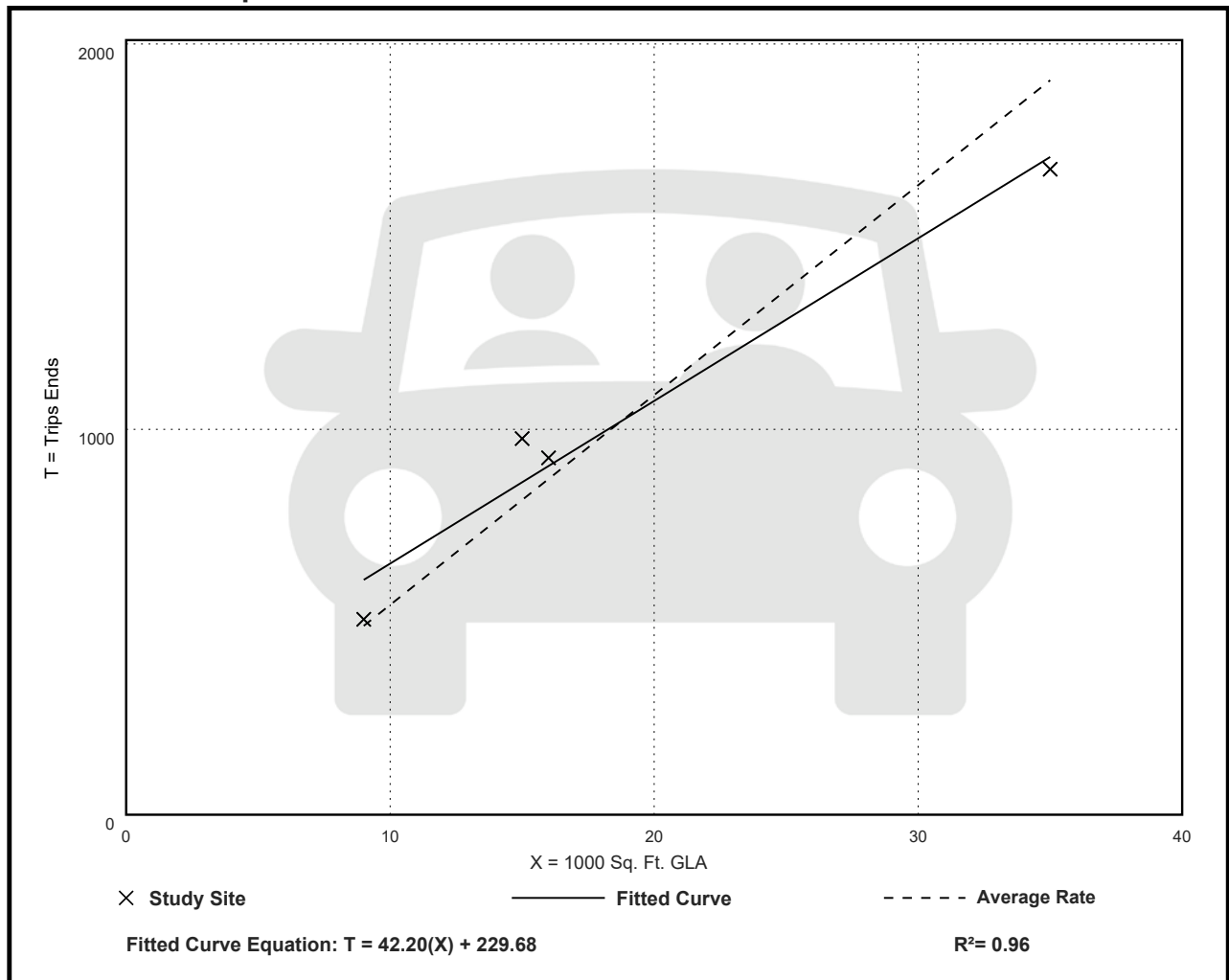
Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 4
Avg. 1000 Sq. Ft. GLA: 19
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
54.45	47.86 - 65.07	7.81

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA

On a: **Weekday,**

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

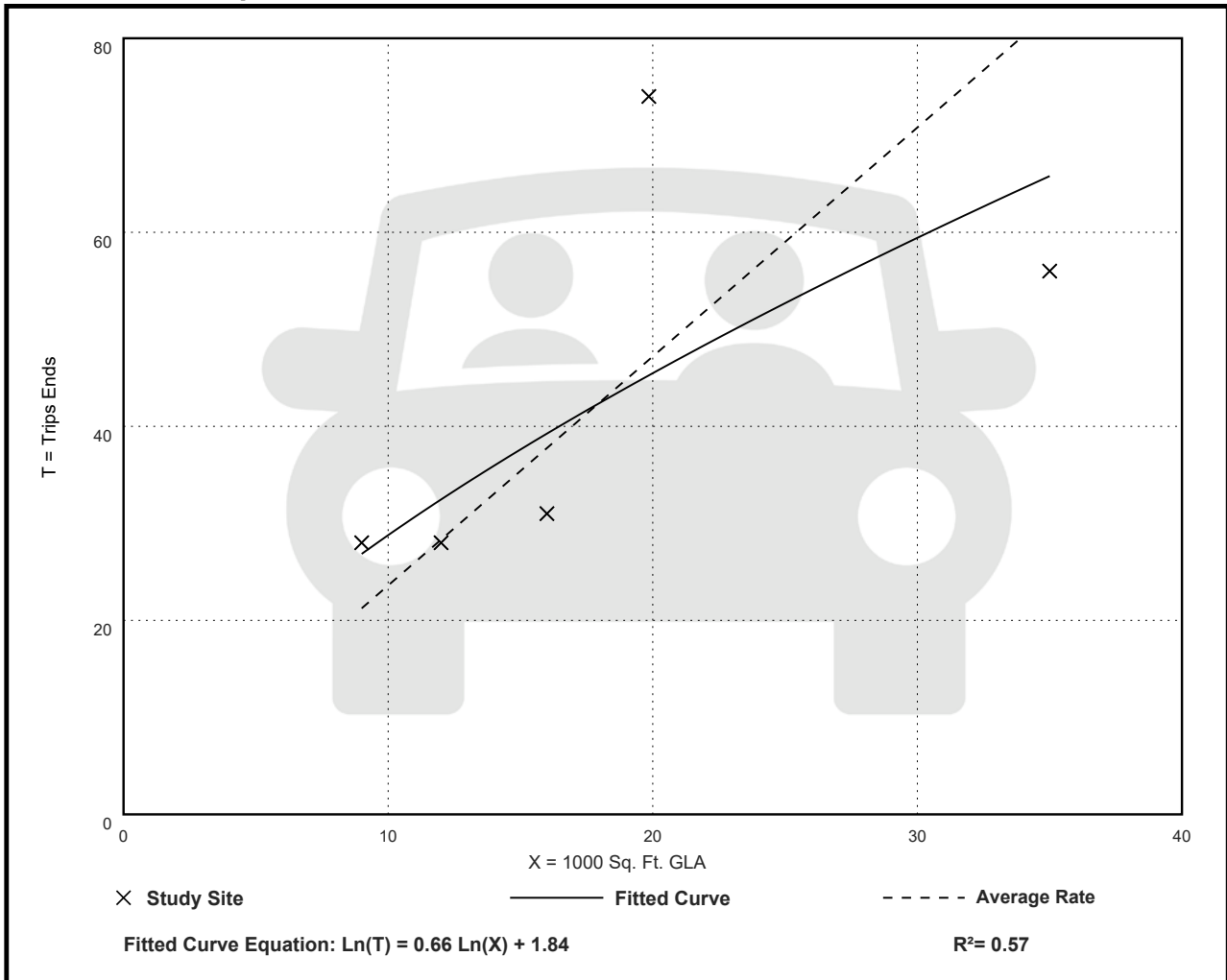
Avg. 1000 Sq. Ft. GLA: 18

Directional Distribution: 60% entering, 40% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
2.36	1.60 - 3.73	0.94

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 25

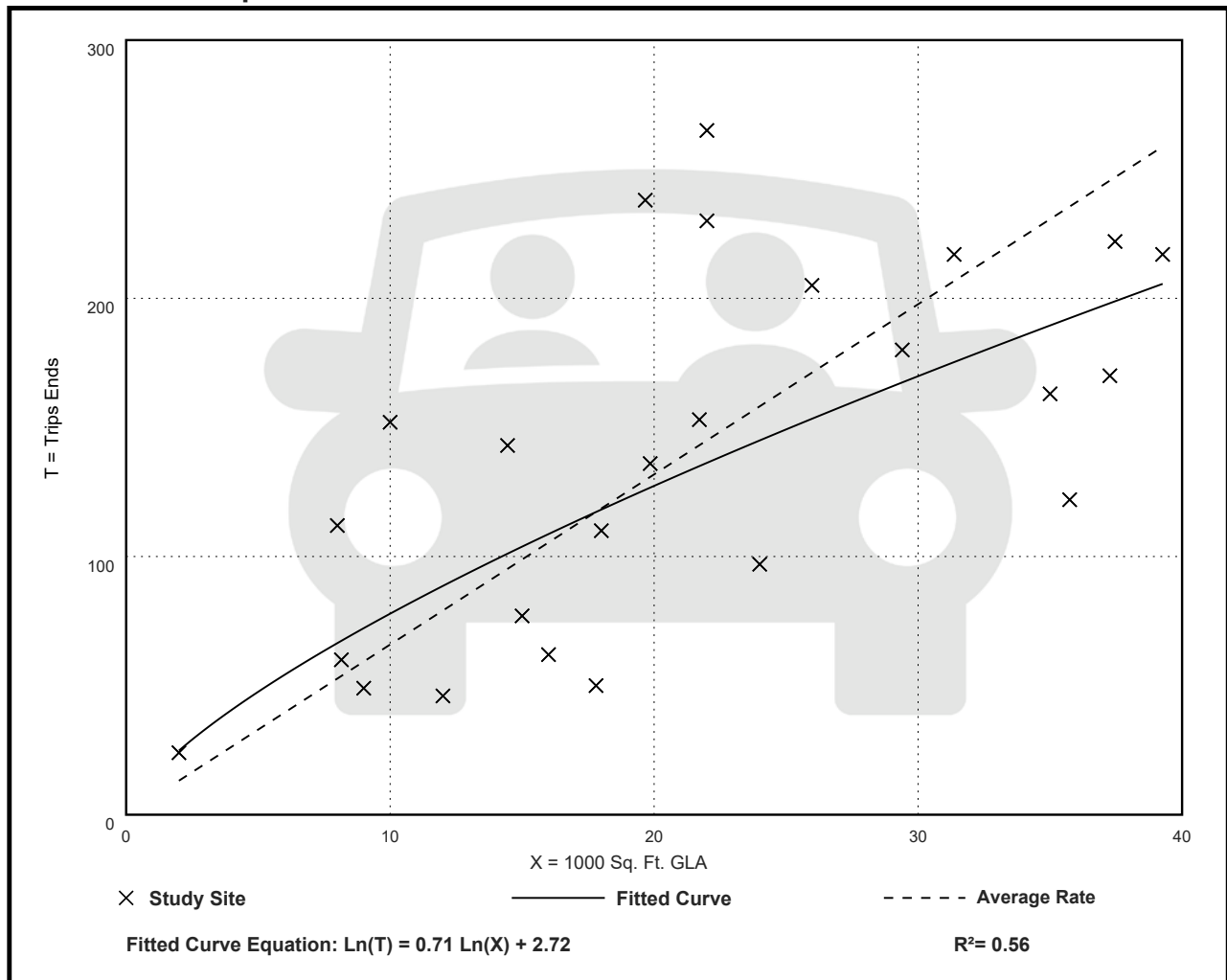
Avg. 1000 Sq. Ft. GLA: 21

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
6.59	2.81 - 15.20	2.94

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA

**On a: Weekday,
AM Peak Hour of Generator**

Setting/Location: General Urban/Suburban

Number of Studies: 6

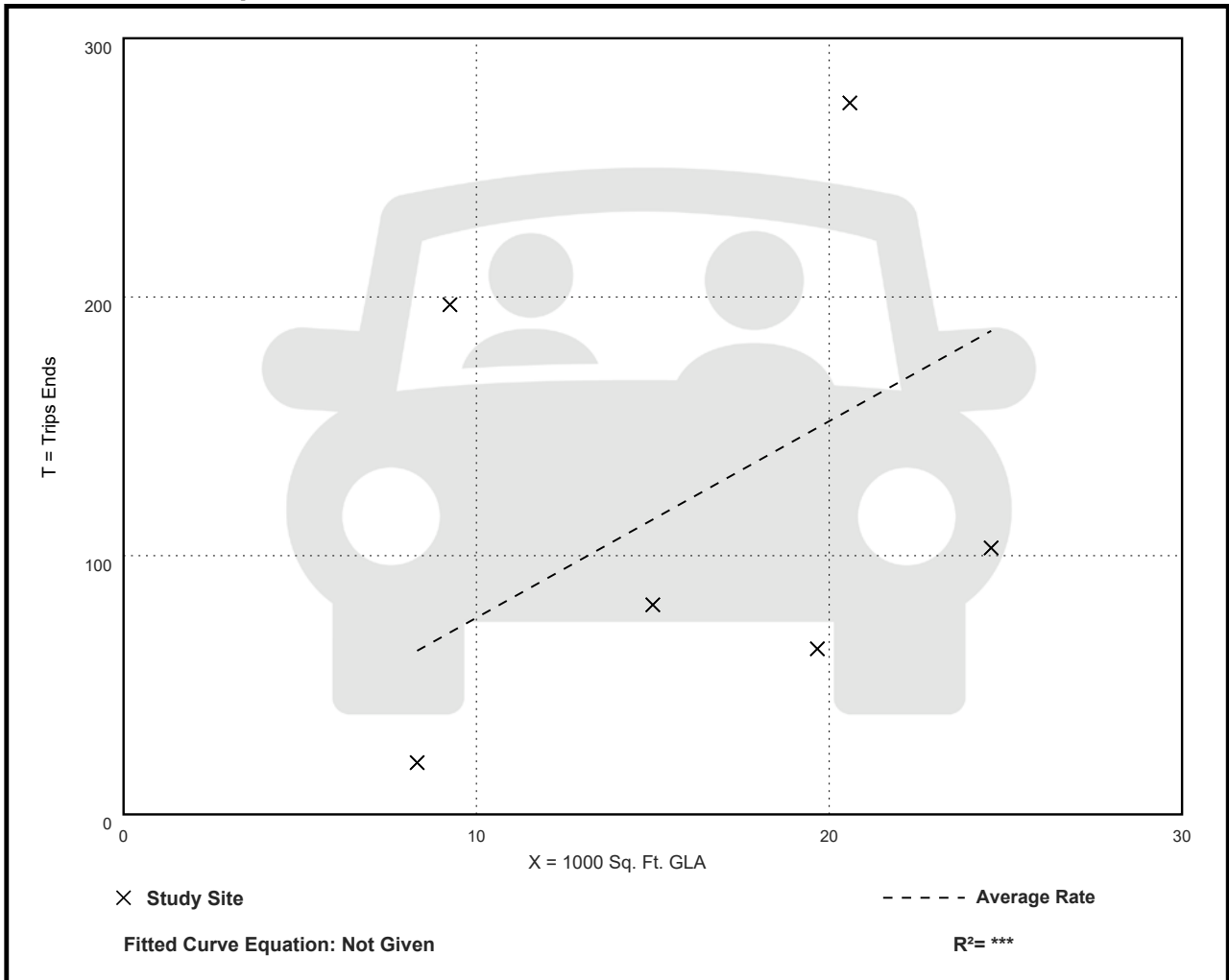
Avg. 1000 Sq. Ft. GLA: 16

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
7.60	2.40 - 21.30	6.45

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday,
PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

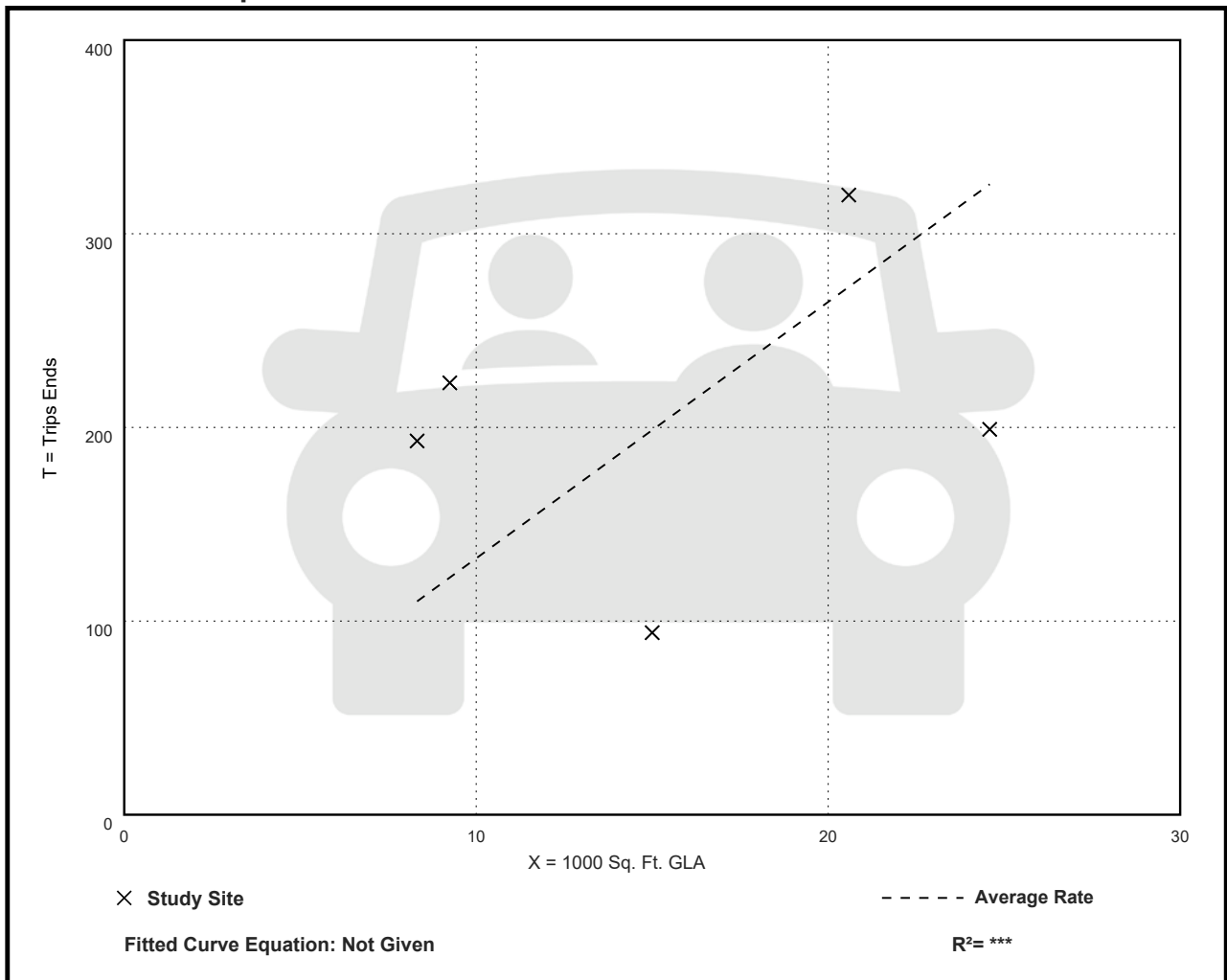
Avg. 1000 Sq. Ft. GLA: 16

Directional Distribution: 54% entering, 46% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
13.24	6.27 - 24.11	7.40

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 12

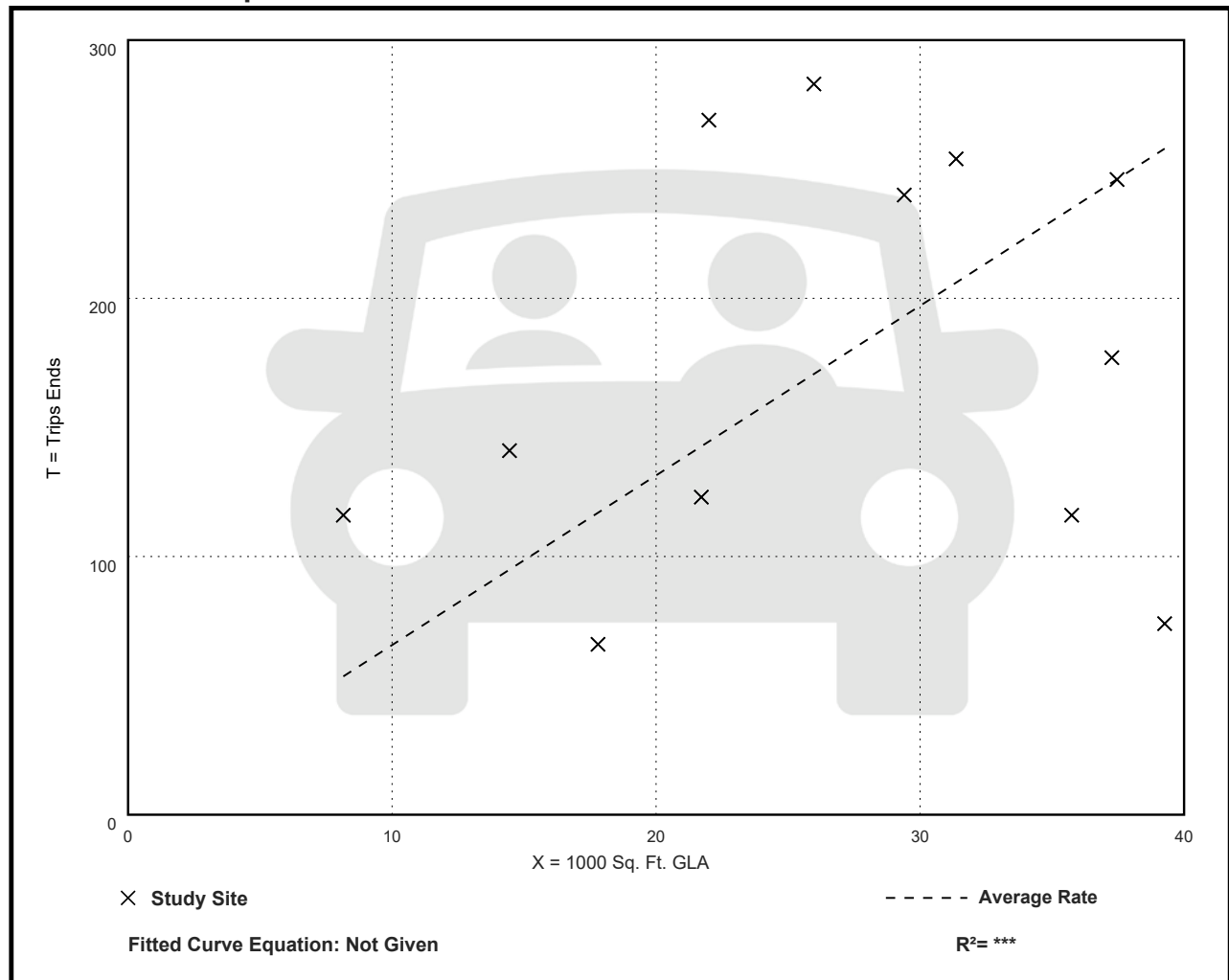
Avg. 1000 Sq. Ft. GLA: 27

Directional Distribution: 51% entering, 49% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
6.57	1.88 - 14.23	3.45

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: Employees
On a: Weekday,
AM Peak Hour of Generator

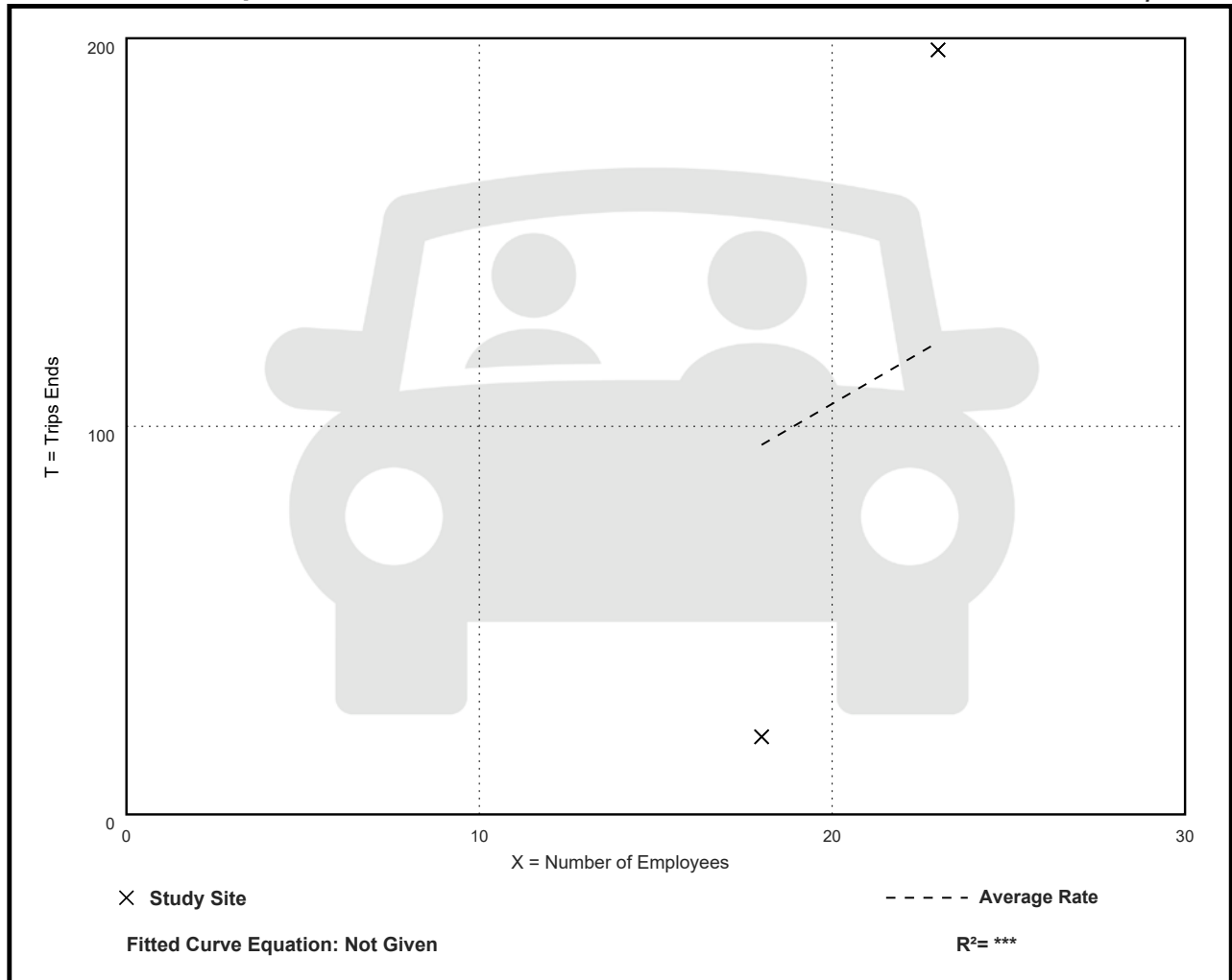
Setting/Location: General Urban/Suburban
 Number of Studies: 2
 Avg. Num. of Employees: 21
 Directional Distribution: 52% entering, 48% exiting

Vehicle Trip Generation per Employee

Average Rate	Range of Rates	Standard Deviation
5.29	1.11 - 8.57	***

Data Plot and Equation

Caution – Small Sample Size



Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: Employees

On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Employees: 21

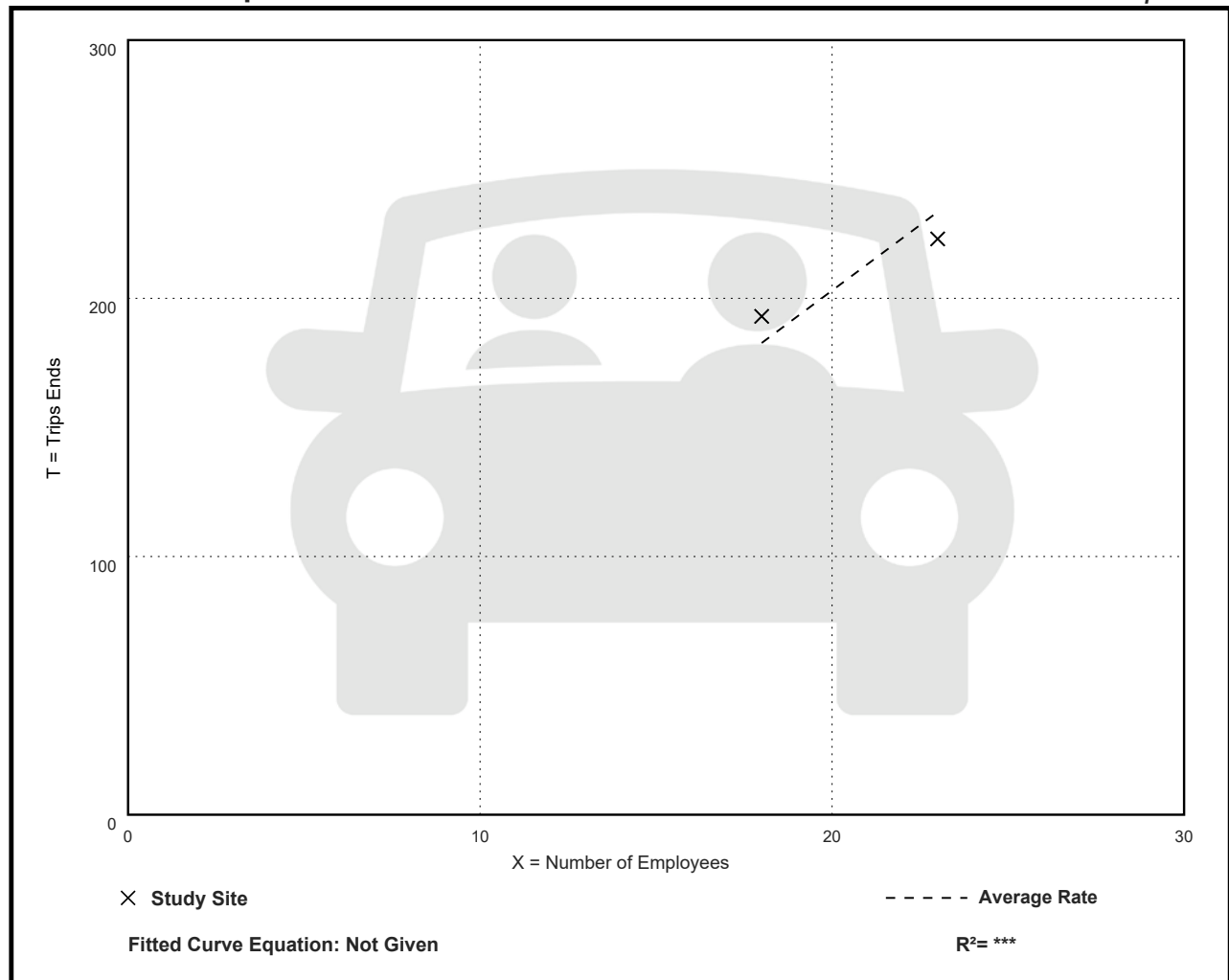
Directional Distribution: 56% entering, 44% exiting

Vehicle Trip Generation per Employee

Average Rate	Range of Rates	Standard Deviation
10.15	9.70 - 10.72	***

Data Plot and Equation

Caution – Small Sample Size



Strip Retail Plaza (<40k) (822)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GLA

On a: **Weekday,**
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. 1000 Sq. Ft. GLA: 20

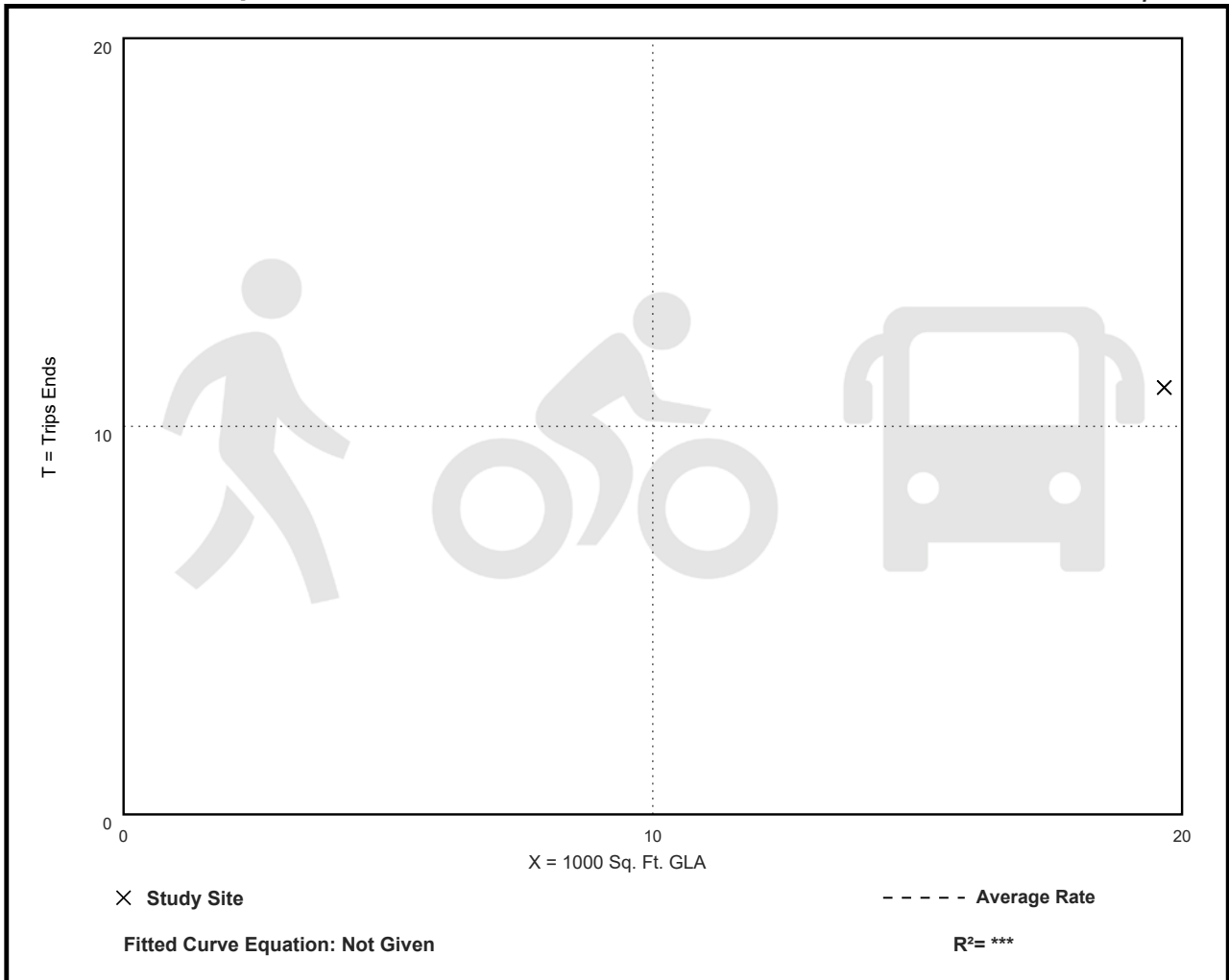
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
0.56	0.56 - 0.56	***

Data Plot and Equation

Caution – Small Sample Size



Strip Retail Plaza (<40k) (822)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GLA

On a: **Weekday,**
AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

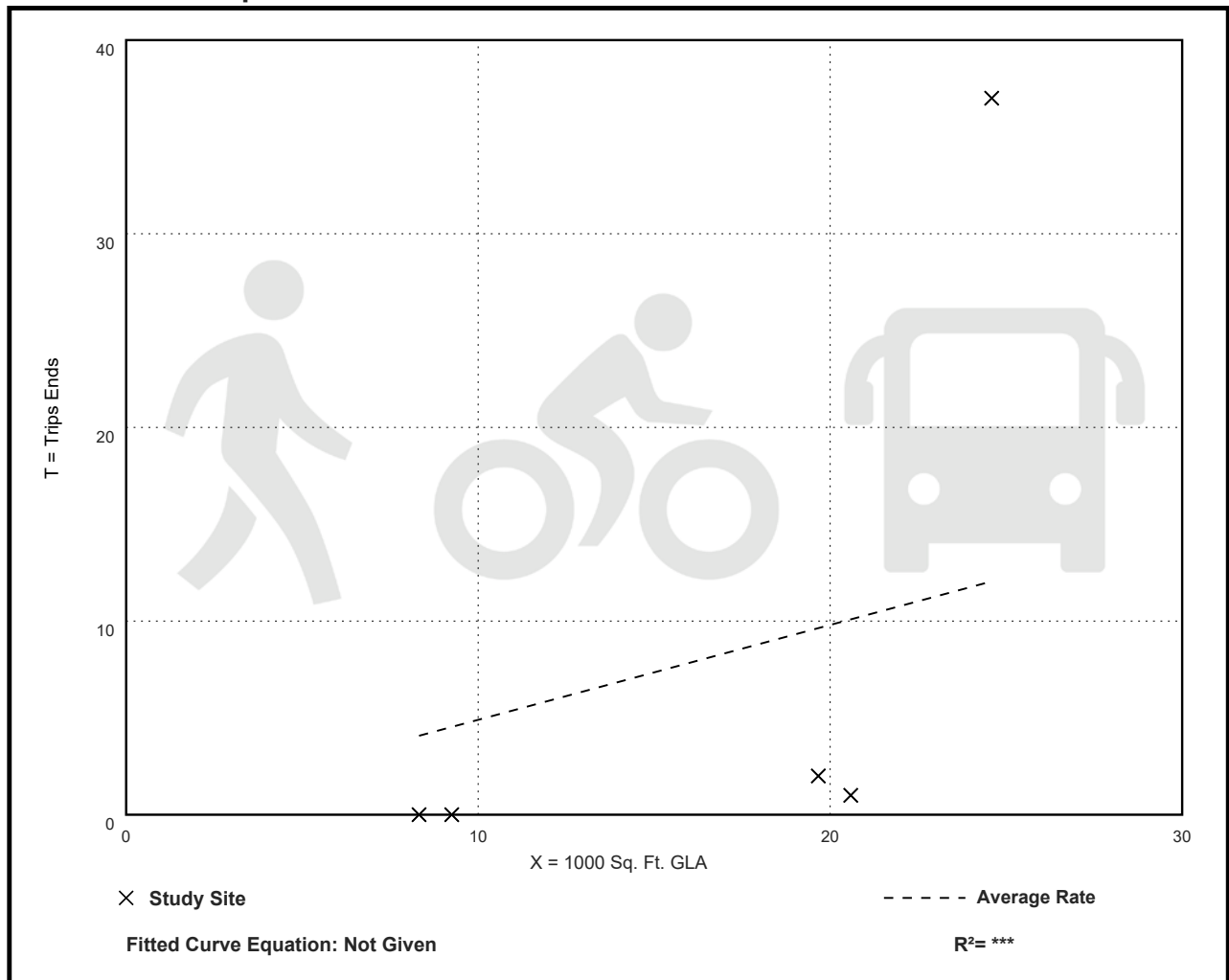
Avg. 1000 Sq. Ft. GLA: 16

Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
0.49	0.00 - 1.50	0.74

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GLA

On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

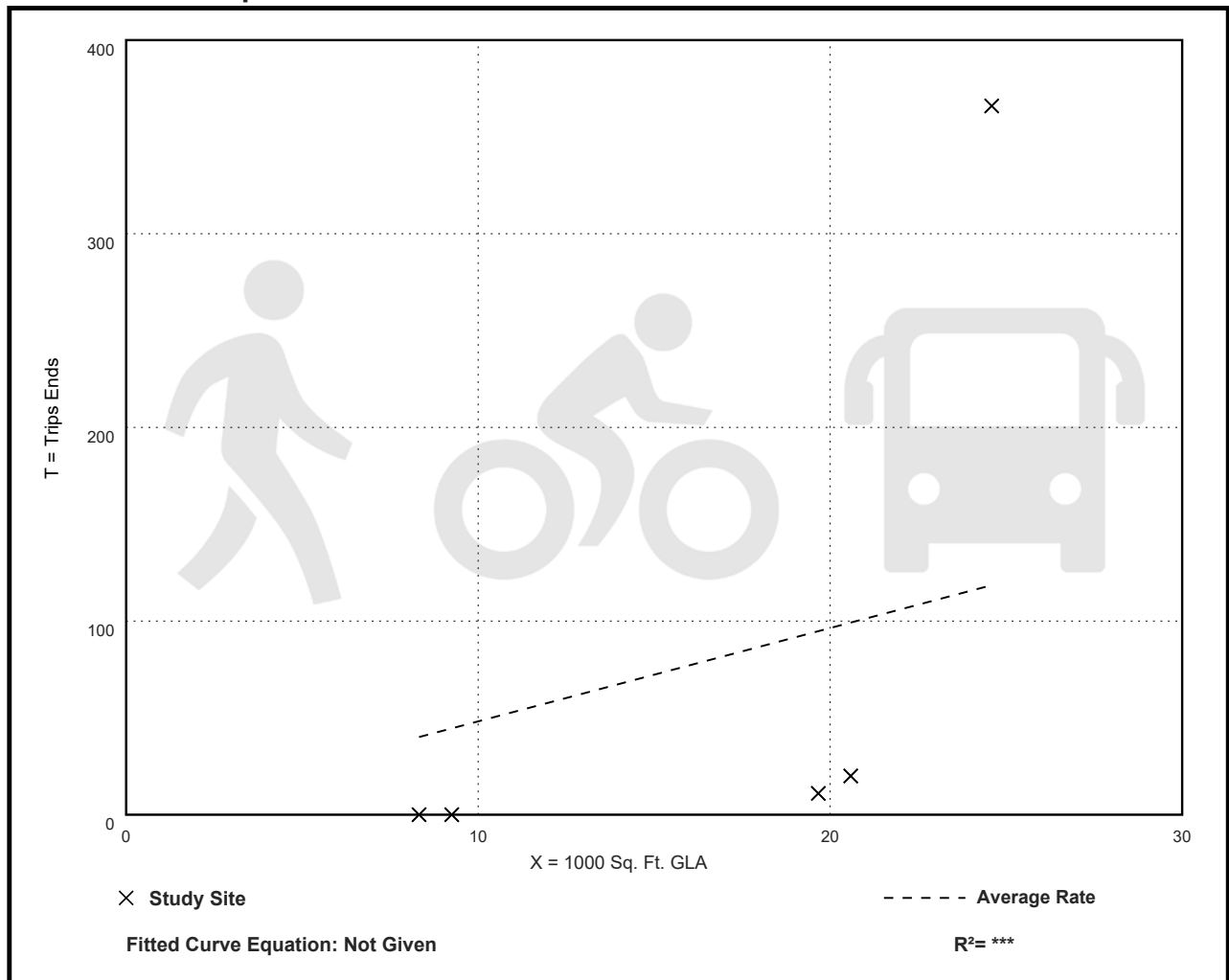
Avg. 1000 Sq. Ft. GLA: 16

Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
4.82	0.00 - 14.88	7.35

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

Walk+Bike+Transit Trip Ends vs: Employees

On a: **Weekday,**
AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Employees: 21

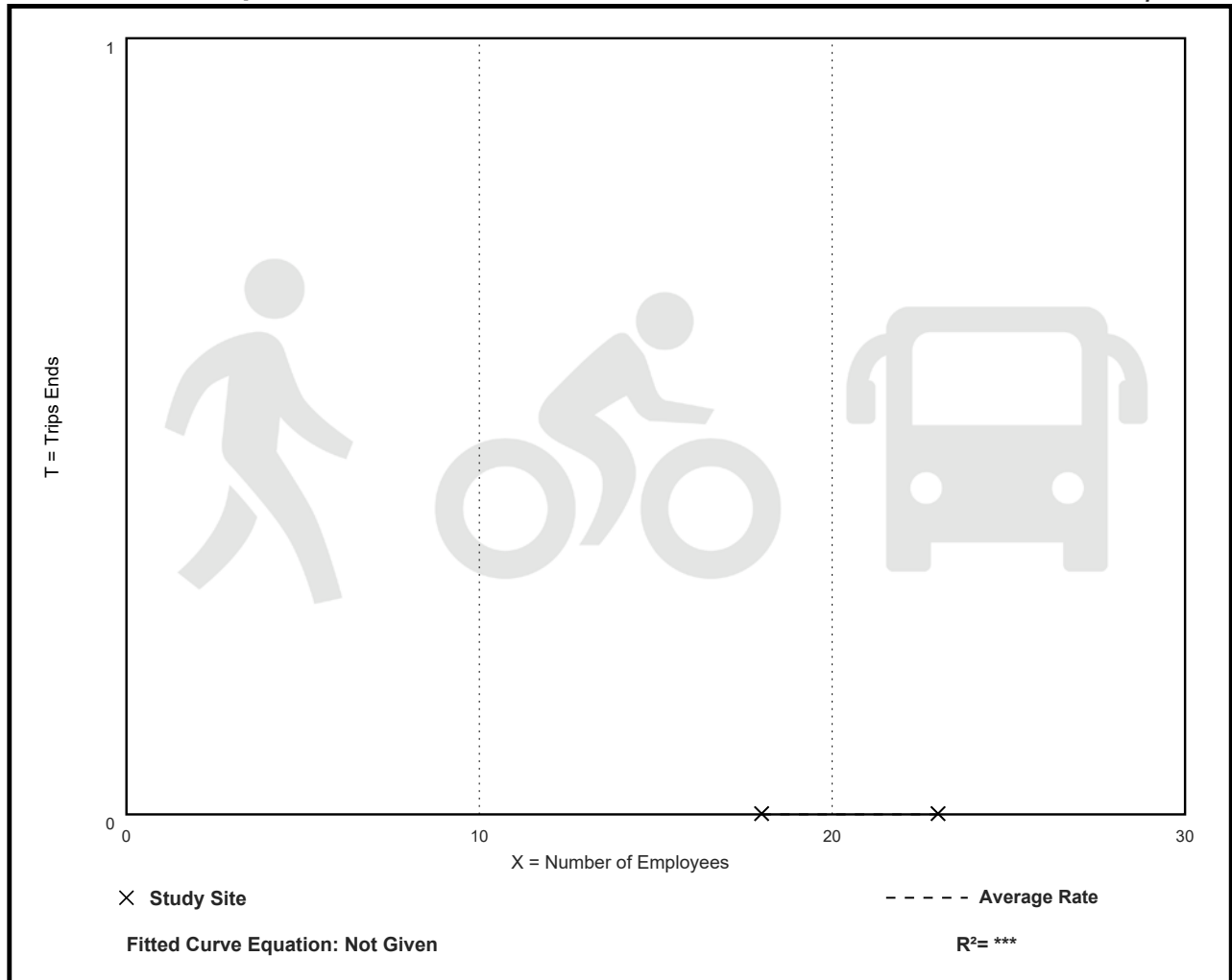
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Employee

Average Rate	Range of Rates	Standard Deviation
0.00	0.00 - 0.00	***

Data Plot and Equation

Caution – Small Sample Size



Strip Retail Plaza (<40k) (822)

Walk+Bike+Transit Trip Ends vs: Employees

On a: **Weekday,**
PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 2

Avg. Num. of Employees: 21

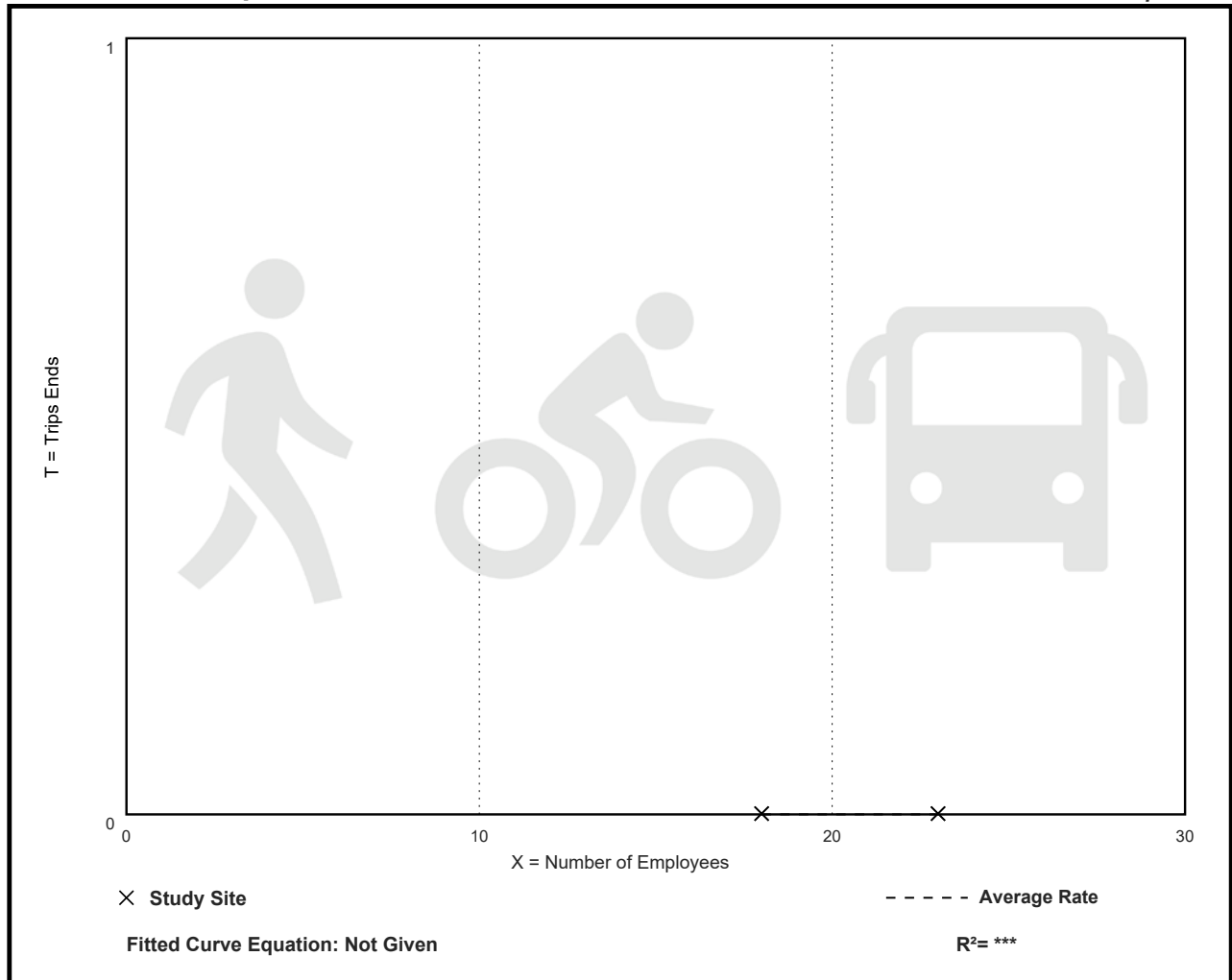
Directional Distribution: Not Available

Walk+Bike+Transit Trip Generation per Employee

Average Rate	Range of Rates	Standard Deviation
0.00	0.00 - 0.00	***

Data Plot and Equation

Caution – Small Sample Size



Martin County 2022 Roadway Level of Service Inventory Report

APPENDIX B

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
Baker Rd	SR-5	CR-723	Class II: 2-Ln Undivided	750	5,256	241	C	0.6%
Berry Ave	Golden Bear Wy	CR-714	Class II: 2-Ln Undivided	750	3,351	169	C	0.5%
Berry Ave	CR-714	Sunset Tr	Class II: 2-Ln Undivided	750	1,868	94	C	0.5%
Britt Rd	Pine Lake Dr	SR-5	Class II: 2-Ln Undivided	750	4,333	209	C	0.5%
Citrus Blvd.	CR-714 (Martin Hwy)	Port St. Lucie Blvd.	Transitional 2-Ln Uninter /Undivided Flow	1160	5,931	476	A/B	3.6%
Commerce Ave	Salerno Rd	Monroe St	Class II: 2-Ln Undivided	750	6,272	464	TCEA	0.5%
Commerce Ave	Monroe St	Indian St	Class II: 2-Ln Undivided	750	6,474	406	D	0.5%
Country Club Dr	Palm Beach County	Island Way	Class II: 2-Ln Undivided	750	2,594	144	C	0.5%
Country Club Dr	Island Way	Little Club Dr	Class II: 2-Ln Undivided	750	3,338	163	C	0.5%
County Line Rd	Little Club Dr	SR-5	Class II: 2-Ln Undivided	750	2,397	136	C	0.5%
Cove Rd	SR-76	Willoughby Blvd	Class I: 2-Ln Undivided	880	14,117	903	F	0.5%
Cove Rd	Willoughby Blvd	SR-5	Class I: 2-Ln Undivided	880	16,254	748	C	2.2%
Cove Rd	SR-5	CR-A1A	Class II: 2-Ln Undivided	750	11,234	482	TCEA	0.5%
Cove Rd	CR-A1A	End	2-Ln Undivided Non-State	675	5,463	242	TCEA	0.5%
CR-609 (Allapattah Rd)	SR-710	CR-714	Uninterrupted Rural Hwy: 2-Ln Undivided	730	2,147	109	A/B	4.6%
CR-609 (Allapattah Rd)	CR-714	St Lucie County	Uninterrupted Rural Hwy: 2-Ln Undivided	730	2,043	116	A/B	3.0%
CR-707 (Beach Rd)	Palm Beach County	CR-708	2-Ln Undivided Non-State	675	1,711	122	C	0.5%
CR-707 (Dixie Hwy)	CR-723/CR-707	CR-707 (Indian River Dr)	Class II: 2-Ln Undivided	750	5,854	242	TCEA	2.0%
CR-707 (Indian River Dr)	CR-707 (Dixie Hwy)	CR-707A (Jensen Beach Blvd.)	2-Ln Undivided Non-State	675	10,577	671	D	0.5%
CR-707 (Indian River Dr)	CR-707A	SR-732	Class II: 2-Ln Undivided	750	6,760	393	TCEA	4.4%
CR-707 (Indian River Dr)	SR-732	St. Lucie County	2-Ln Undivided Non-State	675	8,224	553	D	7.2%
CR-707A (Jensen Beach Blvd)	CR-723	Skyline Dr	Class II: 4-Ln Divided	1630	20,058	888	D	0.5%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
CR-707A (Jensen Beach Blvd)	Skyline Dr	Pineapple Way	Class II: 4-Ln Divided	1630	18,083	780	D	0.5%
CR-707A (Jensen Beach Blvd)	Pineapple Wy	CR-707	2-Ln Undivided Non-State	675	8,761	362	TCEA	0.5%
CR-708 (Bridge Rd)	SR-76	CR-711	Uninterrupted Rural Hwy: 2-Ln Undivided	730	990	57	A/B	1.8%
CR-708 (Bridge Rd)	CR-711	I-95	Uninterrupted Rural Hwy: 2-Ln Undivided	730	3,444	208	A/B	0.5%
CR-708 (Bridge Rd)	I-95	Powerline Ave	Transitional 2-Ln Uninter /Undivided Flow	1160	8,524	341	A/B	0.5%
CR-708 (Bridge Rd)	Powerline Ave	SR-5	Class I: 2-Ln Undivided	880	8,932	339	TCEA	0.5%
CR-708 (Bridge Rd)	SR-5	CR-A1A	2-Ln Undivided Non-State	675	8,340	514	TCEA	0.5%
CR-708 (Bridge Rd)	CR-A1A	Gomez Ave	2-Ln Undivided Non-State	675	8,046	378	TCEA	0.6%
CR-708 (Bridge Rd)	Gomez Ave	CR-707	Class II: 2-Ln Undivided	750	5,283	255	C	5.0%
CR-711 (Pratt Whitney Rd)	Palm Beach County	CR-708	Uninterrupted Rural Hwy: 2-Ln Undivided	730	4,283	318	C	5.9%
CR-711 (Pratt Whitney Rd)	CR-708	South Fork High School	Class I: Transitional 2-Ln Undivided	800	4,922	260	C	2.9%
CR-711 (Pratt Whitney Rd)	South Fork High School	SR-76	Class I: Transitional 2-Ln Undivided	800	6,092	291	C	2.0%
CR-713 (High Meadow Ave)	I-95	CR-714	2-Ln Uninter /Undivided Flow	1200	13,990	963	D	1.4%
CR-713 (High Meadow Ave)	CR-714	SR-714	Class I: 2-Ln Undivided	880	10,961	585	C	0.5%
CR-713 (High Meadow Ave)	SR-714	Murphy Rd	2-Ln Uninter /Undivided Flow	1200	11,737	713	C	0.5%
CR-714 (Martin Hwy)	SR-710	Fox Brown Rd	Uninterrupted Rural Hwy: 2-Ln Undivided	730	3,884	181	A/B	5.2%
CR-714 (Martin Hwy)	Fox Brown Rd	CR-609	Uninterrupted Rural Hwy: 2-Ln Undivided	730	4,301	183	A/B	6.0%
CR-714 (Martin Hwy)	CR-609	I-95	Uninterrupted Rural Hwy: 2-Ln Undivided	730	6,598	301	C	5.6%
CR-714 (Martin Hwy)	Florida's Turnpike	CR-713	Class I: 4-Ln Divided	2000	22,061	1,456	C	1.3%
CR-714 (Martin Hwy)	CR-713	Mapp Rd	Class I: 4-Ln Divided	2000	23,197	1,351	C	0.9%
CR-714 (Veteran's Memorial Bridge)	Mapp Rd	SR-76	Class I: 4-Ln Divided	2000	24,463	1,518	C	1.2%
CR-723 (Savanna Rd)	CR-707	NE 24th St	Class I: 2-Ln Undivided	880	8,575	389	C	0.5%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
CR-723 (Savanna Rd)	NE 24th St	CR-707A (Jensen Beach Blvd.)	Class I: 2-Ln Undivided	880	8,982	430	C	0.5%
CR-726 (Citrus Blvd)	SR-710	Greenridge Ln	Uninterrupted Rural Hwy: 2-Ln Undivided	730	2,724	124	A/B	0.5%
CR-726 (Citrus Blvd)	Greenridge Ln	CR-76A	Uninterrupted Rural Hwy: 2-Ln Undivided	730	2,748	136	A/B	1.9%
CR-76A (Citrus Blvd.)	CR-726	SR-714	Transitional 2-Ln Uninter /Undivided Flow	1160	4,588	223	A/B	2.2%
CR-76A (SW 96th St)	CR-726	Pennsylvania Ave	Transitional 2-Ln Uninter /Undivided Flow	1160	4,380	255	A/B	3.8%
CR-76A (SW 96th St)	Pennsylvania Ave	SR-76	Class I: Transitional 2-Ln Undivided	800	8,677	405	C	1.7%
CR-A1A (Dixie Hwy)	SR-5	CR-708	Class II: 2-Ln Undivided	750	4,200	216	TCEA	6.8%
CR-A1A (Dixie Hwy)	CR-708	Osprey St	Class I: 2-Ln Undivided	880	7,495	487	TCEA	0.5%
CR-A1A (Dixie Hwy)	Osprey St	Heritage Blvd	2-Ln Uninter /Undivided Flow	1200	6,843	313	A/B	0.8%
CR-A1A (Dixie Hwy)	Heritage Blvd	Cove Rd	2-Ln Uninter /Undivided Flow	1200	7,401	341	A/B	0.5%
CR-A1A (Dixie Hwy)	Cove Rd	Salerno Rd	Class II: 2-Ln Divided	790	11,666	562	TCEA	0.5%
CR-A1A (Dixie Hwy)	Salerno Rd	St. Lucie Blvd	Class II: 2-Ln Undivided	750	16,701	785	TCEA	0.5%
CR-A1A (Dixie Hwy)	St Lucie Blvd	Jefferson St	Class II: 2-Ln Undivided	750	13,545	698	D	0.5%
CR-A1A (Dixie Hwy)	Jefferson St	Indian St	Class II: 4-Ln Divided	1630	16,209	722	TCEA	0.5%
CR-A1A (Dixie Hwy)	Indian St	SR-714	Class I: 4-Ln Divided	2000	14,337	738	C	0.5%
CR-A1A (Dixie Hwy)	SR-714	SE Fifth St	2-Ln Undivided Non-State	675	6,545	314	C	0.5%
Farm Rd	Dr Martin Luther King Jr Dr	Palm Wy	Class II: 2-Ln Undivided	750	3,128	127	C	4.9%
Fox Brown Rd	SR-710	CR-714	Uninterrupted Rural Hwy: 2-Ln Undivided	730	433	23	A/B	6.8%
Goldenrod Rd	Britt Rd	SR-732	Class II: 4-Ln Divided	1630	5,161	252	A/B	1.6%
Goldenrod Rd	SR-732	SR-5	Class II: 2-Ln Undivided	750	6,600	318	C	0.5%
Goldenrod Rd	SR-5	Westmoreland Blvd	Class II: 2-Ln Undivided	750	4,449	299	C	0.6%
Gomez Ave	CR-708	Crossrip St	Class II: 2-Ln Undivided	750	3,402	159	C	0.5%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
Gomez Ave	Crossrip St	Osprey St	Class II: 2-Ln Undivided	750	1,248	56	C	3.7%
Green River Parkway	Dixie Hwy	Baker Rd	Class II: 2-Ln Undivided	750	7,474	384	D	2.7%
Green River Parkway	Baker Rd	SR-732	Class I: 2-Ln Undivided	880	8,946	616	C	2.2%
Green River Parkway	SR-732	St. Lucie County	2-Ln Uninter /Undivided Flow	1200	9,512	390	A/B	3.2%
Horseshoe Point Rd	CR-A1A	Kubin Ave	2-Ln Undivided Non-State	675	5,670	257	C	0.5%
Indian St	SR-76	Willoughby Blvd	Class I: 4-Ln Divided	2000	30,577	1,699	C	1.6%
Indian St	Willoughby Blvd	SR-5	Class I: 4-Ln Divided	2000	29,608	1,393	C	1.1%
Indian St	SR-5	Commerce Ave	Class I: 4-Ln Divided	2000	22,586	1,049	C	0.5%
Indian St	Commerce Ave	CR-A1A	Class I: 4-Ln Divided	2000	23,893	1,051	C	0.5%
Indian St	CR-A1A	St Lucie Blvd	2-Ln Undivided Non-State	675	6,293	317	TCEA	0.5%
Indian River Dr	Palmer St	CR-707	Class II: 2-Ln Undivided	750	7,568	462	D	1.1%
Island Way	Palm Beach County	Jupiter Road	Transitional 2-Ln Uninter /Undivided Flow	1160	4,995	297	A/B	3.7%
Island Way	Jupiter Road	Country Club Dr	Class II: 2-Ln Undivided	750	4,808	234	C	0.5%
Jack James Rd	SR-76	Blue Water Wy	Class II: 2-Ln Undivided	750	4,093	268	C	8.0%
Lares St	CR-708	CR-A1A	2-Ln Undivided Non-State	675	2,649	147	TCEA	0.5%
Little Club Wy	Country Club Dr	Wooden Bridge Wy	2-Ln Undivided Non-State	675	2,139	111	C	0.5%
Locks Rd	Canal St	SR-76	2-Ln Undivided Non-State	675	3,588	204	C	0.5%
MacArthur Blvd	Sailfish Point	SR-A1A	2-Ln Undivided Non-State	675	5,917	384	D	2.4%
Mapp Rd	South End	CR-714	Class II: 2-Ln Undivided	750	5,342	263	C	0.5%
Mapp Rd	CR-714	SR-714	Class II: 2-Ln Undivided	750	11,431	545	TCEA	0.5%
Mapp Rd	SR-714	Matheson Ave	Class II: 2-Ln Undivided	750	4,712	262	C	0.5%
Mapp Rd	Matheson Ave	North End	Class II: 2-Ln Undivided	750	7,452	340	C	0.5%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
Market Pl	SR-5	Commerce Ave	Class II: 2-Ln Undivided	750	4,716	278	C	1.6%
Matheson Ave	SR-714	Mapp Rd	Class II: 2-Ln Undivided	750	6,306	313	C	5.6%
Monroe St	SR-5	Commerce Ave	Class II: 2-Ln Undivided	750	1,654	102	C	0.5%
Murphy Rd	Mapp Rd	High Meadow Ave	Class II: 2-Ln Undivided	750	5,422	281	C	0.5%
Murphy Rd	High Meadow Ave	St Lucie County	Class II: 2-Ln Undivided	750	8,515	682	D	0.5%
Ocean Blvd	Flagler Ave	Palm Beach Rd	Class II: 2-Ln Divided	790	8,750	525	D	0.5%
Ocean Blvd	Palm Beach Rd	SR-714	4-Ln Divided Non-State	1465	11,275	586	C	0.5%
Osprey St	SR-5	CR-A1A	Class II: 2-Ln Undivided	750	6,087	253	C	4.7%
Osprey St	CR-A1A	Gomez Ave	Class II: 2-Ln Undivided	750	2,074	100	C	2.7%
Palm Beach Rd	SR-714	Ocean Blvd	2-Ln Divided Non-State	710	8,026	435	D	0.5%
Palm City Rd	SR-714	SR-5	Class II: 2-Ln Undivided	750	7,419	391	D	0.5%
Palmer St	CR-707	Indian River Dr	Class II: 2-Ln Undivided	750	3,599	242	C	4.6%
Pineapple Wy	CR-707A(Jensen Beach Blvd.)	SR-732	Class II: 2-Ln Undivided	750	9,986	598	TCEA	0.5%
Pomeroy St	SR-76	Willoughby Blvd	Class II: 2-Ln Divided	790	10,016	590	D	3.3%
Pomeroy St	Willoughby Blvd	SR-5	Class II: 2-Ln Divided	790	9,000	450	D	0.5%
Salerno Rd	SR-76	Willoughby Blvd	Class I: 2-Ln Undivided	880	8,267	434	C	1.0%
Salerno Rd	Willoughby Blvd	SR-5	Class II: 2-Ln Divided	790	9,348	535	D	0.5%
Salerno Rd	SR-5	Commerce Ave	Class II: 2-Ln Undivided	750	9,215	430	TCEA	0.5%
Salerno Rd	Commerce Ave	CR-A1A	Class II: 2-Ln Undivided	750	7,533	335	TCEA	0.5%
Seabranh Branch Blvd	Doubletree Dr	SR-5	Class I: 4-Ln Divided	2000	6,122	303	C	0.5%
Sewalls Pt Rd	SR-A1A	Palmer St	2-Ln Undivided Non-State	675	9,978	448	D	1.1%
Skyline Dr	CR-707A	CR-707	2-Ln Undivided Non-State	675	2,654	131	C	6.5%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
SR-5 (US-1)	Palm Beach County	CR-A1A	Transitional 4-Ln Uninter /Divided Flow	3120	21,987	942	A/B	0.5%
SR-5 (US-1)	CR-A1A	CR-708	Class I: 4-Ln Divided	2000	16,899	872	C	0.5%
SR-5 (US-1)	CR-708	Osprey St	Class I: 4-Ln Divided	2000	24,631	1,542	C	0.5%
SR-5 (US-1)	Osprey St	Seabbranch Blvd	Class I: 4-Ln Divided	2000	25,721	1,152	C	1.7%
SR-5 (US-1)	Seabbranch Blvd	Cove Rd	Class I: 6-Ln Divided	3020	30,025	1,404	C	0.5%
SR-5 (US-1)	Cove Rd	Salerno Rd	Class I: 6-Ln Divided	3020	31,755	1,458	C	0.5%
SR-5 (US-1)	Salerno Rd	Monroe St	Class I: 6-Ln Divided	3020	40,006	1,734	C	0.5%
SR-5 (US-1)	Monroe St	Indian St	Class I: 6-Ln Divided	3020	39,740	1,621	C	0.5%
SR-5 (US-1)	Indian St	SR-714	Class I: 6-Ln Divided	3020	45,150	2,202	C	0.5%
SR-5 (US-1)	SR-714	SR-5A	Class I: 6-Ln Divided	3020	36,532	1,662	C	0.5%
SR-5 (US-1)	SR-5A(Cut-off Rd)	SR-76	Class I: 6-Ln Divided	3020	34,638	1,513	C	0.5%
SR-5 (US-1)	SR-76	Palm City Rd	Class II: 6-Ln Divided	2520	47,422	2,011	D	0.5%
SR-5 (US-1)	Palm City Rd	Joan Jefferson Wy	Class II: 6-Ln Divided	2520	50,263	2,364	D	0.5%
SR-5 (US-1)	Joan Jefferson Wy	Wright Blvd	Class I: 6-Ln Divided	3020	58,659	2,809	C	0.5%
SR-5 (US-1)	Wright Blvd	Baker Rd	Class I: 6-Ln Divided	3020	51,911	2,299	C	0.5%
SR-5 (US-1)	Baker Rd	Britt Rd	Class I: 6-Ln Divided	3020	47,008	2,192	C	0.5%
SR-5 (US-1)	Britt Rd	SR-732	Class I: 7-Ln Divided	3530	56,191	3,068	C	0.5%
SR-5 (US-1)	SR-732	Westmoreland Blvd	Class I: 8-Ln Divided	4040	63,287	2,896	C	0.5%
SR-5 (US-1)	Westmoreland Blvd	St Lucie County	Class I: 8-Ln Divided	4040	62,019	2,679	C	0.5%
SR-710 (Warfield Blvd)	Okeechobee County	Fox Brown Rd	Uninterrupted Rural Hwy: 2-Ln Undivided	670	7,244	273	C	3.9%
SR-710 (Warfield Blvd)	Fox Brown Rd	CR-609 (Allapattah)	2-Ln Uninter /Undivided Flow	890	8,600	323	A/B	3.2%
SR-710 (Warfield Blvd)	CR-609 (Allapattah)	Van Buren	Class I: 2-Ln Undivided	870	13,836	802	C	3.7%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
SR-710 (Warfield Blvd)	Van Buren	CR-726 (Citrus)	Class II: 4-Ln Divided	1710	13,836	802	D	3.7%
SR-710 (Warfield Blvd)	CR-726	SR-76 (Kanner)	Transitional 4-Ln Uninter /Divided Flow	2470	12,449	572	A/B	4.3%
SR-710 (Warfield Blvd)	SR-76	Palm Beach County	Transitional 4-Ln Uninter /Divided Flow	2470	9,077	407	A/B	7.2%
SR-714 (Martin Hwy)	I-95	CR-76A (Citrus)	Transitional 2-Ln Uninter /Undivided Flow	1160	14,983	744	C	3.8%
SR-714 (Martin Hwy)	CR-76A (Citrus)	Florida's Turnpike	Class I: 4-Ln Divided	2000	22,166	1,170	C	1.7%
SR-714 (Martin Downs Blvd)	Florida's Turnpike	CR-713	Class I: 4-Ln Divided	2000	19,874	909	C	0.5%
SR-714 (Martin Downs Blvd)	CR-713	Matheson Ave	Class I: 4-Ln Divided	2000	28,563	1,474	C	0.5%
SR-714 (Martin Downs Blvd)	Matheson Ave	Mapp Rd	Class I: 4-Ln Divided	2000	32,124	1,703	C	0.5%
SR-714 (Palm City Bridge)	Mapp Rd	SR-76	Class I: 4-Ln Divided	2000	35,049	1,865	C	0.5%
SR-714 (Monterey Rd)	SR-76	Willoughby Blvd	Class I: 4-Ln Divided	2000	22,584	1,098	C	0.5%
SR-714 (Monterey Rd)	Willoughby Blvd	Monterey Extension	Class I: 4-Ln Divided	2000	24,714	1,247	C	0.5%
SR-714 (Monterey Rd)	Monterey Extension	SR-5	Class I: 4-Ln Divided	2000	17,662	886	C	3.2%
SR-714 (Monterey Rd)	SR-5	CR-A1A	Class II: 4-Ln Divided	1630	22,403	1,247	D	2.6%
SR-714 (Monterey Rd)	CR-A1A	SR-A1A	Class I: 4-Ln Divided	1910	23,275	1,327	C	6.3%
SR-732 (Causeway Blvd)	CR-707	SR-A1A	2-Ln Uninter /Undivided Flow	1200	13,701	781	C	0.6%
SR-732 (Jensen Beach Blvd)	SR-5	Green River Pkwy	Class I: 4-Ln Divided	2000	25,282	1,264	C	0.5%
SR-732 (Jensen Beach Blvd)	Green River Pkwy	CR-723	Class I: 4-Ln Divided	2000	24,284	1,090	C	0.5%
SR-76 (Kanner Hwy)	SR-15	SR-710	Uninterrupted Rural Hwy: 2-Ln Undivided	730	2,317	93	A/B	2.3%
SR-76 (Kanner Hwy)	SR-710	CR-708	Uninterrupted Rural Hwy: 2-Ln Undivided	730	3,800	165	A/B	2.2%
SR-76 (Kanner Hwy)	CR-708	CR-711/CR-76A	Transitional 2-Ln Uninter /Undivided Flow	1160	3,369	198	A/B	5.3%
SR-76 (Kanner Hwy)	CR-711/CR76A	Locks Rd	Class I: 4-Ln Divided	2000	13,751	853	C	1.6%
SR-76 (Kanner Hwy)	Locks Rd	Jack James	Class I: 4-Ln Divided	2000	19,109	914	C	0.5%

Segments with shaded LOS require additional analysis.
The peaks are: Cove Road (PM/EB)

Martin County 2022 Roadway Level of Service Inventory Report

Road Name	From	To	Type	Generalized Service Capacity	2022 Average Annual Daily Traffic	2022 Peak Hour Directional Volume	2022 Generalized LOS	Avg. Annual Growth Rate
SR-76 (Kanner Hwy)	Jack James	Cove Rd	Class I: 6-Ln Divided	3020	48,621	2,628	C	1.4%
SR-76 (Kanner Hwy)	Cove Rd	Salerno Rd	Class I: 6-Ln Divided	3020	38,333	1,863	C	4.8%
SR-76 (Kanner Hwy)	Salerno Rd	Indian St	Class I: 6-Ln Divided	3020	35,201	1,562	C	4.5%
SR-76 (Kanner Hwy)	Indian St	SR-714	Class I: 6-Ln Divided	3020	25,146	1,125	C	4.4%
SR-76 (Kanner Hwy)	SR-714	SR-5	Class I: 6-Ln Divided	3020	25,747	1,030	C	0.7%
SR-A1A (Ocean Blvd)	SR-714	St Lucie Blvd	Class II: 4-Ln Divided	1630	20,755	1,210	D	1.7%
SR-A1A (Ocean Blvd)	St Lucie Blvd	Sewalls Point Rd	Class I: 4-Ln Divided	2000	23,410	955	C	2.1%
SR-A1A (Ocean Blvd)	Sewalls Point Rd	Macarthur Blvd	Class I: 2-Ln Divided	925	14,046	780	C	2.3%
SR-A1A (Ocean Blvd)	MacArthur Blvd	SR-732	2-Ln Uninter /Undivided Flow	1200	8,390	408	A/B	0.5%
SR-A1A (Ocean Blvd)	SR-732	St Lucie County	2-Ln Uninter /Undivided Flow	1200	15,211	882	C	0.5%
St Lucie Blvd	CR-A1A	Indian St	2-Ln Undivided Non-State	675	3,480	183	C	3.1%
St Lucie Blvd	Indian St	SR-A1A	2-Ln Undivided Non-State	675	6,454	363	D	0.5%
Westmoreland Blvd	St Lucie County	SR-5	Class II: 2-Ln Divided	790	10,992	554	D	0.5%
Willoughby Blvd	Cove Rd	Salerno Rd	Class I: 2-Ln Undivided	880	4,108	236	C	1.1%
Willoughby Blvd	Salerno Rd	Pomeroy St	Class I: 2-Ln Undivided	880	9,017	474	C	0.5%
Willoughby Blvd	Pomeroy St	Indian St	Class I: 4-Ln Divided	2000	10,354	533	C	0.5%
Willoughby Blvd	Indian St	SR-714	Class I: 4-Ln Divided	2000	10,059	492	C	0.5%
Wright Blvd	SR-5	Dixie Highway	Class II: 2-Ln Undivided	750	10,146	497	D	1.2%

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**Manual Traffic Count - All Traffic
Green River Pkwy and Baker Rd
Stuart, FL**

**File Name : GB
Site Code : MK2307
Start Date : 6/28/2023
Page No : 1**

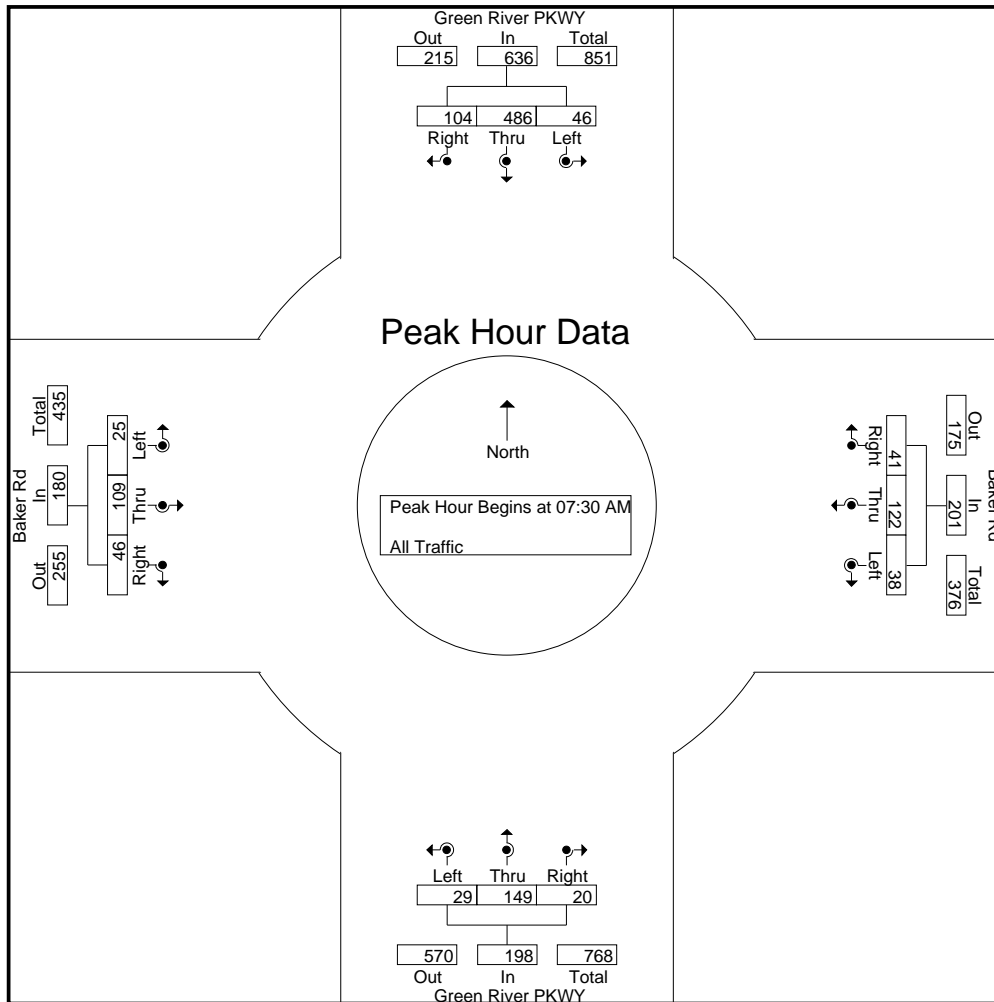
Groups Printed- All Traffic

Start Time	Green River PKWY NB			Green River PKWY SB			Baker Rd EB			Baker Rd WB			Int. Total
	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	
07:00 AM	1	11	6	16	94	8	7	25	3	6	21	7	205
07:15 AM	4	27	5	17	122	6	13	26	6	7	19	7	259
07:30 AM	6	26	2	25	126	12	12	27	9	12	22	10	289
07:45 AM	4	28	6	29	132	10	17	32	8	11	39	8	324
Total	15	92	19	87	474	36	49	110	26	36	101	32	1077
08:00 AM	3	43	9	24	122	17	11	16	1	8	38	9	301
08:15 AM	7	52	12	26	106	7	6	34	7	10	23	11	301
08:30 AM	7	36	9	16	97	14	4	25	7	8	34	9	266
08:45 AM	7	34	10	30	97	9	7	40	10	10	45	17	316
Total	24	165	40	96	422	47	28	115	25	36	140	46	1184
04:00 PM	7	76	19	23	57	13	12	33	16	18	34	4	312
04:15 PM	15	103	12	21	62	11	9	38	15	29	43	6	364
04:30 PM	11	112	13	15	62	9	11	33	12	30	49	8	365
04:45 PM	16	95	13	23	56	13	13	43	14	22	37	10	355
Total	49	386	57	82	237	46	45	147	57	99	163	28	1396
05:00 PM	7	117	14	22	63	8	13	40	17	27	43	5	376
05:15 PM	10	108	11	21	81	10	6	49	16	27	44	5	388
05:30 PM	5	89	10	13	58	16	11	39	10	25	32	6	314
05:45 PM	9	73	9	19	60	11	7	24	11	20	28	5	276
Total	31	387	44	75	262	45	37	152	54	99	147	21	1354
Grand Total	119	1030	160	340	1395	174	159	524	162	270	551	127	5011
Apprch %	9.1	78.7	12.2	17.8	73.1	9.1	18.8	62	19.2	28.5	58.1	13.4	
Total %	2.4	20.6	3.2	6.8	27.8	3.5	3.2	10.5	3.2	5.4	11	2.5	

**Manual Traffic Count - All Traffic
Green River Pkwy and Baker Rd
Stuart, FL**

**File Name : GB
Site Code : MK2307
Start Date : 6/28/2023
Page No : 2**

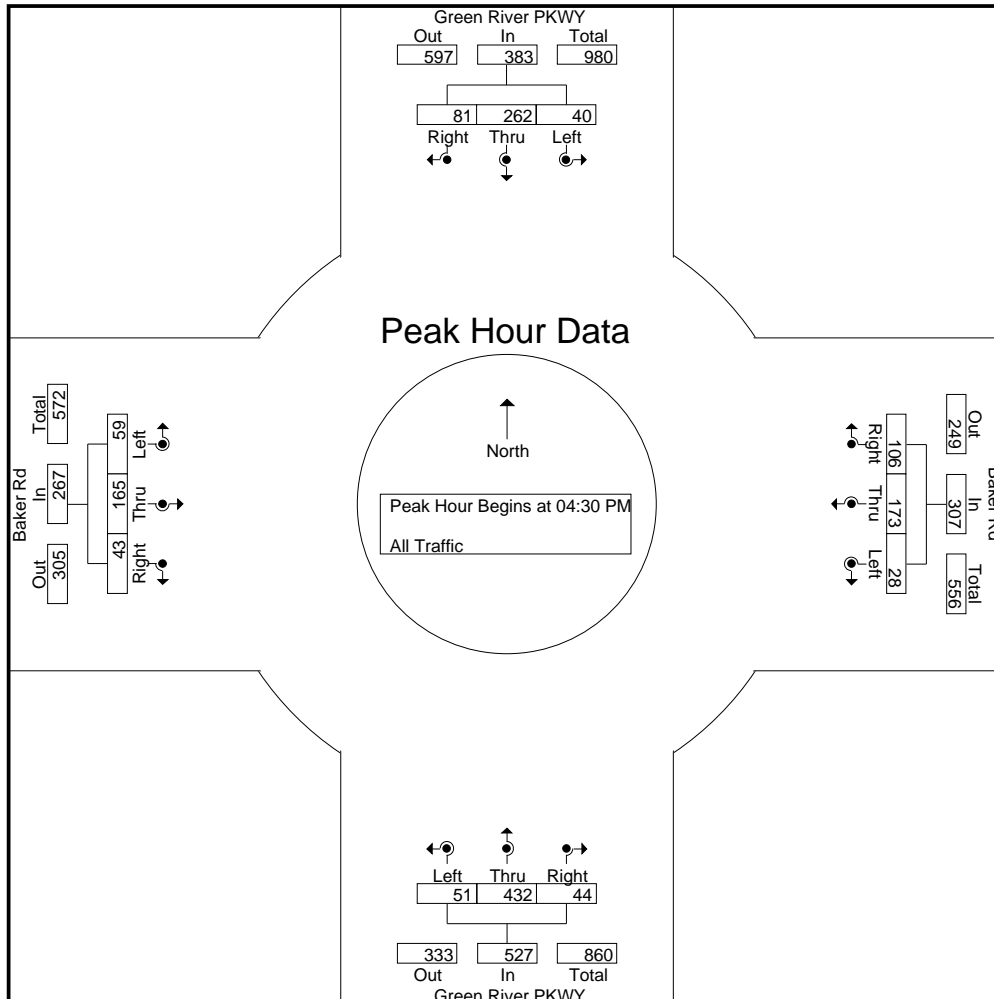
Start Time	Green River PKWY NB				Green River PKWY SB				Baker Rd EB				Baker Rd WB				Int. Total
	Right	Thru	Left	App. Total	Right	Thru	Left	App. Total	Right	Thru	Left	App. Total	Right	Thru	Left	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30 AM																	
07:30 AM	6	26	2	34	25	126	12	163	12	27	9	48	12	22	10	44	289
07:45 AM	4	28	6	38	29	132	10	171	17	32	8	57	11	39	8	58	324
08:00 AM	3	43	9	55	24	122	17	163	11	16	1	28	8	38	9	55	301
08:15 AM	7	52	12	71	26	106	7	139	6	34	7	47	10	23	11	44	301
Total Volume	20	149	29	198	104	486	46	636	46	109	25	180	41	122	38	201	1215
% App. Total	10.1	75.3	14.6		16.4	76.4	7.2		25.6	60.6	13.9		20.4	60.7	18.9		
PHF	.714	.716	.604	.697	.897	.920	.676	.930	.676	.801	.694	.789	.854	.782	.864	.866	.938



Manual Traffic Count - All Traffic
Green River Pkwy and Baker Rd
Stuart, FL

File Name : GB
Site Code : MK2307
Start Date : 6/28/2023
Page No : 3

Start Time	Green River PKWY NB				Green River PKWY SB				Baker Rd EB				Baker Rd WB				Int. Total
	Right	Thru	Left	App. Total	Right	Thru	Left	App. Total	Right	Thru	Left	App. Total	Right	Thru	Left	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	11	112	13	136	15	62	9	86	11	33	12	56	30	49	8	87	365
04:45 PM	16	95	13	124	23	56	13	92	13	43	14	70	22	37	10	69	355
05:00 PM	7	117	14	138	22	63	8	93	13	40	17	70	27	43	5	75	376
05:15 PM	10	108	11	129	21	81	10	112	6	49	16	71	27	44	5	76	388
Total Volume	44	432	51	527	81	262	40	383	43	165	59	267	106	173	28	307	1484
% App. Total	8.3	82	9.7		21.1	68.4	10.4		16.1	61.8	22.1		34.5	56.4	9.1		
PHF	.688	.923	.911	.955	.880	.809	.769	.855	.827	.842	.868	.940	.883	.883	.700	.882	.956



Turn Count Summary

Location: Green River at Dixie, FI
GPS Coordinates: Lat=27.220141, Lon=-80.253566
Date: 2023-06-29
Day of week: Thursday
Weather:
Analyst: NSM

Total vehicle traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
06:59	0	3	0	7	0	0	0	0	5	0	0	0	15
07:00	8	89	0	84	0	2	0	24	35	0	0	0	242
07:15	2	96	0	103	0	2	0	30	37	0	0	0	270
07:30	3	129	0	103	0	4	0	34	47	0	0	0	320
07:45	10	118	0	111	0	1	0	46	57	0	0	0	343
08:00	5	113	0	110	0	2	0	35	74	0	0	0	339
08:15	9	104	0	119	0	1	0	40	67	0	0	0	340
08:30	3	118	0	95	0	2	0	35	49	0	0	0	302
08:45	8	92	0	68	0	5	0	39	57	0	0	0	269
09:00	1	5	0	24	0	0	0	3	9	0	0	0	42

Car traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
06:59	0	3	0	7	0	0	0	0	5	0	0	0	15
07:00	8	89	0	84	0	2	0	24	35	0	0	0	242
07:15	2	96	0	103	0	2	0	30	37	0	0	0	270
07:30	3	129	0	103	0	4	0	34	47	0	0	0	320
07:45	10	118	0	111	0	1	0	46	57	0	0	0	343
08:00	5	113	0	110	0	2	0	35	74	0	0	0	339
08:15	9	104	0	119	0	1	0	40	67	0	0	0	340
08:30	3	118	0	95	0	2	0	35	49	0	0	0	302
08:45	8	92	0	68	0	5	0	39	57	0	0	0	269
09:00	1	5	0	24	0	0	0	3	9	0	0	0	42

Pedestrian volumes

Interval starts	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
06:59	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15	0	0	0	0	0	0	0	0	0	0	0	0	0
08:30	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

07:30 - 08:30

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	27	464	0	443	0	8	0	155	245	0	0	0	1342
Factor	0.68	0.90	0.00	0.93	0.00	0.50	0.00	0.84	0.83	0.00	0.00	0.00	0.98
Approach Factor	0.93			0.94			0.92			0.00			

Peak Hour Vehicle Summary

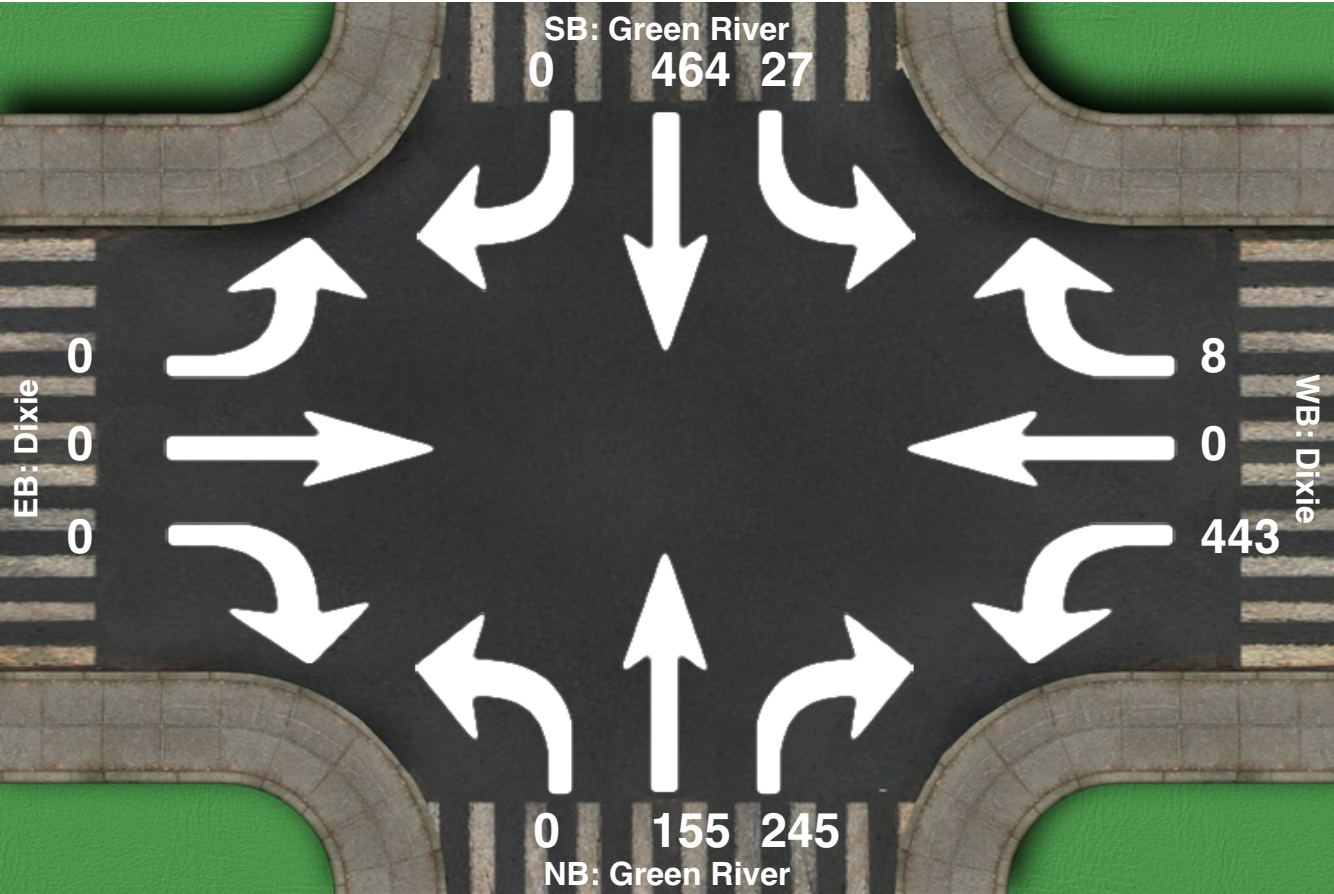
Vehicle	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Car	27	464	0	443	0	8	0	155	245	0	0	0	1342

Peak Hour Pedestrians

	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

Location: Green River at Dixie, FI
GPS Coordinates: Lat=27.220141, Lon=-80.253566
Date: 2023-06-29
Day of week: Thursday
Weather:
Analyst: NSM



Intersection Peak Hour

07:30 - 08:30

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	27	464	0	443	0	8	0	155	245	0	0	0	1342
Factor	0.68	0.90	0.00	0.93	0.00	0.50	0.00	0.84	0.83	0.00	0.00	0.00	0.98
Approach Factor	0.93			0.94			0.92			0.00			

Turn Count Summary

Location: Green River at Dixie, FI
GPS Coordinates: Lat=27.220141, Lon=-80.253566
Date: 2023-06-29
Day of week: Thursday
Weather:
Analyst: NSM

Total vehicle traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:00	3	43	0	69	1	10	0	108	91	0	0	0	325
16:15	4	49	0	66	0	7	0	113	91	0	0	0	330
16:30	2	60	0	57	0	7	0	111	72	0	0	0	309
16:45	6	59	0	70	0	14	0	115	96	0	0	0	360
17:00	7	72	0	83	0	13	0	131	78	0	0	0	384
17:15	5	50	0	61	0	2	0	150	131	0	0	0	399
17:30	5	55	0	59	0	2	0	95	84	0	0	0	300
17:45	3	54	0	73	0	4	0	74	75	0	0	0	283
18:00	0	2	0	7	0	0	0	7	7	0	0	0	23

Car traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:00	3	43	0	69	1	10	0	108	91	0	0	0	325
16:15	4	49	0	66	0	7	0	113	91	0	0	0	330
16:30	2	60	0	57	0	7	0	111	72	0	0	0	309
16:45	6	59	0	70	0	14	0	115	96	0	0	0	360
17:00	7	72	0	83	0	13	0	131	78	0	0	0	384
17:15	5	50	0	61	0	2	0	150	131	0	0	0	399
17:30	5	55	0	59	0	2	0	95	84	0	0	0	300
17:45	3	54	0	73	0	4	0	74	75	0	0	0	283
18:00	0	2	0	7	0	0	0	7	7	0	0	0	23

Pedestrian volumes

Interval starts	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
16:00	0	0	0	0	0	0	0	0	0	0	0	0	0
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	0	0	0	0	0	0	0	0	0	0	0
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	0	0	0	0	0	0
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	0	0	0	0	0	0	0	0	0
18:00	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

16:30 - 17:30

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	20	241	0	271	0	36	0	507	377	0	0	0	1452
Factor	0.71	0.84	0.00	0.82	0.00	0.64	0.00	0.84	0.72	0.00	0.00	0.00	0.91
Approach Factor	0.83			0.80			0.79			0.00			

Peak Hour Vehicle Summary

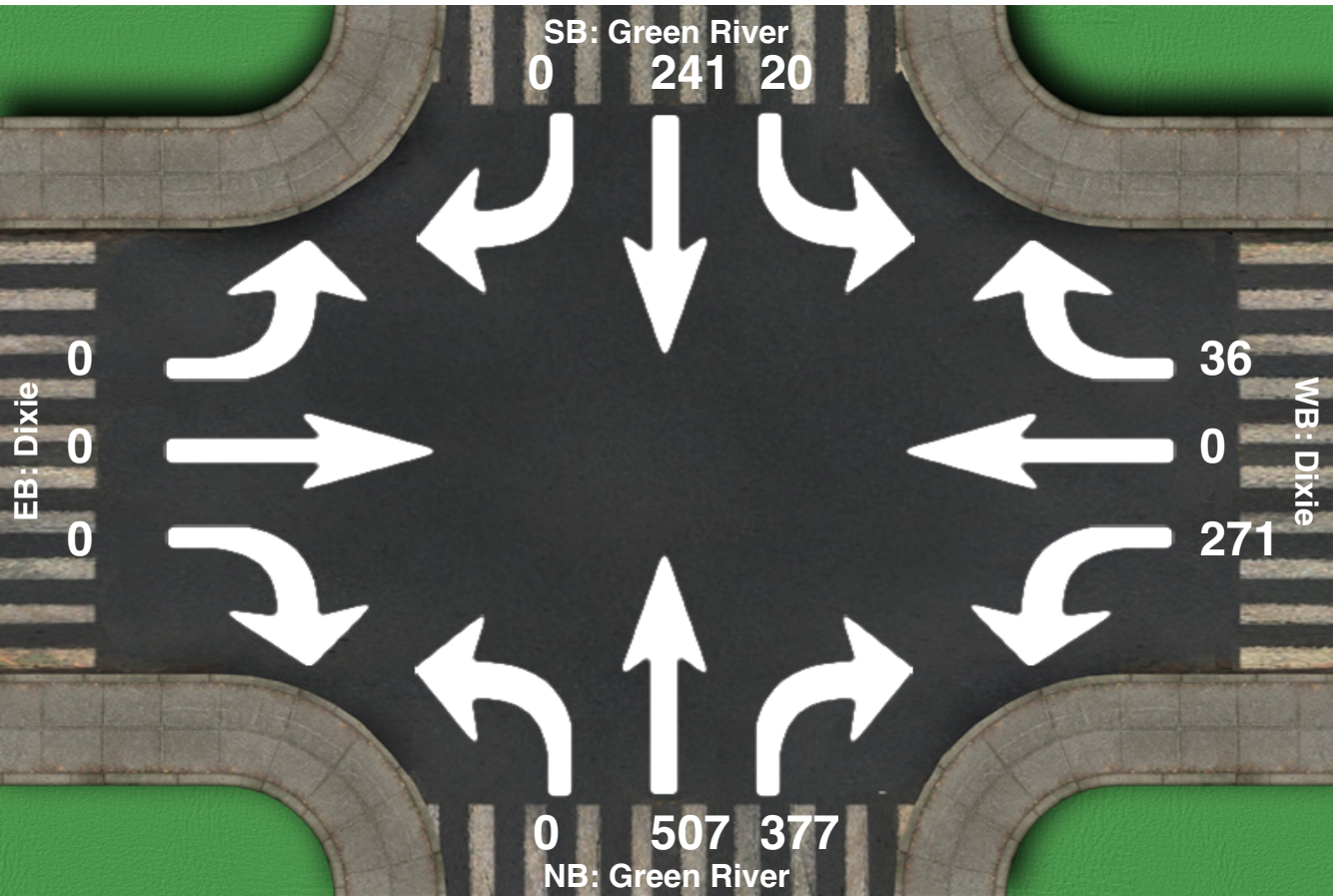
Vehicle	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Car	20	241	0	271	0	36	0	507	377	0	0	0	1452

Peak Hour Pedestrians

	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

Location: Green River at Dixie, FI
GPS Coordinates: Lat=27.220141, Lon=-80.253566
Date: 2023-06-29
Day of week: Thursday
Weather:
Analyst: NSM



Intersection Peak Hour

16:30 - 17:30

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	20	241	0	271	0	36	0	507	377	0	0	0	1452
Factor	0.71	0.84	0.00	0.82	0.00	0.64	0.00	0.84	0.72	0.00	0.00	0.00	0.91
Approach Factor	0.83			0.80			0.79			0.00			

Turn Count Summary

Location: Cardinal at Dixie, Stuart, FI
 GPS Coordinates: Lat=27.187207, Lon=-80.289059
 Date: 2023-06-29
 Day of week: Thursday
 Weather:
 Analyst:

Total vehicle traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
07:12	1	7	2	5	1	0	0	12	1	0	0	1	30
07:15	10	59	0	36	20	5	2	30	9	16	25	5	217
07:30	13	70	1	25	14	2	0	27	17	13	19	0	201
07:45	13	79	10	37	24	7	0	39	29	11	28	3	280
08:00	20	69	4	29	26	6	0	43	27	21	32	3	280
08:15	17	87	7	29	23	9	0	52	33	13	30	1	301
08:30	22	66	7	34	25	21	1	46	27	20	31	3	303
08:45	18	66	3	23	32	14	0	42	17	13	25	3	256
09:00	21	47	4	30	27	17	0	46	27	19	30	0	268
09:15	0	0	0	0	0	0	0	0	0	0	4	1	5

Car traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
07:12	1	7	2	5	1	0	0	12	1	0	0	1	30
07:15	10	59	0	36	20	5	2	30	9	16	25	5	217
07:30	13	70	1	25	14	2	0	27	17	13	19	0	201
07:45	13	79	10	37	24	7	0	39	29	11	28	3	280
08:00	20	69	4	29	26	6	0	43	27	21	32	3	280
08:15	17	87	7	29	23	9	0	52	33	13	30	1	301
08:30	22	66	7	34	25	21	1	46	27	20	31	3	303
08:45	18	66	3	23	32	14	0	42	17	13	25	3	256
09:00	21	47	4	30	27	17	0	46	27	19	30	0	268
09:15	0	0	0	0	0	0	0	0	0	0	4	1	5

Pedestrian volumes

Interval starts	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
07:12	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30	0	0	0	0	0	0	0	0	0	0	0	0	0
07:45	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15	0	0	0	0	0	0	0	0	0	0	0	0	0
08:30	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00	0	0	0	0	0	0	0	0	0	0	0	0	0
09:15	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

07:45 - 08:45

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	72	301	28	129	98	43	1	180	116	65	121	10	1164
Factor	0.82	0.86	0.70	0.87	0.94	0.51	0.25	0.87	0.88	0.77	0.95	0.83	0.96
Approach Factor	0.90			0.84			0.87			0.88			

Peak Hour Vehicle Summary

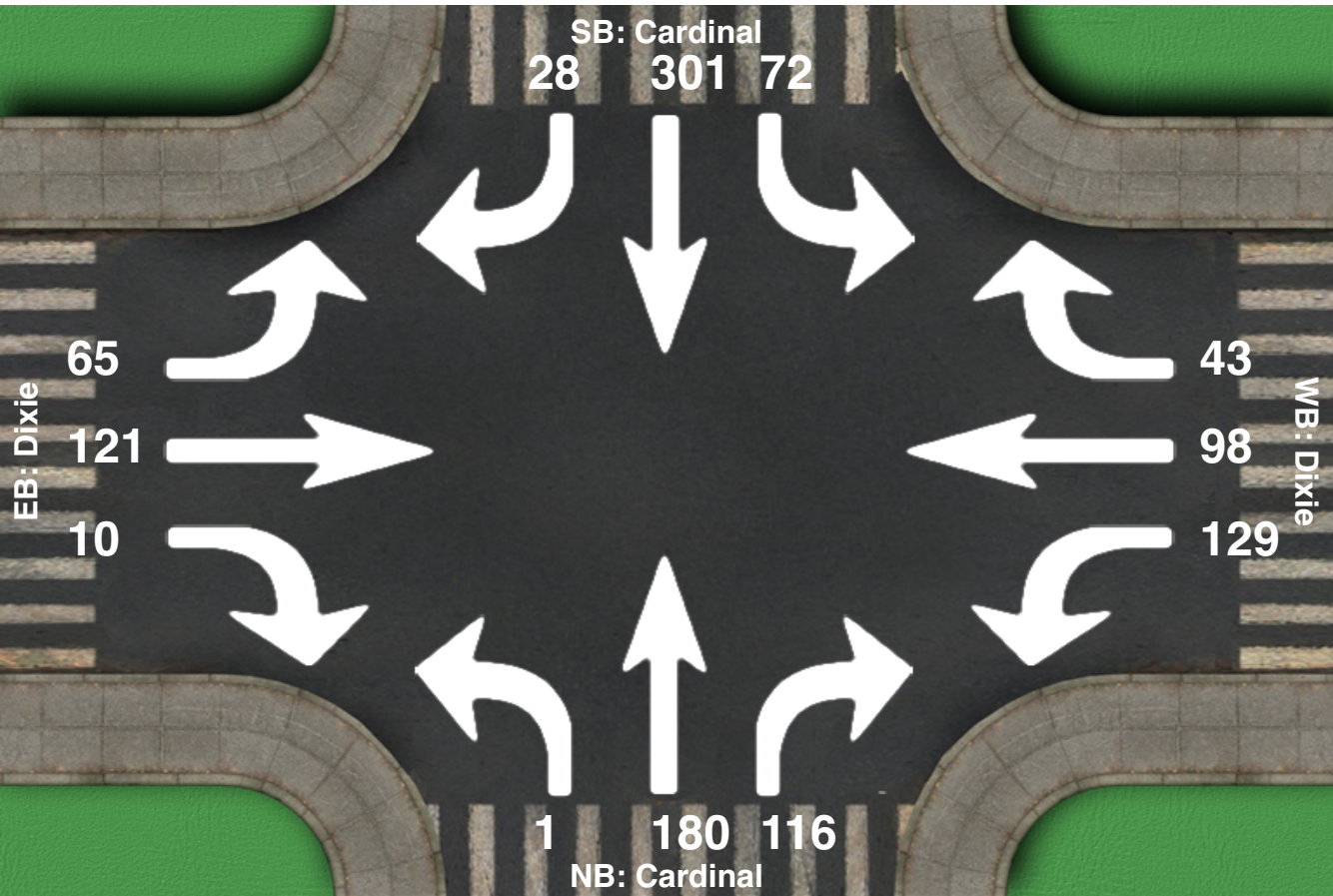
Vehicle	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Car	72	301	28	129	98	43	1	180	116	65	121	10	1164

Peak Hour Pedestrians

	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

Location: Cardinal at Dixie, Stuart, FL
GPS Coordinates: Lat=27.187207, Lon=-80.289059
Date: 2023-06-29
Day of week: Thursday
Weather:
Analyst:



Intersection Peak Hour

07:45 - 08:45

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	72	301	28	129	98	43	1	180	116	65	121	10	1164
Factor	0.82	0.86	0.70	0.87	0.94	0.51	0.25	0.87	0.88	0.77	0.95	0.83	0.96
Approach Factor	0.90			0.84			0.87			0.88			

Turn Count Summary

Location: Cardinal at Dixie, Stuart, FI
 GPS Coordinates: Lat=27.187207, Lon=-80.289059
 Date: 2023-06-29
 Day of week: Thursday
 Weather:
 Analyst:

Total vehicle traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:12	3	7	1	4	10	4	0	10	6	0	3	0	48
16:15	17	42	4	28	38	18	1	80	41	17	29	0	315
16:30	23	42	5	20	31	23	0	78	32	13	27	0	294
16:45	17	49	2	25	34	17	0	60	24	11	15	0	254
17:00	22	48	1	25	33	21	2	85	30	18	33	2	320
17:15	18	53	0	31	41	19	2	73	30	14	31	0	312
17:30	17	36	0	21	37	19	0	111	31	17	31	0	320
17:45	13	41	1	28	24	22	1	73	29	7	24	2	265
18:00	17	42	2	28	29	21	2	51	22	8	14	3	239

Car traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:12	3	7	1	4	10	4	0	10	6	0	3	0	48
16:15	17	42	4	28	38	18	1	80	41	17	29	0	315
16:30	23	42	5	20	31	23	0	78	32	13	27	0	294
16:45	17	49	2	25	34	17	0	60	24	11	15	0	254
17:00	22	48	1	25	33	21	2	85	30	18	33	2	320
17:15	18	53	0	31	41	19	2	73	30	14	31	0	312
17:30	17	36	0	21	37	19	0	111	31	17	31	0	320
17:45	13	41	1	28	24	22	1	73	29	7	24	2	265
18:00	17	42	2	28	29	21	2	51	22	8	14	3	239

Pedestrian volumes

Interval starts	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
16:12	0	0	0	0	0	0	0	0	0	0	0	0	0
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	0	0	0	0	0	0	0	0	0	0	0
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	0	0	0	0	0	0
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	0	0	0	0	0	0	0	0	0
18:00	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

17:00 - 18:00

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	70	178	2	105	135	81	5	342	120	56	119	4	1217
Factor	0.80	0.84	0.50	0.85	0.82	0.92	0.62	0.77	0.97	0.78	0.90	0.50	0.95
Approach Factor	0.88			0.88			0.82			0.84			

Peak Hour Vehicle Summary

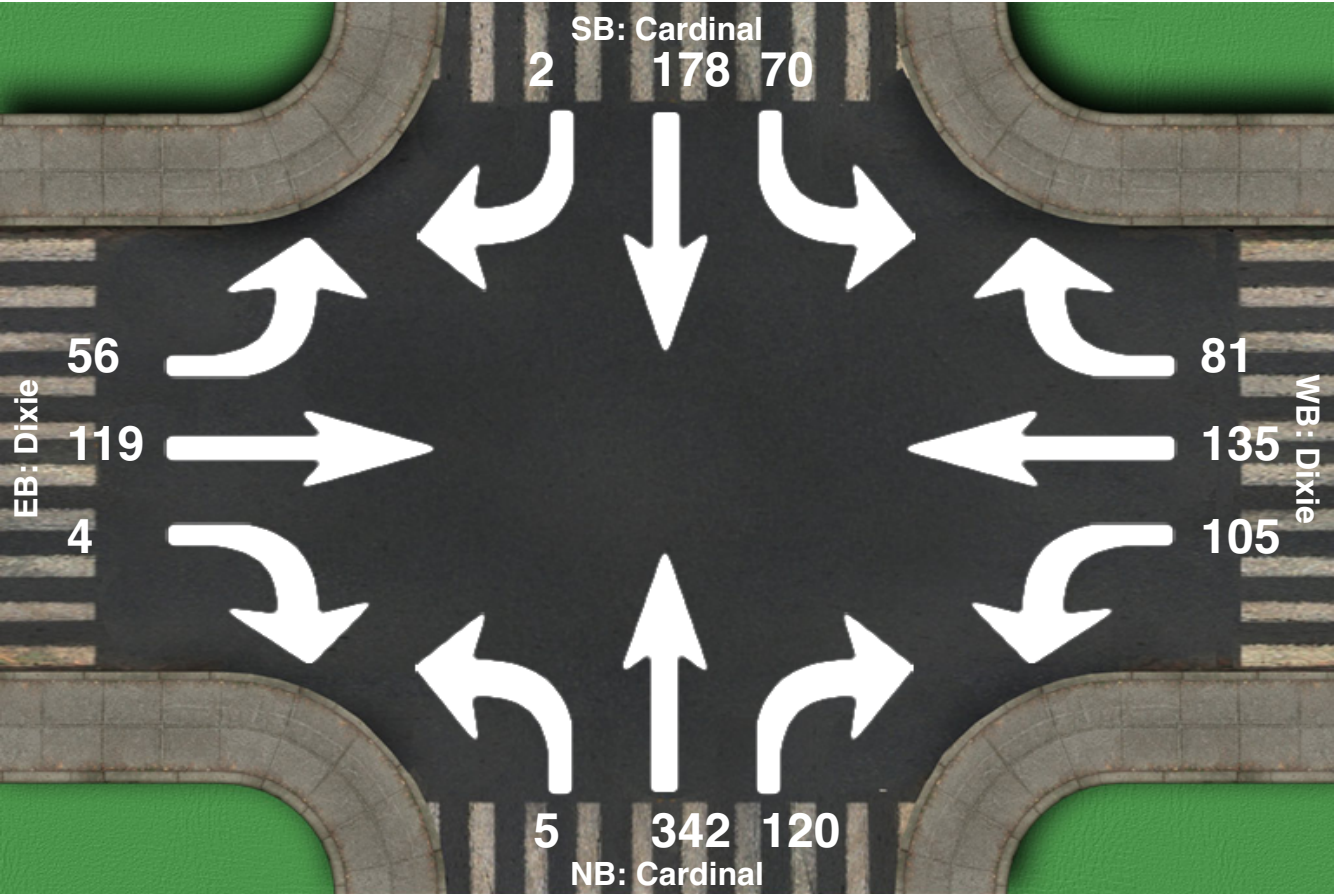
Vehicle	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Car	70	178	2	105	135	81	5	342	120	56	119	4	1217

Peak Hour Pedestrians

	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0

Intersection Peak Hour

Location: Cardinal at Dixie, Stuart, FL
GPS Coordinates: Lat=27.187207, Lon=-80.289059
Date: 2023-06-29
Day of week: Thursday
Weather:
Analyst:



Intersection Peak Hour

17:00 - 18:00

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	70	178	2	105	135	81	5	342	120	56	119	4	1217
Factor	0.80	0.84	0.50	0.85	0.82	0.92	0.62	0.77	0.97	0.78	0.90	0.50	0.95
Approach Factor	0.88			0.88			0.82			0.84			



Data Output

- Field - 2020-12-18 1600
- Field - 2022-05-12 1600
- Field - 2022-10-18 1600
- Field - 2021-10-07 0700
- Field - 2021-12-09 0700
- Field - 2001-01-01 0658
- Field - 2020-10-29 0700

Turn Count Summary

Location: Cardinal at Baker,
 GPS Coordinates: Lat=27.174187, Lon=-80.270838
 Date: 2001-01-01
 Day of week: Monday
 Weather:
 Analyst: SGM

Total vehicle traffic

Interval starts	Southbound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
06:58	0	1	1	0	0	0	2	0	0	0	0	0	4
07:00	0	9	7	0	8	0	14	3	0	2	0	0	26
07:15	0	6	10	0	9	0	12	4	0	3	2	22	46
07:30	0	8	11	0	8	1	27	3	0	1	1	40	100
07:45	0	13	14	0	13	0	24	2	0	3	1	44	114
08:00	0	5	18	0	15	0	16	4	0	3	0	36	97
08:15	0	14	14	0	13	0	24	6	0	6	0	38	115
08:30	0	11	8	0	15	0	26	4	0	10	1	28	103
08:45	0	9	6	0	17	1	30	4	0	10	0	44	121
09:00	0	2	1	0	1	0	0	0	0	0	0	4	8



Field - 2020-08-26 1808

Field - 2001-01-01 1558

Field - 2018-05-10 1903

Field - 2021-03-24 0630

Field - 2021-03-25 0630

Location: Cardinal at Baker,
GPS Coordinates: Lat=27.174187, Lon=-80.270838
Date: 2001-01-01
Day of week: Monday
Weather:
Analyst: SGM

Total vehicle traffic

Interval starts	Southbound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
15:58	0	0	1	0	3	0	5	0	0	1	0	3	13
16:00	0	3	5	0	18	1	35	6	0	14	0	43	125
16:15	0	1	4	0	13	2	34	4	0	9	0	39	106
16:30	0	4	9	0	15	1	36	7	0	11	0	24	107
16:45	0	8	6	1	8	3	26	13	0	13	0	43	121
17:00	0	3	7	0	22	2	36	6	0	10	0	39	125
17:15	0	5	13	0	13	1	28	8	0	18	1	42	129
17:30	0	4	10	1	3	2	25	4	0	13	2	25	89

APPENDIX D

2022 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 8900 EAST- A1A TO US1

WEEK	DATES	SF	MOCF: 0.89 PSCF
1	01/01/2022 - 01/01/2022	1.00	1.12
2	01/02/2022 - 01/08/2022	0.98	1.10
3	01/09/2022 - 01/15/2022	0.95	1.07
* 4	01/16/2022 - 01/22/2022	0.94	1.06
* 5	01/23/2022 - 01/29/2022	0.92	1.03
* 6	01/30/2022 - 02/05/2022	0.90	1.01
* 7	02/06/2022 - 02/12/2022	0.88	0.99
* 8	02/13/2022 - 02/19/2022	0.86	0.97
* 9	02/20/2022 - 02/26/2022	0.87	0.98
*10	02/27/2022 - 03/05/2022	0.87	0.98
*11	03/06/2022 - 03/12/2022	0.87	0.98
*12	03/13/2022 - 03/19/2022	0.87	0.98
*13	03/20/2022 - 03/26/2022	0.89	1.00
*14	03/27/2022 - 04/02/2022	0.90	1.01
*15	04/03/2022 - 04/09/2022	0.92	1.03
*16	04/10/2022 - 04/16/2022	0.93	1.04
17	04/17/2022 - 04/23/2022	0.95	1.07
18	04/24/2022 - 04/30/2022	0.97	1.09
19	05/01/2022 - 05/07/2022	0.98	1.10
20	05/08/2022 - 05/14/2022	1.00	1.12
21	05/15/2022 - 05/21/2022	1.01	1.13
22	05/22/2022 - 05/28/2022	1.03	1.16
23	05/29/2022 - 06/04/2022	1.05	1.18
24	06/05/2022 - 06/11/2022	1.07	1.20
25	06/12/2022 - 06/18/2022	1.08	1.21
26	06/19/2022 - 06/25/2022	1.09	1.22
27	06/26/2022 - 07/02/2022	1.09	1.22
28	07/03/2022 - 07/09/2022	1.09	1.22
29	07/10/2022 - 07/16/2022	1.10	1.24
30	07/17/2022 - 07/23/2022	1.10	1.24
31	07/24/2022 - 07/30/2022	1.10	1.24
32	07/31/2022 - 08/06/2022	1.10	1.24
33	08/07/2022 - 08/13/2022	1.10	1.24
34	08/14/2022 - 08/20/2022	1.10	1.24
35	08/21/2022 - 08/27/2022	1.11	1.25
36	08/28/2022 - 09/03/2022	1.12	1.26
37	09/04/2022 - 09/10/2022	1.13	1.27
38	09/11/2022 - 09/17/2022	1.14	1.28
39	09/18/2022 - 09/24/2022	1.12	1.26
40	09/25/2022 - 10/01/2022	1.10	1.24
41	10/02/2022 - 10/08/2022	1.07	1.20
42	10/09/2022 - 10/15/2022	1.05	1.18
43	10/16/2022 - 10/22/2022	1.05	1.18
44	10/23/2022 - 10/29/2022	1.05	1.18
45	10/30/2022 - 11/05/2022	1.05	1.18
46	11/06/2022 - 11/12/2022	1.04	1.17
47	11/13/2022 - 11/19/2022	1.04	1.17
48	11/20/2022 - 11/26/2022	1.03	1.16
49	11/27/2022 - 12/03/2022	1.02	1.15
50	12/04/2022 - 12/10/2022	1.01	1.13
51	12/11/2022 - 12/17/2022	1.00	1.12
52	12/18/2022 - 12/24/2022	0.98	1.10
53	12/25/2022 - 12/31/2022	0.95	1.07

* PEAK SEASON

23-FEB-2023 09:11:22

830UPD

4_8900_PKSEASON.TXT

APPENDIX E

MARTIN COUNTY TRAFFIC

Intersection: 135 SR707 (DIXIE HIGHWAY) @ CR723 (SAVANNAH RD)/BAKER RD

PHASE	from R/R	from Baker		from JB	from Baker	from R/R		to Post office
	WBLT	EB	NBLT	SB	EBLT	WB	SBLT	NB
	1	2	3	4	5	6	7	8
Min green	7	7	7	10	7	7	7	10
Vehicle Ext	3	3	3	5	3	3	3	5
Max 1	20	25	15	45	15	25	15	45
Max 2								
Yellow	3.7	3.4	3.7	4.4	3.4	4	4	4
Red Clear	3.4	3.4	2.4	2	3.3	2	2.4	2

Walk	0	0	0	0	0	5	0	0
Ped Clear	0	0	0	0	0	13	0	0

Start Up		Yellow				Yellow		
Locking Detector		X				X		
Veh. Recalls		Min				Min		

Assigned ph.	1		3		5		7	
Cross Switch Ph	6		8		2		4	

May 2, 2024

MARTIN COUNTY TRAFFIC

Intersection: 136 SR707 (DIXIE HIGHWAY) @ NW GREEN RIVER PKWY

PHASE	1	SB 2	3	WB 4	SBLT 5	NB 6	7	8
Min green	0	15	0	7	4	15	0	0
Vehicle Ext	0	5	0	4	4	5	0	0
Max 1	0	55	0	45	15	55	0	0
Max 2								
Yellow	3	4	3	4	4	4	3	3
Red Clear	0	2.2	0	2.3	2	2.2	0	0

Walk	0	0	0	7	0	7	0	0
Ped Clear	0	0	0	13	0	22	0	0

Start Up		Yellow				Yellow		
Locking Detector		X				X		
Veh. Recalls		Min				Min		

Assigned ph.					5			
Cross Switch Ph					2			

May 2, 2024

APPENDIX F

Provide minimum 8-foot separation between vehicles traveling in opposing direction. Separation may be less than 8 feet when:

- (1) Turning paths are highly visible and speeds are low, or
- (2) Signal left turn phases are not concurrent for the opposing directions.

212.13 Islands

See **FDM 210.3** for island criteria.

212.14 Auxiliary Lanes

The primary function of auxiliary lanes at intersections is to accommodate speed changes, storage and maneuvering of turning traffic. The length of the auxiliary lanes is the sum of the deceleration length, queue length and approach end taper. Pavement marking requirements for auxiliary lanes are included in [Standard Plans](#), **Index 711-001**.

212.14.1 Deceleration Length

The required total deceleration length is that needed for a safe and comfortable stop from the design speed of the highway. See **Exhibit 212-1** for minimum deceleration lengths (including taper) for left turn lanes.

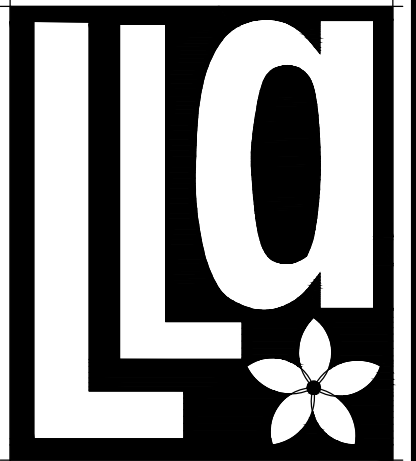
Right turn lane tapers and lengths are identical to left turn lanes under stop control conditions. Right turn lane tapers and lengths are site-specific for free-flow or yield conditions.

212.14.2 Queue Length

The queue length provided should be based on a traffic study.

For low volume intersections where a traffic study is not justified, a minimum 50-foot queue length (2 vehicles) should be provided for C1, C2, and C3R context classifications. A minimum 100-foot queue length (4 vehicles) should be provided in C2T, C3C, C4, C5, and C6 context classifications. Locations with over 10% truck traffic should accommodate at least one car and one truck.

For queue lengths at signalized intersections, refer to **FDM 232.2**.



**Litterick
Landscape
Architecture**

2740 SW Martin Downs Blvd. #199
Palm City, FL 34990
561-719-3876
Jason.L1677@yahoo.com

Project Name

Parcel 10 - Avonlea

City of Stuart, Florida

Landscape Architect of Record



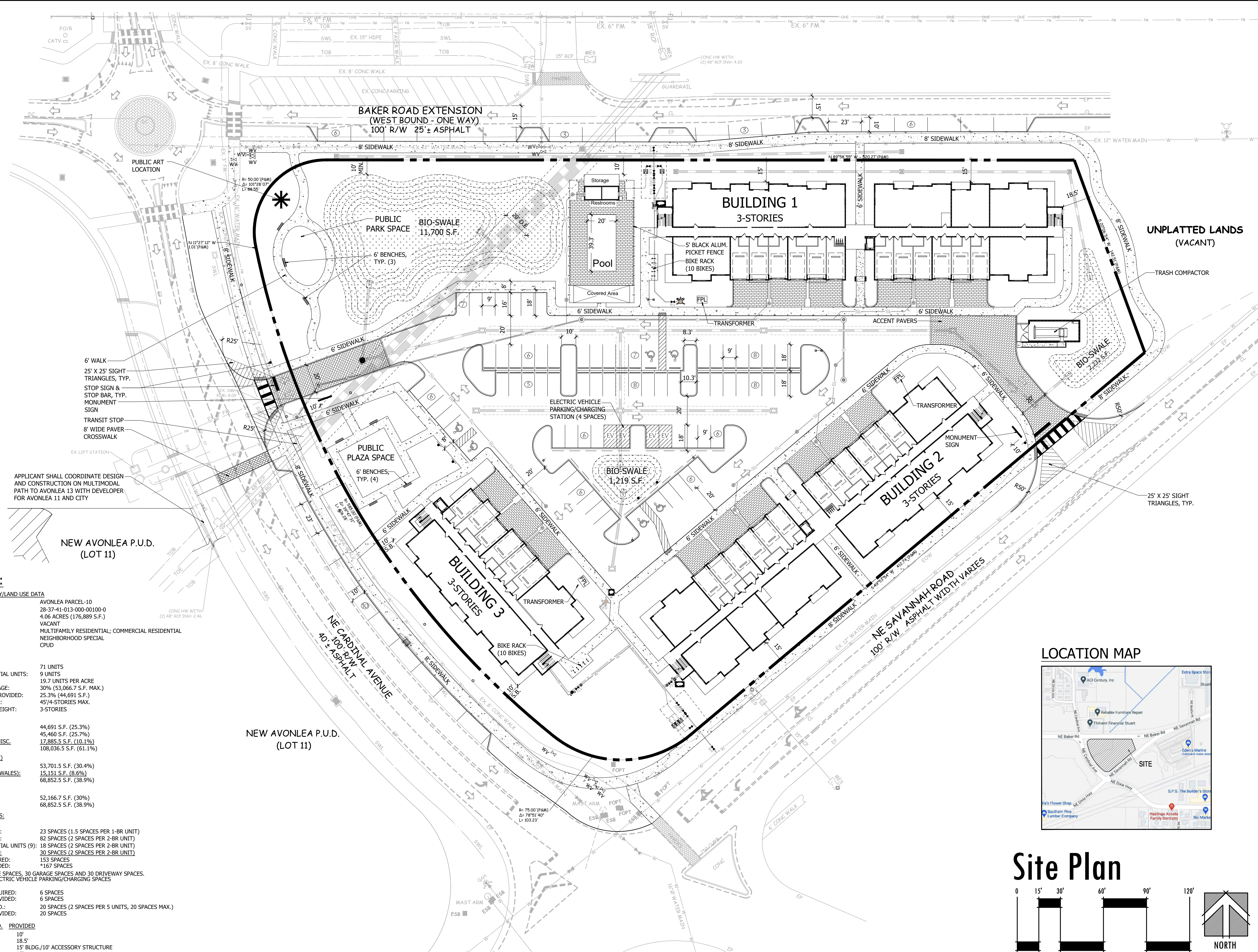
Jason M. Litterick, RLA
(LA0001677)

Scale: 1" = 30'-0"

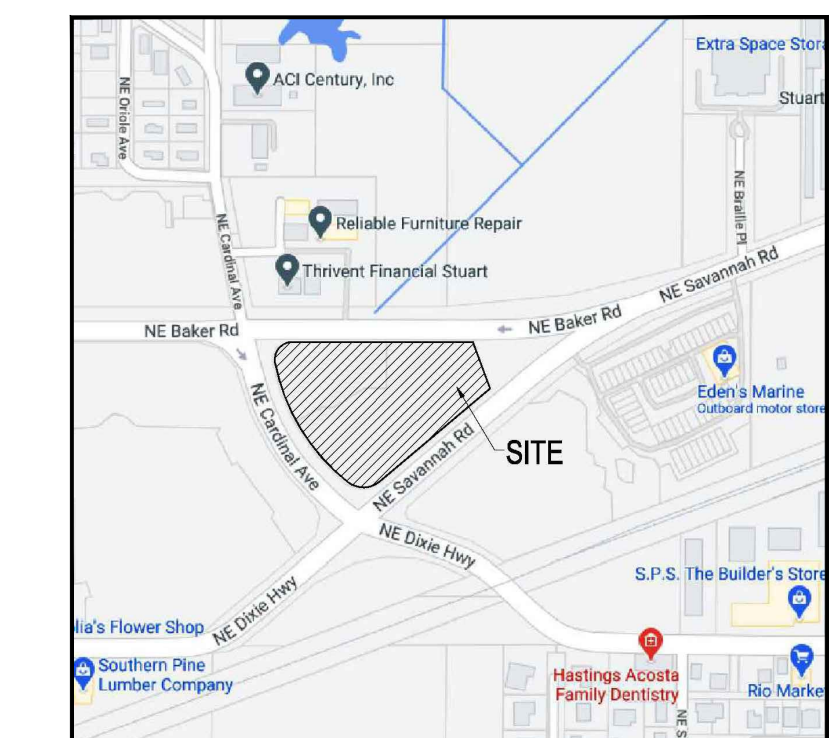
Designed: JML
Drawn: JML
Approved: JML
Date: 11/28/22
Job no.
Revisions: 4/14/23

Sheet No.

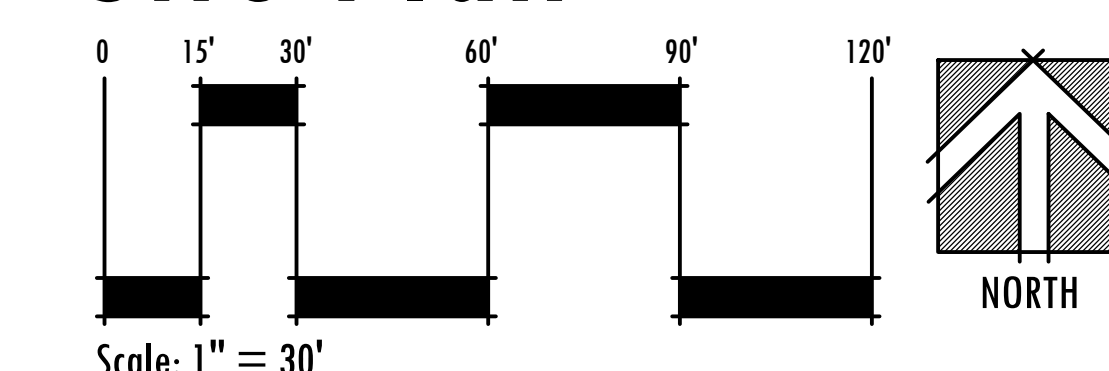
SP-1



LOCATION MAP



Site Plan



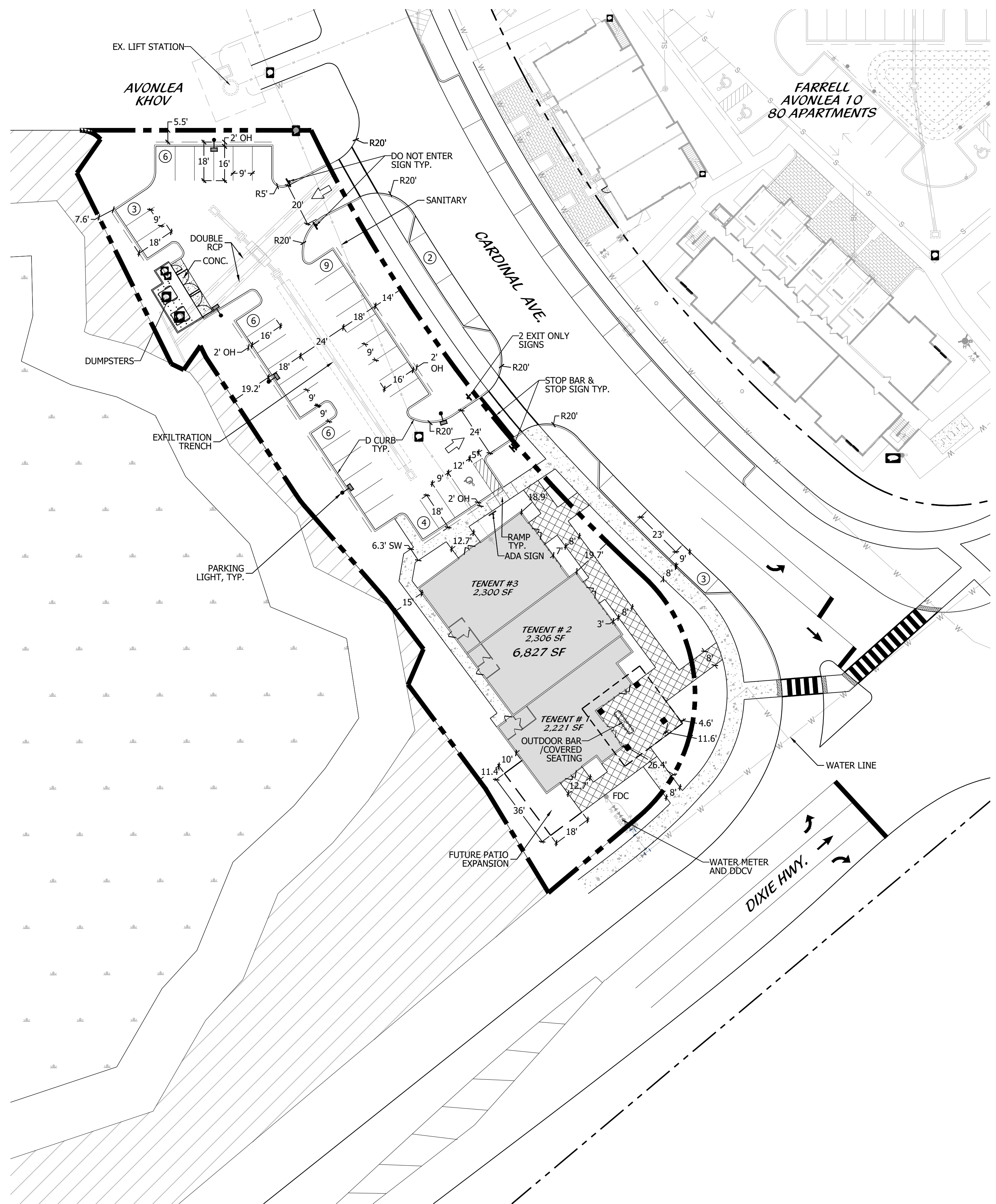
SITE DATA:

APPLICATION/PROPERTY/LAND USE DATA	
NAME OF APPLICATION:	AVONLEA PARCEL-10
SITE ID:	28-37-41-013-000-00100-0
SITE AREA:	4.06 ACRES (176,889 S.F.)
EXISTING USE:	VACANT
PROPOSED USE:	MULTIFAMILY RESIDENTIAL; COMMERCIAL RESIDENTIAL
FUTURE LAND USE:	NEIGHBORHOOD SPECIAL
ZONING:	CPUD
PROPOSED USES:	
RESIDENTIAL UNITS:	71 UNITS
COMMERCIAL RESIDENTIAL UNITS:	9 UNITS
PROPOSED DENSITY:	19.7 UNITS PER ACRE
MAX. BUILDING COVERAGE:	30% (53,066.7 S.F. MAX.)
BUILDING COVERAGE PROVIDED:	25.3% (44,691 S.F.)
MAX. BUILDING HEIGHT:	45/4-STORIES MAX.
PROPOSED BUILDING HEIGHT:	3-STORIES
IMPERVIOUS	
BUILDING COV.:	44,691 S.F. (25.3%)
VEHICULAR USE AREA:	45,460 S.F. (25.7%)
SIDEWALKS/CURBING/MISC. TOTAL:	17,885.5 S.F. (10.1%)
PERVIOUS (OPEN SPACE) TOTAL:	108,036.5 S.F. (61.1%)
LANDSCAPE AREA:	
DRY RETENTION (BIO-SWALES):	53,701.5 S.F. (30.4%)
TOTAL:	15,151 S.F. (8.6%)
TOTAL:	68,852.5 S.F. (38.9%)
OPEN SPACE:	
REQUIRED:	52,166.7 S.F. (30%)
PROVIDED:	68,852.5 S.F. (38.9%)
PARKING CALCULATIONS:	
REQUIRED PARKING	
1-BEDROOM UNITS (15):	23 SPACES (1.5 SPACES PER 1-BR UNIT)
2-BEDROOM UNITS (41):	82 SPACES (2 SPACES PER 2-BR UNIT)
COMMERCIAL RESIDENTIAL UNITS (9):	18 SPACES (2 SPACES PER 2-BR UNIT)
3-BEDROOM UNITS (15):	30 SPACES (2 SPACES PER 2-BR UNIT)
TOTAL PARKING REQUIRED:	153 SPACES
TOTAL PARKING PROVIDED:	1167 SPACES
*INCLUDES 28 OFF-SITE SPACES, 30 GARAGE SPACES AND 30 DRIVEWAY SPACES. ALSO INCLUDES 4 - ELECTRIC VEHICLE PARKING/CHARGING SPACES	
HANDICAP SPACES REQUIRED:	
HANDICAP SPACES PROVIDED:	6 SPACES
BICYCLE PARKING REQ'D:	20 SPACES (2 SPACES PER 5 UNITS, 20 SPACES MAX.)
HANDICAP SPACES PROVIDED:	20 SPACES
SETBACKS:	
FRONT (WEST):	10'
REAR (EAST):	18.5'
SIDE (NORTH):	15' BLDG./10' ACCESSORY STRUCTURE
SIDE (SOUTH):	15'
*SETBACK REQUIREMENTS AS DETERMINED BY CITY COMMISSION	

APPLICANT SHALL COORDINATE DESIGN AND CONSTRUCTION ON MULTIMODAL PATH TO AVONLEA 13 WITH DEVELOPER FOR AVONLEA 11 AND CITY

NEW AVONLEA P.U.D. (LOT 11)

NEW AVONLEA P.U.D. (LOT 11)



SITE DATA

NAME OF PROJECT	AVONLEA LOT 11		
PROPERTY CONTROL NUMBER	28-37-41-013-000-00110-0		
FEMA FLOOD ZONE	X		
LAND USE DESIGNATION	NEIGHBORHOOD/SPECIAL COMMERCIAL PLANNED UNIT OVERLAY DISTRICT		
MAXIMUM BUILDING HEIGHT	35'		
NUMBER OF STORIES	1		
NUMBER OF BUILDINGS	1		
PROPOSED USE	COMMERCIAL		
TOTAL SITE AREA	0.90 AC		
	39,049.97 SF		
BUILDING DATA			
GROSS SQUARE FOOTAGE	6,827.00 SF		
TOTAL SQUARE FOOTAGE	6,827.00 SF		
FAR	1.00		
LAND USE	SF	AC	%
BUILDING LOT COVERAGE	6,827	0.16	17.48%
VEHICULAR USE AREA	13,027	0.30	33.36%
OPEN SPACE	19,196	0.44	49.16%
TOTAL	39,050	0.90	100.00%
LOT COVERAGE	SF	AC	%
IMPERVIOUS AREA	6,827	0.16	17.48%
BUILDING LOT COVERAGE	13,027	0.30	33.36%
VEHICULAR USE AREA	4,150	0.10	10.63%
SIDEWALKS & HARDSCAPE	24,004	0.55	61.47%
TOTAL IMPERVIOUS AREA	24,004	0.55	61.47%
PERVIOUS AREA	SF	AC	%
GREEN SPACE	15,046	0.35	38.53%
TOTAL PERVIOUS AREA	15,046	0.35	38.53%
TOTAL SITE AREA	39,050	0.90	100.00%
PARKING DATA	REQ	PROV	
PROFESSIONAL OFFICE (1/250 SF)	27.31	39	
TOTAL	27.31	39	
HANDICAP SPACES (INCLUDED IN TOTAL)	1	1	

GENERAL NOTES

REFUSE SERVICE SHALL BE ACCOMPLISHED BY WAY OF INDIVIDUAL DUMPSTER AND RECYCLING AS SHOWN ON THE SITE PLAN.

10' X 30' SAFE SIGHT TRIANGLE SHALL BE PROVIDED AT THE PROJECTS ENTRY ROADS.

TYPE D OR F RAISED CONCRETE CURBING SHALL BE PROVIDED AROUND ALL PLANTING ISLANDS WITHIN VEHICULAR USE AREAS.

ON STREET PARKING IS UTILIZED IN THE CALCULATION FOR REQUIRED PARKING IN ACCORDANCE WITH THE PROVISIONS OF THE MxD CODE.

RAMPS SHALL BE PROVIDED AT ALL PLAZA, SIDEWALK AND STREET INTERSECTIONS IN ACCORDANCE WITH ADA AND THE FLORIDA HANDICAP ACCESSIBILITY CODE.

ALL A/C UNITS SHALL BE LOCATED TO THE REAR OR SIDE OF ALL BUILDINGS AND FULLY SCREENED ON THREE SIDES WITH LANDSCAPING.

THE BUILDING, LANDSCAPE AND LIGHTING DESIGN SHALL MAXIMIZE THE USE OF OPTED DESIGN PRINCIPLES.

MINIMUM TYPE CONSTRUCTION FOR ALLOWABLE HEIGHT & BUILDING AREA MUST BE COMPLIANCE WITH THE FLORIDA BUILDING CODE T-300 (AFTER OCT. 1, 2003 FBC 2004).

ALL SIGNAGE SHALL CONFORM TO THE REQUIREMENTS OF THE TOWN OF JUPITER LDR'S.

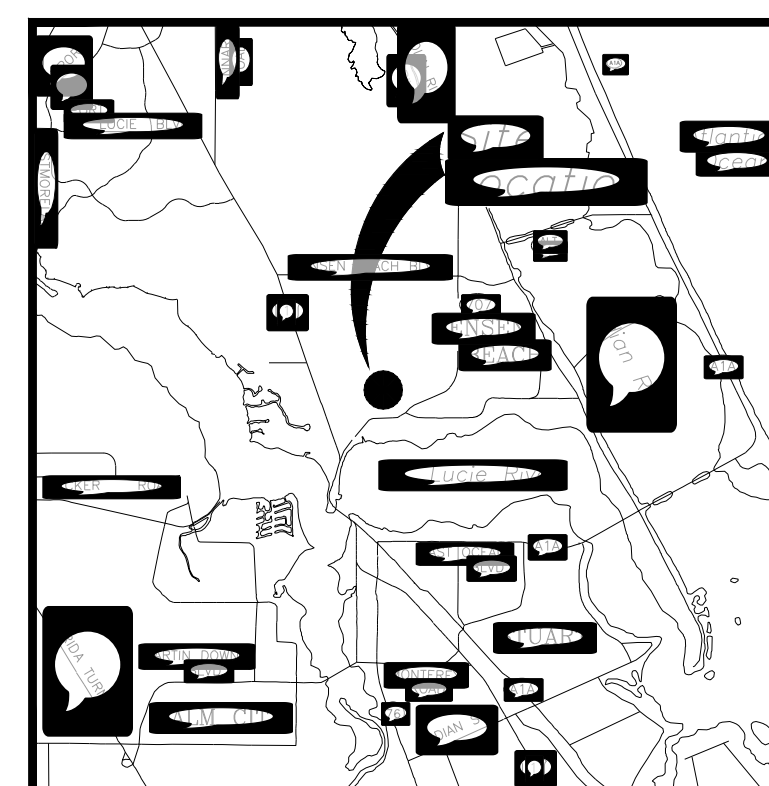
"PEDESTRIAN XING" SIGNS SHALL BE PROVIDED FOR ALL MID-BLOCK CROSSINGS.

PROJECT ID SIGNS SHALL BE ILLUMINATED WITH GROUND MOUNTED FIXTURES ON PHOTOCELLS.

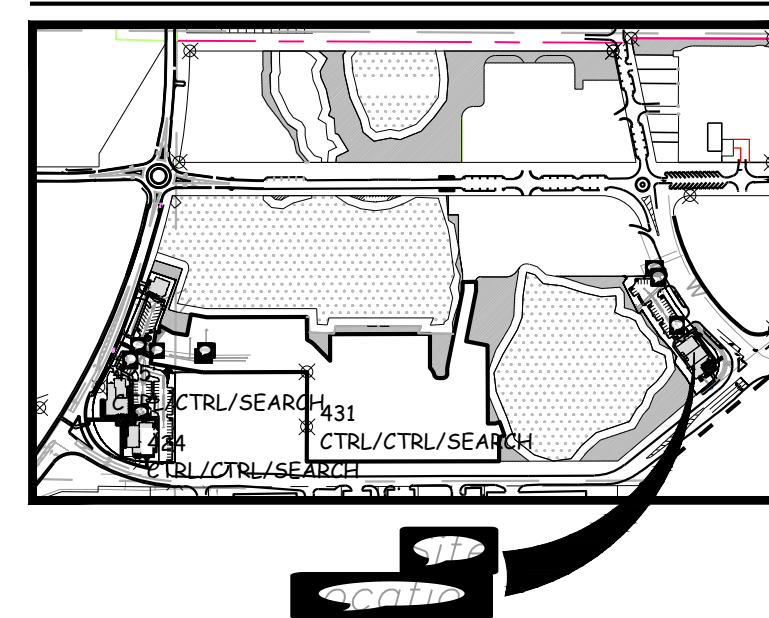
THE PHOTOMETRIC PLAN REFLECTS MAINTAINED LIGHTING VALUES IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN OF JUPITER LDR'S.

STREET LIGHTS TO BE FPL COACH LIGHTS MOUNTED ON 12' TALL POLES. LIGHT LEVELS SHALL NOT EXCEED THE MAXIMUM TO MINIMUM ESTABLISHED IN TABLE 1 OF ORDINANCE NO. 66-98.

LOCATION MAP



KEY MAP



PROJECT TEAM

OWNER/CLIENT:
AVONLEA COMMERCIAL CORNERS, LLC
100 SW ALBANY AVENUE, SUITE 200
STUART, FL 34994
772.286.6292
CONTACT: NIKOLAUS SCHROTH

LANDSCAPE ARCHITECT/PLANNER:
COTLEUR & HEARING, INC.
1934 COMMERCE LANE, SUITE 1
JUPITER, FL 33458
561.747.6336
CONTACT: DON HEARING

ARCHITECT:
TI ARCHITECTS
3000 HIGH RIDGE ROAD, BAY 4
BOYNTON BEACH, FL 33426
561.860.2905
CONTACT: HAROLD TUTTLE AIA

CIVIL ENGINEER:
VELCON GROUP
702 SW PORT ST. LUCIE BOULEVARD
PORT ST. LUCIE, FL 34953
772.879.0477
CONTACT: BOB KEMMERSON, PE

SURVEYOR:
VELCON GROUP
702 SW PORT ST. LUCIE BOULEVARD
PORT ST. LUCIE, FL 34953
772.879.0477
CONTACT: DARREN GUETTLER

TRAFFIC CONSULTANT:
MACKENZIE ENGINEERING AND PLANNING, INC.
1172 SW 30TH STREET, SUITE 500
PALM CITY, FL 34990
772.286.8030
CONTACT: SHAUN G. MACKENZIE, PE

LEGEND

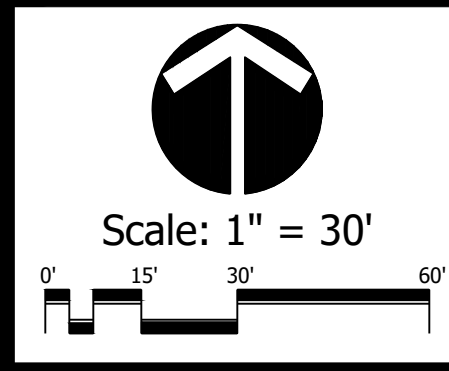
ADA	AMERICANS WITH DISABILITIES ACT	HC SIGN
LB	LANDSCAPE BUFFER	STOP SIGN
R	RADIUS	DO NOT ENTER
SB	SETBACK	PARKING LIGHT
SW	SIDEWALK	
TYP	TYPICAL	

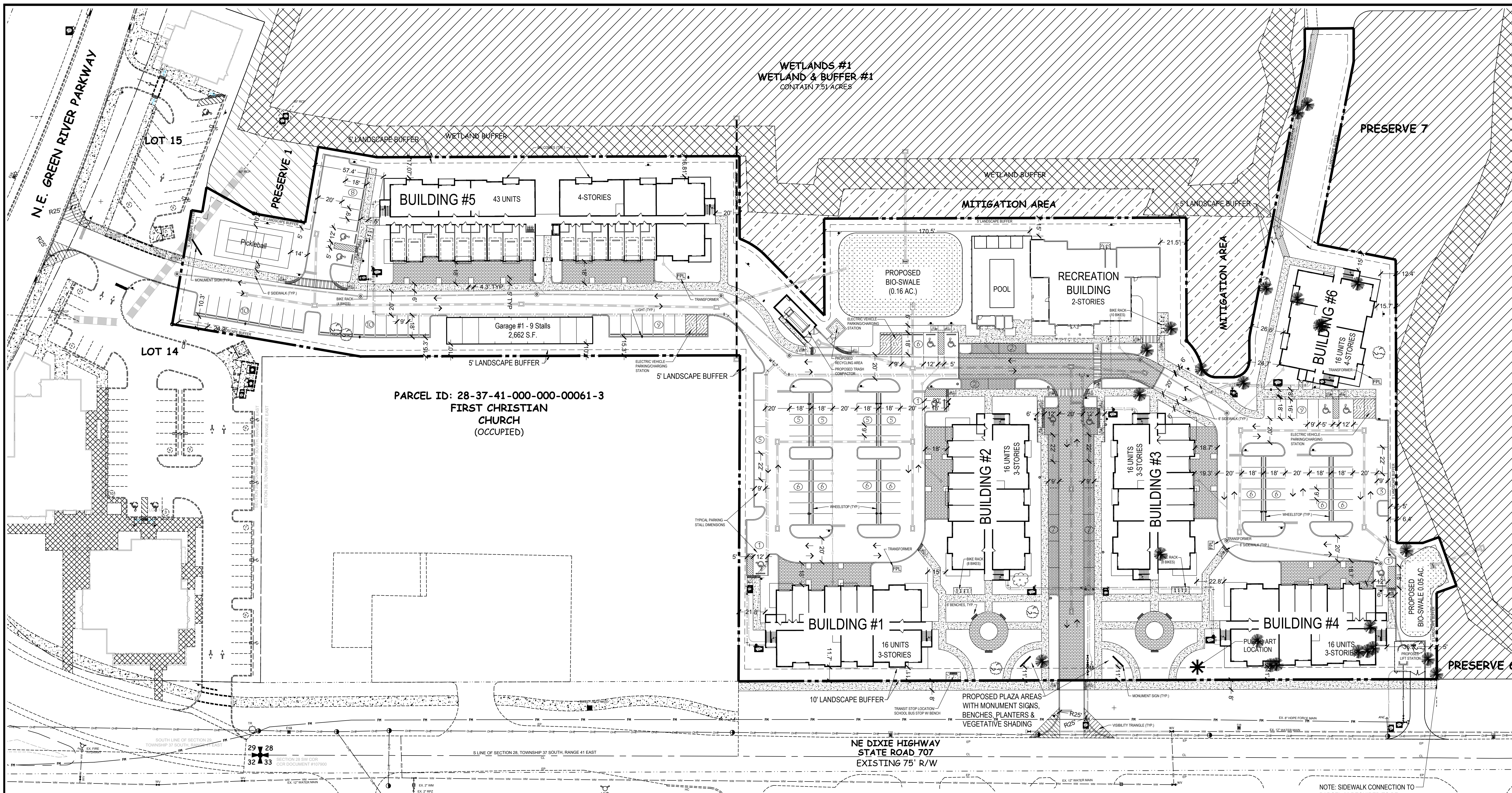
Cotleur & Hearing
Landscape Architects
Land Planners
Environmental Consultants
1934 Commerce Lane
Suite 1
Jupiter, Florida 33458
561.747.6336 · Fax 747.1377
www.cotleurhearing.com
Lic# LC-26000535

Avonlea Commercial Corners

Lot 11 Site Plan
Stuart, Florida

DESIGNED	DEH
DRAWN	RO
APPROVED	DEH
JOB NUMBER	21-0401
DATE	08-29-22
REVISIONS	11-11-22





SITE DATA:

APPLICATION/PROPERTY/LAND USE DATA	
NAME OF APPLICATION:	AVONLEA PARCEL-13
PARCEL ID:	28-37-41-013-000-00130-0
SITE AREA:	6.35 ACRES (276,447 S.F.)
EXISTING USE:	VACANT
PROPOSED USE:	MULTIFAMILY RESIDENTIAL
FUTURE LAND USE:	NEIGHBORHOOD SPECIAL
ZONING:	RPUD
PROPOSED USES:	
TOTAL RESIDENTIAL UNITS:	123 UNITS (AS ALLOWED PER AVONLEA PUD MASTER PARCEL PLAN)
PROPOSED DENSITY:	19.37 UNITS PER ACRE
MAX. BUILDING COVERAGE:	30% (82,934.1 S.F. MAX.)
BUILDING COVERAGE PROVIDED:	22.2% (61,457 S.F.)
MAX. BUILDING HEIGHT:	45/4-STORIES MAX.
PROPOSED BUILDING HEIGHT:	4-STORIES
IMPERVIOUS	
BUILDING COV.:	61,457 S.F. (22.2%)
VEHICULAR USE AREA:	82,769.66 S.F. (29.9%)
SIDEWALKS/CURBING/MISC. TOTAL:	36,208.04 S.F. (13.1%)
	180,434.7 S.F. (65.3%)
PERVIOUS (OPEN SPACE)	
LANDSCAPE AREA:	86,991.5 S.F. (31.5%)
DRY RETENTION (BIO-SWALES): TOTAL:	9,020.8 S.F. (3.3%)
	96,012.3 S.F. (34.7%)
OPEN SPACE:	
REQUIRED:	82,934.1 S.F. (30%)
PROVIDED:	96,012.3 S.F. (34.7%)

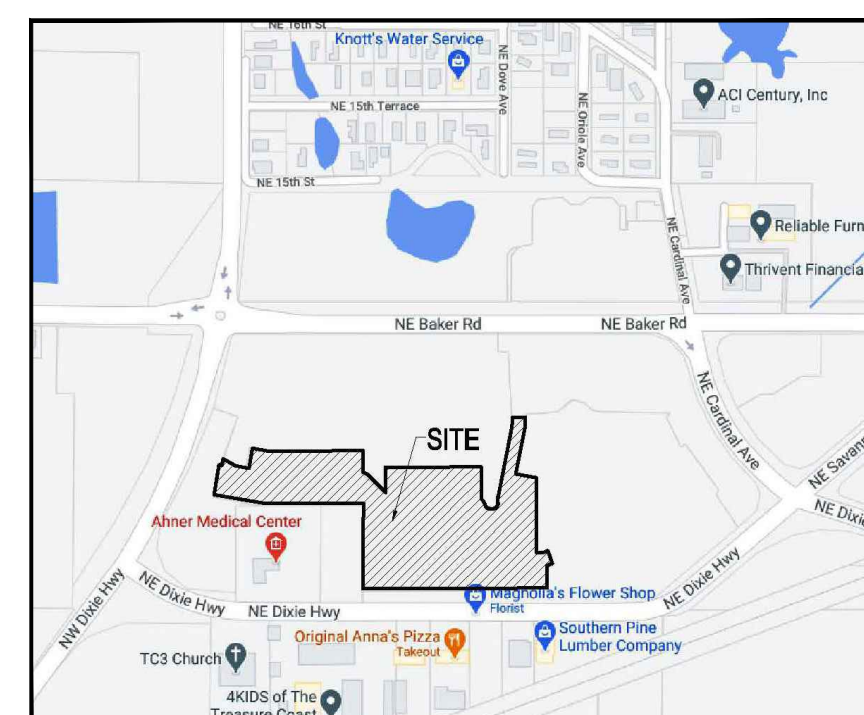
SITE DATA (CONT.):

PARKING CALCULATIONS:	
REQUIRED PARKING	
1-BEDROOM UNITS (24):	36 SPACES (1.5 SPACES PER 1-BR UNIT)
2-BEDROOM UNITS (75):	150 SPACES (2 SPACES PER 2-BR UNIT)
3-BEDROOM UNITS (24):	48 SPACES (2 SPACES PER 2-BR UNIT)
TOTAL PARKING REQUIRED:	234 SPACES
TOTAL PARKING PROVIDED:	*236 SPACES
*INCLUDES 149 ON-GRADE PARKING, 48 GARAGE SPACES AND 39 DRIVEWAY SPACES. ALSO INCLUDES 6 - ELECTRIC VEHICLE PARKING/CHARGING SPACES	
HANDICAP SPACES REQUIRED:	
150 SPACES (2 SPACES PER 2-BR UNIT)	7 SPACES
48 SPACES (2 SPACES PER 2-BR UNIT)	11 SPACES
TOTAL HANDICAP SPACES REQUIRED:	20 SPACES (2 SPACES PER 5 UNITS, 20 SPACES MAX.)
TOTAL HANDICAP SPACES PROVIDED:	30 SPACES
SETBACKS:	
FRONT (SOUTH):	11'
REAR (NORTH):	15'
SIDE (EAST):	12.4'
SIDE (WEST):	57.4'
*SETBACK REQUIREMENTS AS DETERMINED BY CITY COMMISSION	

GENERAL NOTES

- This project shall be completed in one phase.
- All prohibited exotic plant species shall be removed prior to the issuance of a Certificate of Occupancy.
- All required landscaping shall be installed prior to the issuance of a Certificate of Occupancy. Refer to the Landscape Plans for landscape details and specifications.
- Refer to Construction Drawings for location of traffic control devices.
- The applicant will request in writing, prior to any land clearing, that a field inspection be performed to determine if barricades have been properly installed on the site.
- All signs will be reviewed for compliance with the applicable regulations at the time the sign permit is issued.
- During construction activities, existing native vegetation shall be retained to act as buffers between adjacent land uses, and to minimize nuisance dust and noise. Barricades shall be used on site to preserve the vegetation to be retained.
- All building, parking and access areas shall document compliance with the requirements of the American Disabilities Act prior to the issuance of a building permit.
- The property owner shall be required to provide adequate easements to ensure non-exclusive access to duty licensed cable television services to persons residing in this development. The easement shall specifically provide for the use by the franchised cable television system.
- The parking areas shown hereon are private and will be owned and maintained by the property owner.
- Finished Floor Elevations "FFE" are NAVD 88.
- Water and sewer service shall be provided by Martin County Utilities.
- Photometric site lighting plans shall be provided to the City of Stuart for review and approval prior to site development permit approval.
- The drainage system shown hereon is private and will be owned and maintained by the property owner.
- Bike rack shall be per Sec. 6.01.20 LDC.

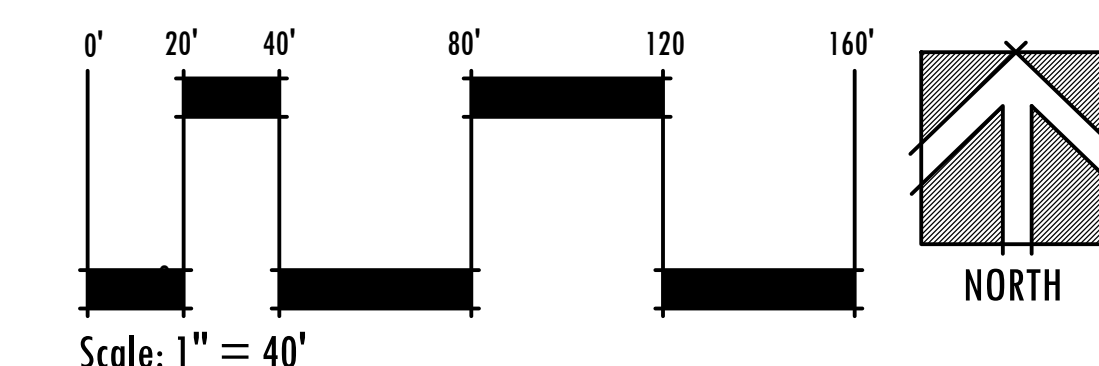
LOCATION MAP



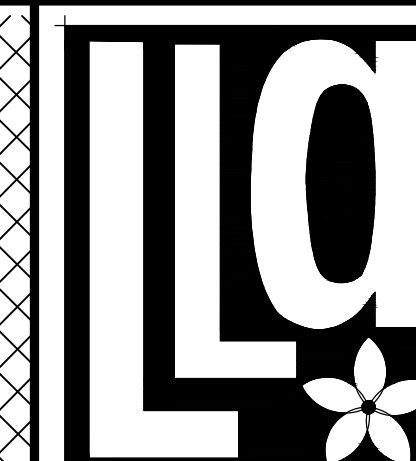
LEGEND

	Pavers		Dry Retention
	Pavers		Sidewalk

Site Plan



NOTE: SIDEWALK CONNECTION TO PARCEL 11 REQUIRED TO BE CONSTRUCTED WITH AVONLEA 13 AS PER AVONLEA STREETSCAPE DESIGN PLAN.



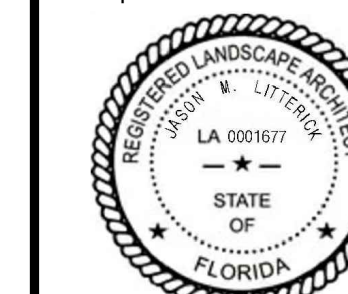
Litterick Landscape Architecture

2740 SW Martin Downs Blvd. #199
Palm City, FL 34990
561-719-3876
JasonLA1677@yahoo.com

Project Name

Parcel 13 - Avonlea
City of Stuart, Florida

Landscape Architect of Record



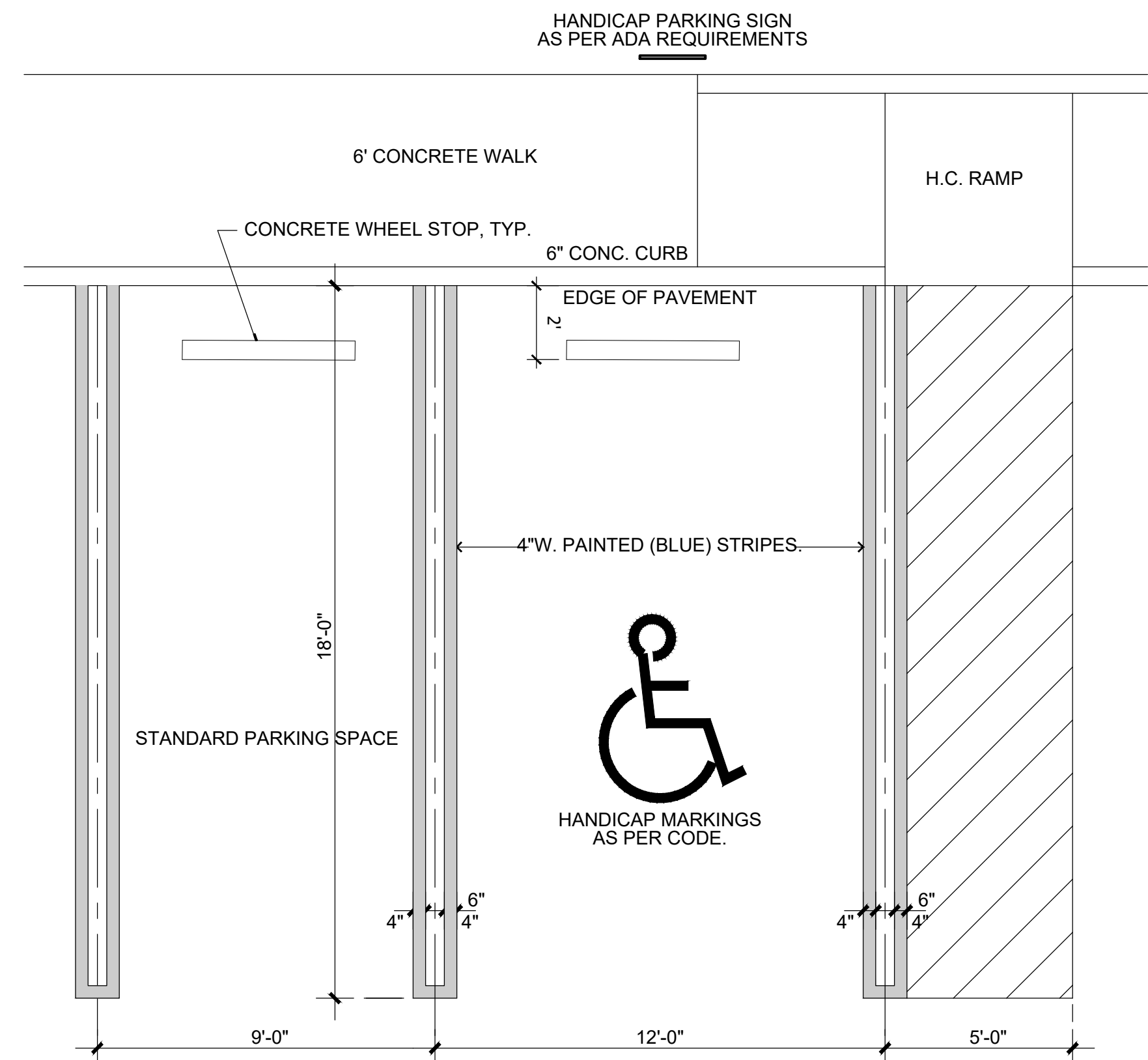
Jason M. Litterick, RLA (LA0001677)

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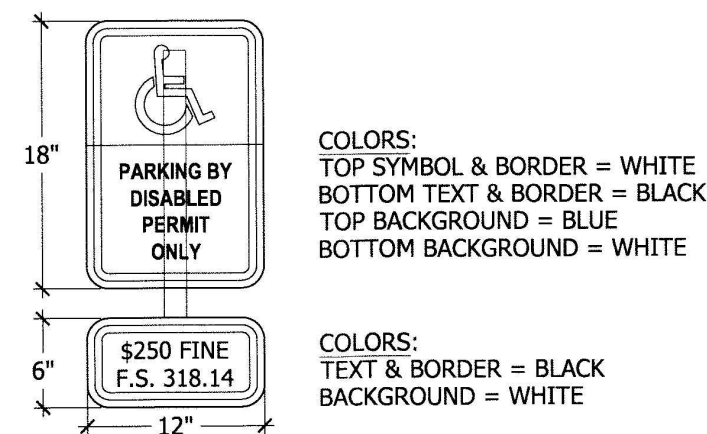
Designed: JML
Drawn: JML
Approved: JML
Date: 11/28/22
Job no.
Revisions:

Sheet No.

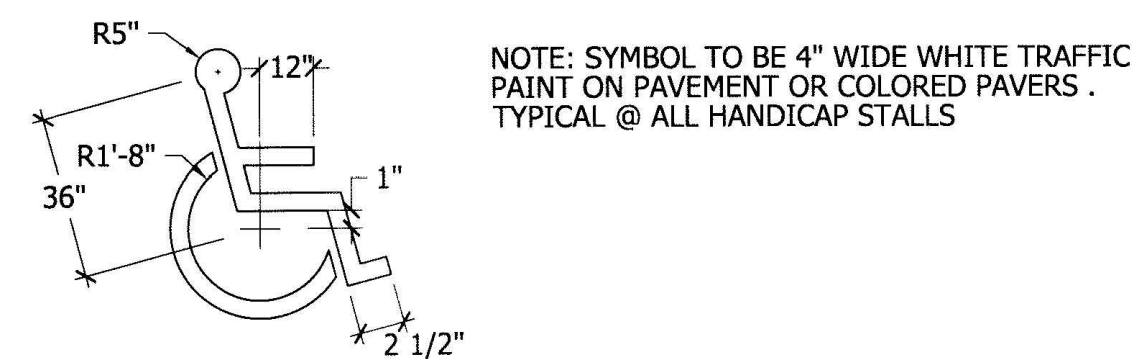
SP-1



STANDARD & HANDICAP PARKING LAYOUT
N.T.S.



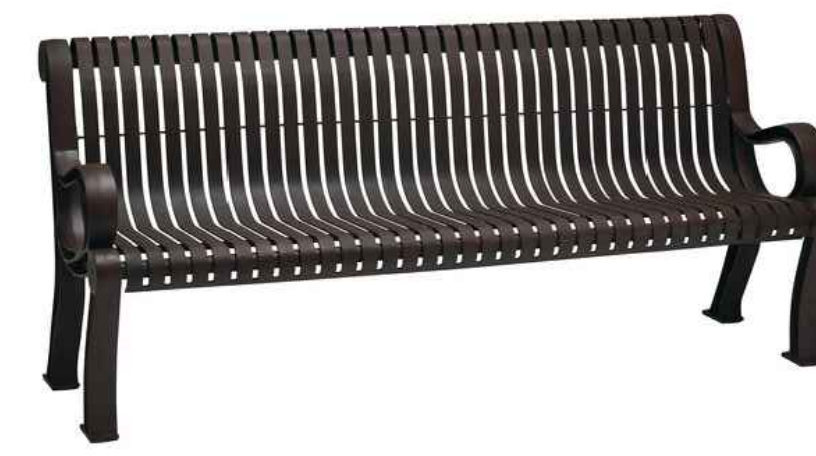
HANDICAP SIGN DETAIL
N.T.S.



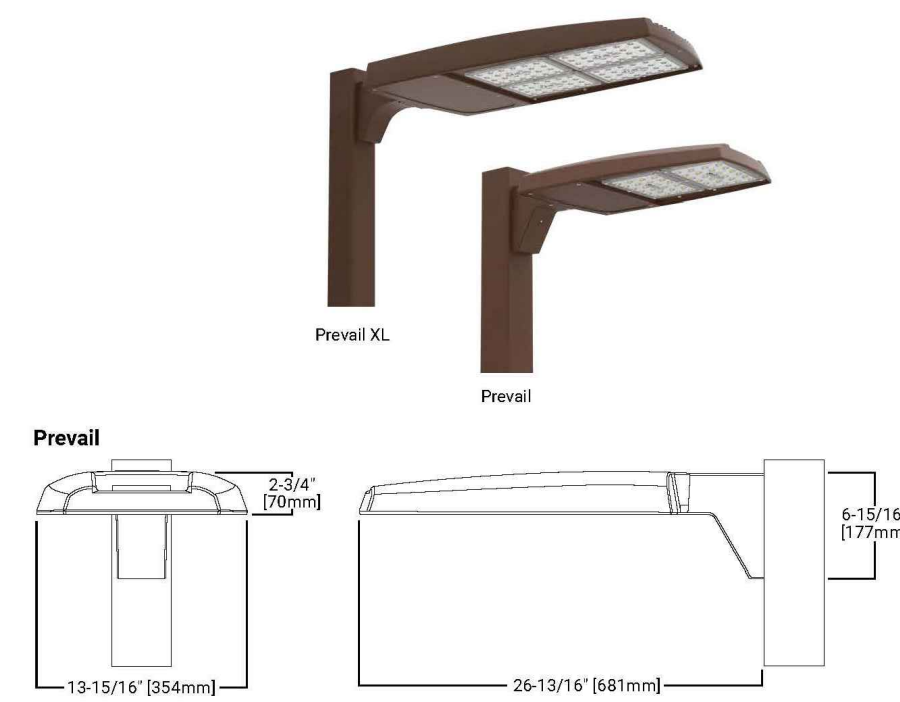
HANDICAP SYMBOL DETAIL
N.T.S.



BLACK POWDER COATED "U-SHAPED" BIKE RACK
QTY: 10 (TEN)
BIKE RACK DETAIL
N.T.S.



WABASH VALLEY BU113C BUTLER 72" SURFACE MOUNT POWDER COATED ALUMINUM VERTICAL SLAT OUTDOOR BENCH W/ ARMS (QTY: 7)
6' BENCH DETAIL
SCALE: NOT TO SCALE



VALMONT STRUCTURES, INC.
DIRECT BURIAL CONCRETE POLE
PARKING LOT LIGHTING FIXTURE
TO BE USED ON ALL PARKING LOT LIGHTING (20' MOUNTING HEIGHT)
N.T.S.



GWC GALLEON WALL

1-2 Light Squares
Solid State LED

WALL MOUNT LUMINAIRE

WaveLinx

MCGRAW - EDISON WALL MOUNT LED LUMIERE
BUILDING MOUNTED LIGHTING
N.T.S.



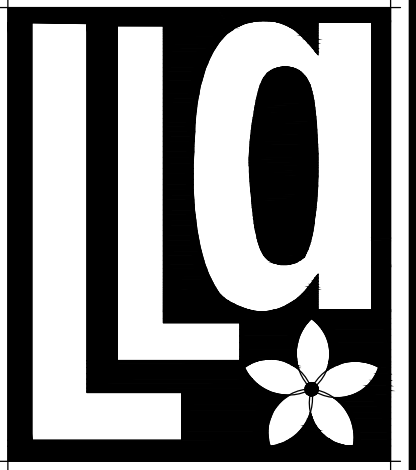
GPC GALLEON PEDESTRIAN COMPANION

1-2 Light Squares
Solid State LED

AREA/SITE LUMINAIRE

WaveLinx

MCGRAW - EDISON 12' POLE MOUNT LED LUMIERE
PEDESTRIAN LIGHTING
N.T.S.



Litterick Landscape Architecture

2740 SW Martin Downs Blvd. #199
Palm City, FL 34990
561-719-3876
JasonLA1677@yahoo.com

Project Name

Parcel 13 - Avonlea
City of Stuart, Florida

Landscape Architect of Record



Jason M. Litterick, RLA
(LA0001677)

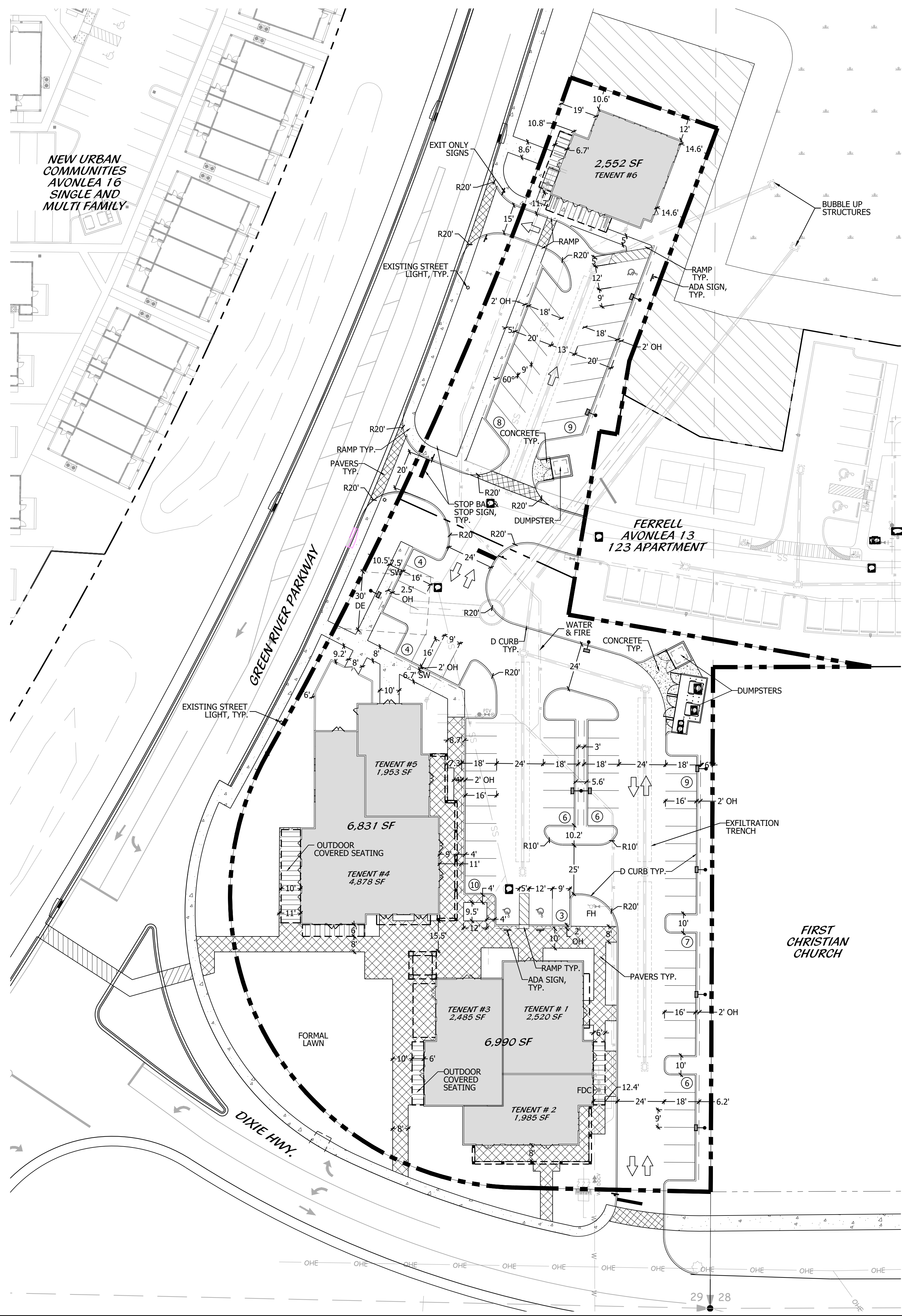
Scale: As Noted

Designed: JML
Drawn: JML
Approved: JML
Date: 11/28/22
Job no.
Revisions:

Sheet No.

SP-2

Site Details



SITE DATA

NAME OF PROJECT	AVONLEA LOTS 14 & 15
PROPERTY CONTROL NUMBER	28-37-41-013-000-00150-0 28-37-41-013-000-00140-0
FEMA FLOOD ZONE	X
LAND USE DESIGNATION	NEIGHBORHOOD/SPECIAL COMMERCIAL PLANNED UNIT
ZONING	COMMUNITY REDEVELOPMENT AREA
OVERLAY DISTRICT	
MAXIMUM BUILDING HEIGHT	35- FEET
NUMBER OF STORIES	1
NUMBER OF BUILDINGS	3

TOTAL SITE AREA	2.05 AC
	89,505.14 SF

BUILDING DATA	2,552.00 SF
	6,831.00 SF
	6,990.00 SF
TOTAL SQUARE FOOTAGE	16,373.00 SF

FAR 1.00

LAND USE	SF	AC	%
BUILDING LOT COVERAGE	16,372	0.38	18.29%
VEHICULAR USE AREA	31,725	0.73	35.44%
OPEN SPACE	41,409	0.95	46.26%
TOTAL	89,505	2.05	100.00%

LOT COVERAGE IMPERVIOUS AREA	SF	AC	%
BUILDING LOT COVERAGE	16,372	0.38	18.29%
VEHICULAR USE AREA	31,725	0.73	35.44%
SIDEWALKS & HARDCAPE	13,014	0.30	14.54%
TOTAL IMPERVIOUS AREA	61,110	1.40	68.28%

PERVIOUS AREA	SF	AC	%
GREEN SPACE	28,395	0.65	31.72%
TOTAL PERVIOUS AREA	28,395	0.65	31.72%

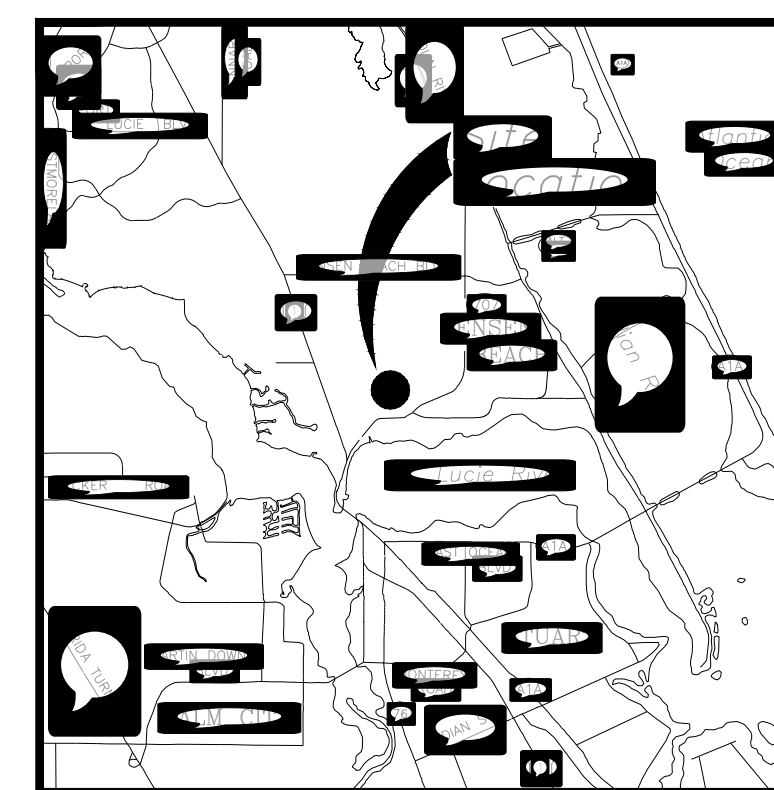
TOTAL SITE AREA 89,505 2.05 100.00%

PARKING DATA	REQ	PROV
PROFESSIONAL OFFICE (1/250 SF)	65.49	72
TOTAL	65.49	72
HANDICAP SPACES (INCLUDED IN TOTAL)	3	3

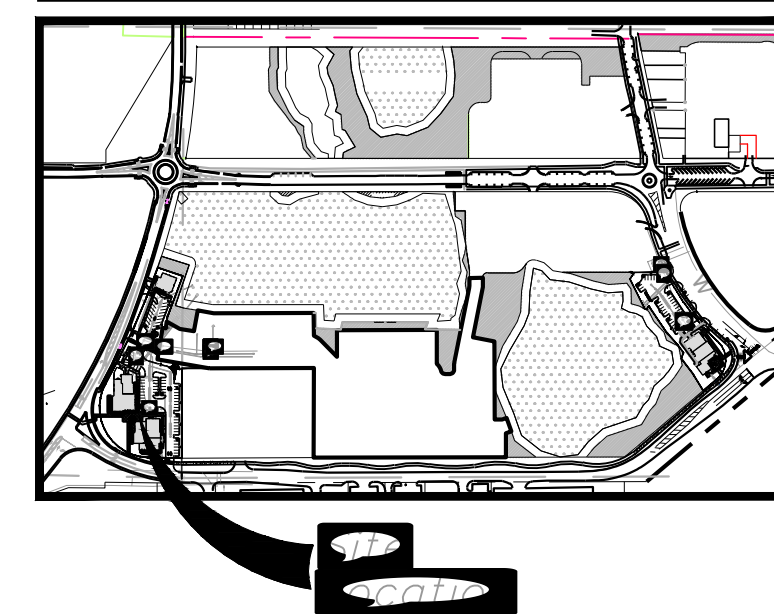
GENERAL NOTES

REFUSE SERVICE SHALL BE ACCOMPLISHED BY WAY OF INDIVIDUAL DUMPSTER AND RECYCLING AS SHOWN ON THE SITE PLAN. 10' X 30' SAFE SIGHT TRIANGLE SHALL BE PROVIDED AT THE PROJECTS ENTRY ROADS. TYPE D OR F RAISED CONCRETE CURBING SHALL BE PROVIDED AROUND ALL PLANTING ISLANDS WITHIN VEHICULAR USE AREAS. ON STREET PARKING IS UTILIZED IN THE CALCULATION FOR REQUIRED PARKING IN ACCORDANCE WITH THE PROVISIONS OF THE MFD CODE. RAMP SHALL BE PROVIDED AT ALL PLAZA, SIDEWALK AND STREET INTERSECTIONS IN ACCORDANCE WITH ADA AND THE FLORIDA HANDICAP ACCESSIBILITY CODE. ALL A/C UNITS SHALL BE LOCATED TO THE REAR OR SIDE OF ALL BUILDINGS AND FULLY SCREENED ON THREE SIDES WITH LANDSCAPING. THE BUILDING, LANDSCAPE AND LIGHTING DESIGN SHALL MAXIMIZE THE USE OF OPTED DESIGN PRINCIPLES. MINIMUM TYPE CONSTRUCTION FOR ALLOWABLE HEIGHT & BUILDING AREA MUST BE COMPLIANCE WITH THE FLORIDA BUILDING CODE T-900 (AFTER OCT. 1, 2005 FBC 2004) ALL SIGNAGE SHALL CONFORM TO THE REQUIREMENTS OF THE TOWN OF JUPITER LDR'S. *PEDESTRIAN KING* SIGNS SHALL BE PROVIDED FOR ALL MID-BLOCK CROSSINGS. PROJECT ID SIGNS SHALL BE ILLUMINATED WITH GROUND MOUNTED FIXTURES ON PHOTOCELLS. THE PHOTOMETRIC PLAN REFLECTS MAINTAINED LIGHTING VALUES IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN OF JUPITER LDR'S. STREET LIGHTS TO BE FPL COACH LIGHTS MOUNTED ON 12' TALL POLES. LIGHT LEVELS SHALL NOT EXCEED THE MAXIMUM TO MINIMUM ESTABLISHED IN TABLE 1 OF ORDINANCE NO. 66-98.

LOCATION MAP



KEY MAP



PROJECT TEAM

OWNER/CLIENT:
AVONLEA COMMERCIAL CORNERS, LLC
100 SW ALBANY AVENUE, SUITE 200
STUART, FL 34994
772.286.6292
CONTACT: NIKOLAUS SCHROTH

LANDSCAPE ARCHITECT/PLANNER:
COTLEUR & HEARING, INC.
1934 COMMERCE LANE, SUITE 1
JUPITER, FL 33458
561.747.6336
CONTACT: DON HEARING

ARCHITECT:
TI ARCHITECTS
3000 HIGH RIDGE ROAD, BAY 4
BOYNTON BEACH, FL 33426
561.860.2905
CONTACT: HAROLD TUTTLE AIA

CIVIL ENGINEER:
VELCON GROUP
702 SW PORT ST. LUCIE BOULEVARD
PORT ST. LUCIS, FL 34953
772.879.0477
CONTACT: BOB KEMMERSON, PE

SURVEYOR:
VELCON GROUP
702 SW PORT ST. LUCIE BOULEVARD
PORT ST. LUCIS, FL 34953
772.879.0477
CONTACT: DARREN GUETTLER

TRAFFIC CONSULTANT:
MACKENZIE ENGINEERING AND PLANNING, INC.
1172 SW 30TH STREET, SUITE 500
PALM CITY, FL 34990
772.286.8030
CONTACT: SHAUN G. MACKENZIE, PE

LEGEND

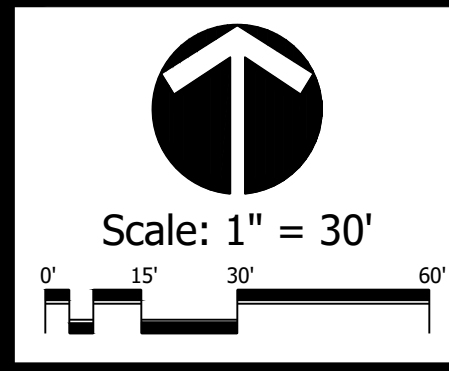
ADA	AMERICANS WITH DISABILITIES ACT	HC SIGN
LB	LANDSCAPE BUFFER	STOP SIGN
R	RADIUS	DO NOT ENTER
SB	SETBACK	PARKING LIGHT
SW	SIDEWALK	
TYP	TYPICAL	

Cotleur & Hearing
Landscape Architects
Land Planners
Environmental Consultants
1934 Commerce Lane
Suite 1
Jupiter, Florida 33458
561.747.6336 · Fax 747.1377
www.cotleurhearing.com
Lic# LC-26000535

Avonlea Commercial Corners

Lots 14 & 15 Site Plan
Stuart, Florida

DESIGNED	DEH
DRAWN	RO
APPROVED	DEH
JOB NUMBER	21-0401
DATE	08-29-22
REVISIONS	



SHEET 1 OF 2
© COTLEUR & HEARING, INC.
These drawings are the property of the architect and are not to be used for extensions or on other projects except by agreement in writing with the architect. Immediately report any discrepancies to the architect.

Basic Info

PIN 28-37-41-013-000-00100-0	AIN 1115822	Situs Address 300 NE BAKER RD STUART FL	Website Updated 4/24/24
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General Information

CHANGE MAILING ADDRESS SIGN UP FOR PROPERTY FRAUD ALERT

Property Owners FARRELL COMMUNITIES AT AVONLEA II LLC	Parcel ID 28-37-41-013-000-00100-0	Use Code/Property Class 9900 - 9900 Vacant Acreage
Mailing Address PO BOX 14 2317 MONTAUK HWY BRIDGEHAMPTON NY 11932	Account Number 1115822	Neighborhood M1 M1-JENSEN/RIO/HI
Tax District STUART	Property Address 300 NE BAKER RD STUART FL	Legal Acres 4.06
	Legal Description LOT 10 NEW AVONLEA PUD ACCORDING TO THE ...	Ag Use Size (Acre\Sq Ft) N/A

Current Value

Year	Land Value	Improvement Value	Market Value	Value Not Taxed	Assessed Value	Total County Exemptions	County Taxable Value
2023	\$ 833,920	\$ 0	\$ 833,920	\$ 0	\$ 833,920	\$ 0	\$ 833,920

Market values shown on the website reflect market conditions as of January 1st, the statutory assessment date. We are prohibited by law from relying on sales that occur after the January 1 assessment date. Therefore, market values shown on the website do not reflect today's market conditions, but rather the market conditions last year. In addition, the statutes require the county Property Appraiser to deduct for typical costs of sale (which include expenses such as commissions, title insurance, appraisals, inspection fees, etc.) when arriving at market value for tax purposes. That is why the market value for tax purposes is different from what a property would sell for today.

Current Sale

Sale Date 5/18/21	Grantor (Seller) FRANK A WACHA JR TRUSTEE	Doc Num 2890800
Sale Price \$ 1,000,000	Deed Type Warranty Deed	Book & Page <u>3226 2723</u>

Legal Description

LOT 10 NEW AVONLEA PUD ACCORDING TO THE PLAT THEREOF RECORDED IN PLAT BOOK 17 PAGE 35 PUBLIC RECORDS MARTIN COUNTY FLORIDA

The legal description is intended for general information only. The Property Appraiser assumes no responsibility for the uses or interpretations of the legal description.

Improvements

PIN 28-37-41-013-000-00100-0	AIN 1115822	Situs Address 300 NE BAKER RD STUART FL	Website Updated 4/24/24
--	-----------------------	---	-----------------------------------

Use Code/Property Class N/A	Total Finished Area 0 SF	Max Stories 0
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Features/Yard Items

Type	Qty	Size	Unit of Measure	Year Blt
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Basic Info

PIN 28-37-41-013-000-00130-0	AIN 1115825	Situs Address 35 NE DIXIE HWY STUART FL	Website Updated 4/24/24
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General Information

CHANGE MAILING ADDRESS

SIGN UP FOR PROPERTY FRAUD ALERT

Property Owners

FARRELL COMMUNITIES AT AVONLEA LLC

Parcel ID

28-37-41-013-000-00130-0

Use Code/Property Class

9900 - 9900 Vacant Acreage

Mailing Address

PO BOX 14 2317 MONTAUK HWY
BRIDGEHAMPTON NY 11932

Account Number

1115825

Neighborhood

M1 M1-JENSEN/RIO/HI

Tax District

STUART

Property Address

35 NE DIXIE HWY STUART FL

Legal Acres

6.35

Legal Description

LOT 13 NEW AVONLEA PUD ACCORDING TO THE ...

Ag Use Size (Acre\Sq Ft)

N/A

Current Value

Year	Land Value	Improvement Value	Market Value	Value Not Taxed	Assessed Value	Total County Exemptions	County Taxable Value
2023	\$ 1,688,670	\$ 0	\$ 1,688,670	\$ 21,774	\$ 1,666,896	\$ 0	\$ 1,666,896

Market values shown on the website reflect market conditions as of January 1st, the statutory assessment date. We are prohibited by law from relying on sales that occur after the January 1 assessment date. Therefore, market values shown on the website do not reflect today's market conditions, but rather the market conditions last year. In addition, the statutes require the county Property Appraiser to deduct for typical costs of sale (which include expenses such as commissions, title insurance, appraisals, inspection fees, etc.) when arriving at market value for tax purposes. That is why the market value for tax purposes is different from what a property would sell for today.

Current Sale

Sale Date 9/21/20	Grantor (Seller) STB OUTDOORS LLC	Doc Num 2838931
Sale Price \$ 1,700,000	Deed Type Special Warranty Deed	Book & Page 3159 2327

Legal Description

LOT 13 NEW AVONLEA PUD ACCORDING TO THE PLAT THEREOF RECORDED IN PLAT BOOK 17 PAGE 35 PUBLIC RECORDS MARTIN COUNTY FLORIDA

The legal description is intended for general information only. The Property Appraiser assumes no responsibility for the uses or interpretations of the legal description.

Improvements

PIN 28-37-41-013-000-00130-0	AIN 1115825	Situs Address 35 NE DIXIE HWY STUART FL	Website Updated 4/24/24
--	-----------------------	---	-----------------------------------

Use Code/Property Class N/A	Total Finished Area 0 SF	Max Stories 0
---------------------------------------	------------------------------------	-------------------------

Features/Yard Items

Type	Qty	Size	Unit of Measure	Year Blt
------	-----	------	-----------------	----------

Basic Info

PIN 28-37-41-013-000-00140-0	AIN 1115826	Situs Address UNASSIGNED	Website Updated 4/24/24
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General Information

CHANGE MAILING ADDRESS

SIGN UP FOR PROPERTY FRAUD ALERT

Property Owners

AVONLEA COMMERCIAL CORNERS LLC

Parcel ID

28-37-41-013-000-00140-0

Use Code/Property Class

1000 - 1000 Vacant Commercial

Mailing Address

100 SW ALBANY AVE #200
STUART FL 34994

Account Number

1115826

Neighborhood

M1 M1-JENSEN/RIO/HI

Tax District

STUART

Property Address

UNASSIGNED

Legal Acres

1.59

Legal Description

LOT 14 NEW AVONLEA PUD ACCORDING TO THE ...

Ag Use Size (Acre\Sq Ft)

N/A

Current Value

Year	Land Value	Improvement Value	Market Value	Value Not Taxed	Assessed Value	Total County Exemptions	County Taxable Value
2023	\$ 273,575	\$ 0	\$ 273,575	\$ 8,755	\$ 264,820	\$ 0	\$ 264,820

Market values shown on the website reflect market conditions as of January 1st, the statutory assessment date. We are prohibited by law from relying on sales that occur after the January 1 assessment date. Therefore, market values shown on the website do not reflect today's market conditions, but rather the market conditions last year. In addition, the statutes require the county Property Appraiser to deduct for typical costs of sale (which include expenses such as commissions, title insurance, appraisals, inspection fees, etc.) when arriving at market value for tax purposes. That is why the market value for tax purposes is different from what a property would sell for today.

Current Sale

Sale Date 7/27/20	Grantor (Seller) FRANK A WACHA JR TRUSTEE	Doc Num 2828921
Sale Price \$ 275,000	Deed Type Special Warranty Deed	Book & Page <u>3147 0003</u>

Legal Description

LOT 14 NEW AVONLEA PUD ACCORDING TO THE PLAT THEREOF RECORDED IN PLAT BOOK 17 PAGE 35 PUBLIC RECORDS MARTIN COUNTY FLORIDA

The legal description is intended for general information only. The Property Appraiser assumes no responsibility for the uses or interpretations of the legal description.

Improvements

PIN 28-37-41-013-000-00140-0	AIN 1115826	Situs Address UNASSIGNED	Website Updated 4/24/24
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Use Code/Property Class N/A	Total Finished Area 0 SF	Max Stories 0
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Features/Yard Items

Type	Qty	Size	Unit of Measure	Year Blt
------	-----	------	-----------------	----------



To: Jodi Kugler
Director
City of Stuart Development Department

From: Alex Memering, P.E.
Kimley-Horn and Associates, Inc. ARM

Date: August 22, 2025

Re: **Avonlea**
Final Review of Traffic Impact Study

Kimley-Horn has reviewed the traffic impact study (revised June 2025) prepared by Makenzie Engineering & Planning, related to the above-mentioned project. The proposed project consists of 23.2 KSF of retail space, 123 low-rise multifamily units and 80 low-rise multifamily units with ground floor commercial. The project site is located at the northwest corner of Baker Road & Green River Parkway in Stuart, Florida.

It is anticipated that the proposed trip generation potential for the site is as follows: 156 (59 entering / 97 exiting) AM peak hour trips, and 257 (145 entering / 112 exiting) PM peak hour trips. The analysis included an assessment of the proposed trip generation, roadway analysis and intersection analysis.

Thank you for the opportunity to assist the City of Stuart in reviewing this project. Please contact us if you have any questions or need additional information.

**CITY OF STUART, FLORIDA
AGENDA ITEM REQUEST
Community Redevelopment Board**

Meeting Date: 2/3/2026

Prepared by: Mary Kindel

Title of Item:

ADVISORY BOARD ORIENTATION REVIEW AND PUBLIC RECORDS PRESENTATION

Summary Explanation/Background Information on Agenda Request:

Public Record and Sunshine Law Board presentation.

Funding Source:

N/A

Recommended Action:

N/A

ATTACHMENTS:

1. Board_Committees Public Records Training_CITY2026

Public Records Training for Board/Committee Members

MARY KINDEL, CITY CLERK

LEE BAGGETT, CITY ATTORNEY



Introduction



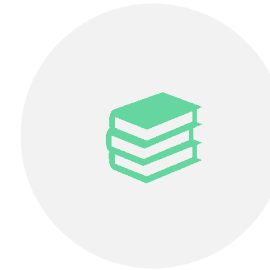
What is a Public Record and who it applies to. The Public Records Law applies to **elected** and **appointed** Board Members



Board members are required to **preserve** and **transfer** any public records in their possession to the City and to **respond** to public records requests received



Duty to **preserve** and **not delete**- know what records should be submitted to the City for saving and what may be deleted/destroyed



Understand Board Member responsibilities after term expiration or board service ends

What is a public record?

- A public record is any record made in connection with the transaction of official City business, regardless of physical form, characteristic or means of transmission.
- This includes, but is not limited to paper, letters, maps, books, tapes, photographs, videos, recordings, voicemails, text messages, emails, and social medial posts.
- It is the **content** of the record that matters, not where it is kept.



Is the Public Records Law applicable to Elected & Appointed Board Members?

Yes, public records law applies to both elected and appointed board members. Any records received or created in connection with board duties – including emails, texts, and written communications, are public record.

Applicable boards include:

City Commission, CRA, CRB, & LPA

This means:

01

You are legally responsible for the preservation and transfer of those records to the City in Compliance with Chapter 119, Florida Statutes

02

This legal obligation to transfer public records extends to records that exist on **personal devices** or in **personal accounts**. All records should be preserved and maintained even after they are transfer-red to the City

03

Upon resignation or expiration of your term, you are required to transfer all public records to the City within 10 days after leaving office pursuant to Sec119.021(4)(b)F.S. **Then** records may be deleted.

Upon Appointment



Attention Elected officials and CRA board members only: Required to take an Annual 4-Hour Ethics Training on Public Records and Sunshine Law.



ALL Boards are to receive training and documentation on Public Records and the Sunshine Law.



Orientation Packet provided by the City Clerk.



Receive the Florida Commission on Ethics Guide to Sunshine Amendment and Code of Ethics for Public Officers and Employees.



Board By-Laws, Public Meeting Calendar, & Helpful Tools informational page.

Upon Appointment

Board Members receive a City assigned email address for public record purposes.

If you are issued a City email, all City/Board communication **must** be conducted using that City email.



If an email, text message, or other electronic communication (ex. social media posts, comments, etc.) is received on a personal account, the following response shall be sent to the sender, with a copy sent to your City Liaison:

“As a Board member, I am subject to the Florida Public Records Law and do not conduct any City business using this private account (or number). Therefore, if your message concerns Board business, you should redirect/resend it to my City of Stuart email address at

_____@stuartfl.gov.”

Submittal of Public Records to the City

Records may be submitted to the City via email, in person or via mail.



Text messages- take a screenshot of the conversation and forward to your City contact via email. Option to utilize text messaging software.



Videos- Download the video and forward to your City contact via email or place the video on a USB and provide to your City contact.



Social Media Posts- take a screenshot of the post, along with all the comments, etc. and forward to your City contact.



Emails sent or received using your non-City assigned email address should be forwarded to your City email address and City contact.



It is your responsibility to keep your records after they are provided in response to a request. Each new request will require you to provide the records.

Are there records that are related to City business that are not considered a **Public Record?**

YES, notes and rough drafts



Notes

Personal notes used only to jog your memory are not public records

Notes may be discarded when no longer needed

Once shared, notes become public records and may be subject to disclosure

Personal notes should be kept private and not accessible to others



Rough Drafts

Jotted down notes used to prepare a draft are not public record

Draft documents are public records once created

Sharing rough draft notes makes them public records

Shared drafts or notes must be provided to the City if requested

Rough draft notes should be kept private and not accessible to others

Public Records Requests

- If anyone makes a request to you for a copy or to inspect records related to your duties on the Board, please direct them to submit the request to the City's Records Custodian. They may submit a request directly **to the City or you**, then you pass it on to the City.
- Once a public records request is received, your City contact will reach out to you via email.
- Search your files, including electronic devices and accounts and gather any records related to the request.
- Send any records found in an email to your City contact.
- If you have no records, confirm it in an email to your City contact.



Expiration or Resignation of Term

Upon resigning or on the expiration of your term, you will:

01

Check your files, devices and accounts, gather any records to be submitted to your City contact.

02

All public records should be turned over to the City within 10 days of exiting Office.

03

This is pursuant to Section 119.021(4)(b), F.S. Criminal consequences can result from failure to turn over the records to the City.

Consequences of Public Records Violations

What happens if I am found to knowingly, purposefully, or maliciously violate the Public Records Law?



Failure to comply with the public records law may result in:

- ✓ The City having a judgement entered against it for violating public records law in a civil action and ordered to pay the attorney fees of the petitioner- there is no cap on attorney fees.
- ✓ Your arrest for a misdemeanor charge, punishable by up to a year in prison, or a \$1,000.00 fine, or both.
- ✓ Issuance of a citation to you for a noncriminal infraction and made to pay \$500.00 if you are found guilty.
- ✓ You could be removed from the Board.
- ✓ You could also be held individually liable in civil court.

Financial Disclosure

Financial disclosure is required of public officials and employees because it enables the public to evaluate potential conflicts of interest, deters corruption, and increases public confidence in government.



The Florida Commission on Ethics requires Board Members to file **financial disclosure forms pursuant to Chapter 112.3145 Florida Statutes.**

New Board Members: **Form 1**; Within 30 days of appointment then annually by July 1 of each year.

Exiting Board Members: **Form 1F**; Within 60 days after leaving appointment, unless taking another position that requires financial disclosure.



Florida Sunshine Law

Chapter 286, Florida Statutes

- **Applies to advisory boards**
- **Covers meetings of two or more members discussing board business**
- **Meetings must be open to the public with reasonable notice**
- **Minutes or records must be maintained**
- **Applies to all formats (in-person, phone, or electronic communication)**

Takeaways

- 1. This Board is subject to Sunshine Law & Public Records Law.**
- 2. It is your responsibility to maintain all Board business related records, except for emails through your City email address.**
- 3. You will be contacted by the City if a Public Records Request is made for your records.**
- 4. Do your best to limit City related texts or communication on personal devices.**
- 5. When separation from the Board occurs, submit all public records to the City Clerk.**

Questions

Mary Kindel, City Clerk

772-288-5306

mkindel@ci.stuart.fl.us

Lee Baggett, City Attorney

772-288-5386

lbaggett@ci.stuart.fl.us